Agenda Item A.1
PUBLIC HEARING
Meeting Date: September 11, 2023

TO: Planning Commissioners

FROM: Peter Imhof, Planning and Environmental Review Director

CONTACT: Mary Chang, Supervising Senior Planner
Christina McGuire, Associate Planner

SUBJECT: Request for approval of a Conditional Use Permit and Development Plan to allow a mixed-use project consisting of 14 residential units and 1 office unit and determination that the mixed-use project is exempt under the California Environmental Quality Act at 6491 Calle Real; APN 077-160-066; Case Nos. 22-0005-DP, 22-0001-CUP, 23-0028-DRB.

RECOMMENDATION:

It is recommended that the Planning Commission:

1. Open a public hearing to take verbal and written testimony; and

2. After considering the evidence presented during the public hearing, adopt Planning Commission Resolution No. 23-___, titled "A resolution of the Planning Commission of the City of Goleta, California, approving the conditional use permit and development plan for a mixed-use project to allow 14 residential units and commercial space for a total of 14,257 square feet and associated site improvements to be located at 6491 Calle Real and finding the project to be categorically exempt from the California Environmental Quality Act under State CEQA Guidelines Section 15332; APN 077-160-066; Case Nos. 22-0005-DP, 22-0001-CUP, 23-0028-DRB" (Attachment 1).

PROPERTY OWNER
6491 Calle Real Partners, LLC
1933 Cliff Dr Suite 2
Santa Barbara, CA. 93109-1585

APPLICANT/AGENT
Tony Tomasello, Principal Planner
RRM Design Group
10 East Figueroa St. Suire 200
Santa Barbara, CA 93101

JURISDICTION

The City is the lead agency for the Project under the California Environmental Quality Act (CEQA), and the Planning Commission will be the final decision maker for the entire Project. The Planning Commission's role is to review the adequacy of the Notice of Exemption (NOE) as well as the merits of the various Project components. As the Planning Commission is the final decision maker for the project, if there is an appeal, the Project would go to the City Council per 17.50.020.

BACKGROUND:

Site Development History

The Project site is currently vacant and was previously used as an orchard until the completion of Los Carneros Road in the 1970s. Through the years, various projects have been proposed at the site, but none have received entitlements, so the site remains vacant.

Current Applications

SB 330

In May 2022, Tony Tomasello, on behalf of RRM Design Group, submitted an SB 330 Preliminary Housing Application, which was deemed complete on May 18, 2022. Under SB 330, the City is limited to a maximum of five hearings on a residential project, including an appeal. This State regulation provides that, if a proposed housing project complies with the applicable, objective General Plan and zoning standards that are in effect when an application is deemed complete, then the City may hold a maximum of five "hearings" in connection with the approval of that project. "Hearing" is defined broadly to include any public hearing, appeal, workshop, or similar meeting, whether by the City Council, the Planning Commission, or any other City department, board, commission, or body, including committees or subcommittees, such as Goleta's DRB or an ad hoc committee comprising a fraction of the members of a legislative body. A continued hearing counts as one of the five hearings allowed. Under SB 330, the City must consider and either approve or disapprove the project at one of the five allowed hearings. The DRB hearing on July 25, 2023 was the first hearing and this Planning Commission hearing will be the second of the five allowed hearings.

Housing Accountability Act

A housing development project is subject to the Housing Accountability Act if the project is mixed-use, such as if it consists of residential and nonresidential uses, and if at least two-thirds of the project's square footage is designated for residential use. This project meets those criteria, so this project is subject to the Housing Accountability Act. The City must disregard density-bonus units and related concessions and waivers when determining whether the proposed project is consistent with objective standards. The Housing Accountability Act, among other things, prohibits a local agency from disapproving or conditioning approval in a manner that renders infeasible a housing

development project for very-low-income, low-income, or moderate-income households or emergency shelter, unless the City makes specific written findings based upon substantial evidence in the record.

State Density Bonus

State Density Bonus Law (SDBL) requires the City to grant a density bonus and waivers or reductions in development standards for a project that commits a certain percentage of its units to affordable housing. In particular, this Project is using SDBL to do the following:

- Increase the number of units from 11, which is allowed by zoning, to 14. The density of the project will go from 20 units/acre allowed in the Office Institutional (OI) zoning district to 26 units/acre. Two of the fourteen units will be deed-restricted for low-income residents for 55 years.
- Decrease the number of required parking spaces for the residential units. Consistent with the SDBL provision, the applicant is providing 14 uncovered parking spaces for the 14 residential units. The applicant is providing 2 parking spaces for the office use consistent with Title 17 parking standards for an office use. In addition, the Project includes 4 guest parking spaces, yielding a total of 20 spaces for the entire project. If the City's parking standards had been used, 28 spaces would have been required for the residential component and 4 guest parking spaces, and 2 for the commercial component, yielding a total of 34 required spaces.
- Receive a concession from the City to not meet the City's parking lot heat island requirements (parking lot shade); and
- Receive a waiver to allow an extra 2.5 feet for the height of a portion of the building.

Conditional Use Permit and Development Plan

Both a Development Plan and a Conditional Use Permit for the proposed mixed-use housing application at 6491 Calle Real with associated parking and site improvements ("Project") are required.

This mixed-use housing Project proposes to construct a three-story, 14,257-square-foot building containing 14 dwelling units and one commercial office space building with parking and associated site improvements on a vacant 0.53-acre parcel at the southeast corner of the roundabout intersection of Los Carneros Road and Calle Real. A more detailed project description is provided later in the staff report.

Santa Barbara Municipal Airport Land Use Compatibility Plan

The project site lies outside of the Santa Barbara Airport's noise exposure range, as indicated in Figure 4-1 of the 2023 Adopted Airport Land Use Compatibility Plan (ALUCP), and it is located in Safety Zone 6, as indicated in Figure 4-2 of the ALUCP. Therefore, the ALUCP does not prescribe any density limitation regarding the number of people on this project site and Airport Land Use Commission review is not warranted.

Design Review

The purpose of Design Review is to encourage the highest quality of design, both visually and functionally, and to reduce or prevent the negative effects of development while also promoting the health, safety, and general welfare of the City's public. The City has adopted Guidelines, Municipal Code provisions, and General Plan policies establishing criteria and review for commercial projects in the City.

On July 25, 2023, the Design Review Board (DRB) conducted both Conceptual and Preliminary Design Review of the proposed Project. The DRB found that the project was well-designed overall and recognized the need for more housing. The DRB unanimously recommended that the Planning Commission grant Preliminary Design approval based on the findings of GMC Section 17.58.080 (see Section D of Attachment 1), subject to the incorporation/consideration of design changes discussed below under the Development Plan in the Discussion section of this report. Further, staff has included resolution of these issues in Condition #55B in Exhibit D of Attachment 1.

The Planning Commission is the decision maker for the design aspect in addition to the land use entitlement components. The applicant has expressed a willingness to work with the DRB and has provided updated elevations, which partially address the DRB's comments (see Figure 5 below later in the report).The Planning Commission may decide if all the topics listed by the DRB need to be resolved prior to Planning Commission action or if they can be left to the DRB to handle at the Final Design Review stage.

These design changes are described further below and listed in Conditional Use Permit and Development Plan Conditions of Approval No. 16.B. The DRB agenda report and meeting video can be accessed as follows:

https://goleta.legistar.com/LegislationDetail.aspx?ID=6294782&GUID=A749908D-A9AA-495B-9CDE-DF72B27C7FD0 (Agenda Materials)

https://goleta.granicus.com/player/clip/1883?view_id=5&redirect=true&h=8eb502eb9787d1b0e0de823e1d7f32ed (Agenda Video)

PROJECT LOCATION/LAND USE DESIGNATIONS

The Project site is located at 6491 Calle Real, at the southeast corner of the Calle Real/Los Carneros roundabout intersection, and it is identified as Assessor Parcel Number 077-160-066. The location is shown in Figure 1 below. Further, the site has a General Plan land use designation and zoning designation of Office Institutional (OI). Mixed-use development is allowed in the OI Zone with a Major Conditional Use Permit.

Figure 1: Project Location and Vicinity

PROJECT DESCRIPTION

The Project includes the following elements:

Conditional Use Permit: This element would allow mixed-use development in the Office Institutional (OI) Zone. Mixed-use development requires a Major Conditional Use Permit in the OI Zoning District. The CUP would establish the parameters of the use of the site including the office component.

Development Plan: The request is to construct a new three-story, mixed-use building on a vacant 0.53-acre lot proposed as a Housing Accountability Act housing development project. The building would total 14,257 square feet, comprising 1) one commercial office space of 585 square feet; plus 2) 14 one-bedroom residential units totaling 11,520 square

feet; and 3) 2,152 square feet of common areas dedicated to residential use. As indicated above, the project is using State density bonus law (SDBL) to increase the density, reduce parking requirements (adhering to parking numbers per SDBL), receive a concession from the City's parking lot heat island requirements, and seek a height waiver to increase the building height by 2.5 feet.

The 14 residential units would range in size from 694 to 979 square feet. Each residential unit will have a private patio or balcony associated with the unit. Title 17 requires 1,400 square feet of Common Open Space and 200 square feet of restricted open space (i.e., private open space) for ground units and 60 square feet for units without floor area on the ground story. The Project meets both of these requirements with the provision of patios and balconies as restricted open space for the individual units and common open space areas on the ground level. These include an approximately 585-square-foot patio that would be shared between the commercial and residential tenants, and 3,454 square feet of outdoor area that would be shared by residents only.

Parking includes 20 uncovered automobile parking spaces plus 10 short-term and 16 long-term bicycle parking spaces. The project includes a trash enclosure and other site amenities. The project will be all-electric and no gas connection is proposed.

Figures 2 – 4 are provided below to show the site and parking layout, the building elevations, and a rendering.

Figure 2: Site Plan

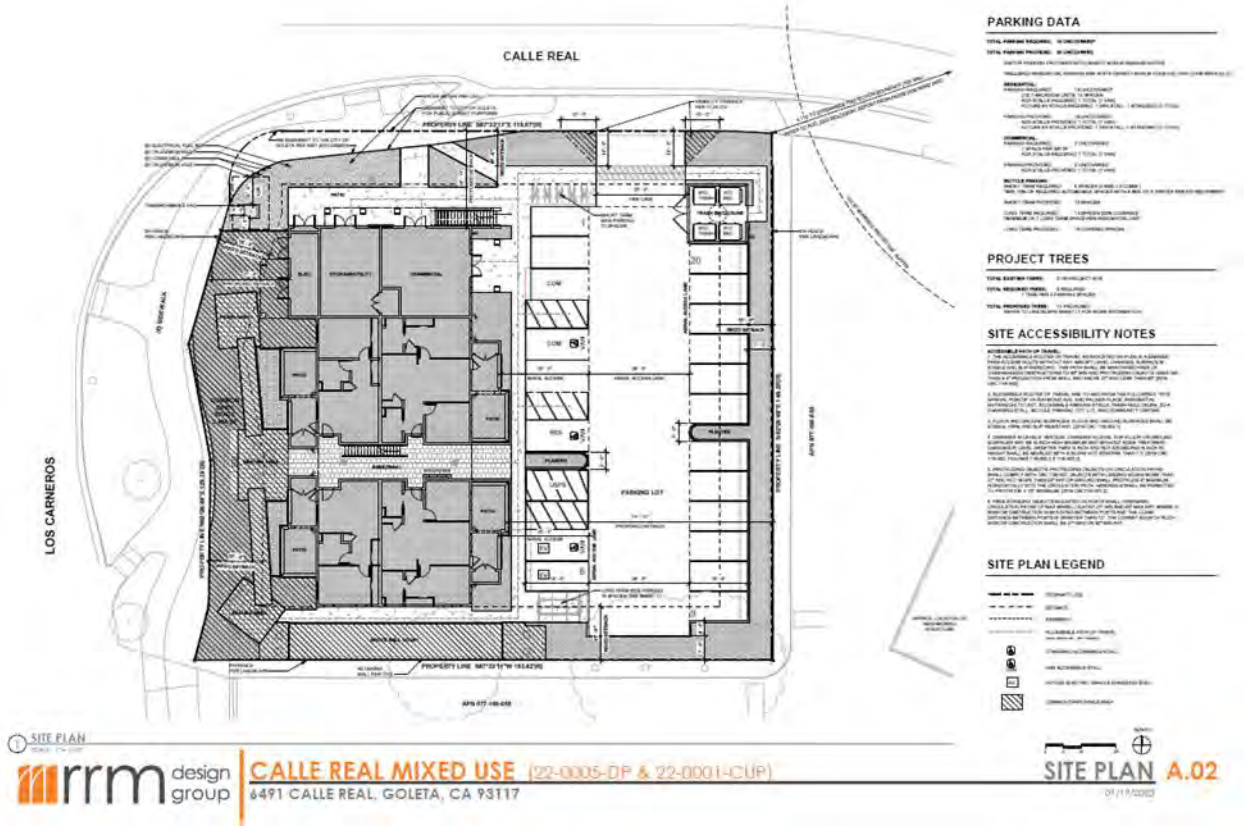

Figure 3: Project Elevations

Figure 4: Rendering

DISCUSSION

Conditional Use Permit

The City of Goleta allows multi-unit development, specifically mixed-use (commercial and residential) in the OI zone only if a Major Conditional Use Permit is approved by the review authority. The commercial component must be allowed by right or entitlement in the Zoning district. The review authority for this project is the Planning Commission.

Staff believes that the findings to recommend approval of this request can be made and the additional mixed-use development standards for open space have been met. In general, the applicable findings contained in Section 17.57.050 to grant the CUP require the project to be found consistent with the General Plan and that the use will not be injurious to the health, safety, and welfare of the uses in the zoning district. The Project is consistent with all the applicable General Plan policies as stated in Exhibit B of Attachment 1 pertaining to use of the site, the provision of housing, aesthetic/viewshed standards, and circulation. Further, the Project site is surrounded on multiple sides by existing development and as such, is considered an in-fill parcel. The proposed building footprint is 5,048 square feet, which results in a lot coverage of 21%. This proposed lot coverage is well below the Office Institutional (OI) lot building coverage maximum of 40%. The layout of the Project fits well within the shape of the site: the building, parking, amenities, etc. fit well at their locations, and their usage with the expected pedestrian and automobile traffic appears to be very reasonable. In summary, the site is adequate in size, shape, location, and physical characteristics to accommodate the density and intensity of the proposed development.

The project's consistency with the Conditional Use Permit findings are outlined in the proposed Resolution (Attachment 1). In addition, Exhibits B and C to the Resolution contain the General Plan and Zoning Consistency analyses.

Development Plan

The requested new Development Plan for the mixed-use housing Project is appropriate because it will provide more rental housing in the City. Further, the Project location is identified as site V16 in Table 10A-28 in the City's adopted 2023-2031 Housing Element. In addition, the site is categorized in the Housing Element at a density of 20 dwelling units per acre, which would yield a maximum of 11 units for the site. This Project is utilizing SDBL and proposes to construct 14 dwelling units, which would be a density of 26.4 units per acre. This represents a 30% density bonus. Based on the City's inclusionary requirements, the project is required to provide 2.8 units as affordable units. To satisfy this requirement, two of the 14 units will be deed-restricted, low-income units and the Applicant will make an in-lieu payment for the remaining fractional requirement.

In accordance with the General Plan land use designation for the site, the proposed project is located on a parcel zoned Office Institutional. As mentioned above, mixed-use development may be allowed with a Major Conditional Use Permit in this zoning district. There are a few ways in which the Project deviates from the City's development

standards, including dwelling unit density, height, parking lot heat island reduction, and parking amount; however, because the project is utilizing SDBL, these standards can be either waived or granted a concession. In order to not grant the requested concessions or waivers, the City would have to make specific written findings based upon substantial evidence in the record to decided that the Project does not meet the objective standards; staff does not believe that there is sufficient substantial evidence to make such findings.

Design and Infrastructure

The proposed Project is located in a neighborhood with a variety of uses that have differing mass, bulk, designs and heights. Directly adjacent to the site there is a 35-foot-high Community Assembly use across Calle Real to the north of the project in an eclectic style using hexagonal forms, red tile roofing, and board and batten siding. Further, there are approximately 20-foot-high single story commercial buildings in a gabled roof-concrete masonry block commercial style located to the east and south of the project; and there is agricultural use across Los Carneros Road to the west of the project. Approximately 0.25 miles to the south of the adjacent commercial buildings (across the US 101 and the Union Pacific Railroad line) are several multiple family neighborhoods (namely Willow Springs I and II and the Village at Los Carneros). The proposed mixed-use project's commercial use is clearly compatible with the existing neighborhood commercial use and the proposed project's residential use is acceptable considering that the neighborhood's existing uses are already so varied.

During the Design Review Board (DRB) meeting, the DRB suggested some design modifications, which the applicant will address when the project returns to the DRB. These suggested design modifications include:

- Relocating or screening infrastructure items at the northwest corner of the site.
- Reconsider color palette of the project.
- Restudy pitched roofs.
- Study South elevation.
- Integrate the downspouts into building and painted to match wall color.
- Better integrate the stair on north elevation with the building.
- Revisit stormwater connection.
- Consider removing bocce ball court and moving bike parking to south of building, and options for an access gate near southwest corner.
- Reconsider specific plant selections.
- Better integrate bike storage.

Overall, the applicant agreed that the comments received by the DRB would enhance the Project and they will be working to address the conditions of the DRB. The applicant has provided proposed elevation changes addressing the roof pitches, palette color, and downspouts.

Figure 5: Proposed design changes in response to DRB's 7-25-23 comments

PROPOSED DESIGN REVISIONS PER DRB COMMENTS 7/25/23 (EAST ELEVATION)

As indicated above, the Planning Commission needs to decide if all the topics listed by the DRB need to be resolved prior to Planning Commission action or if those can be left to the DRB to handle at the Final Design Review stage.

There is adequate infrastructure adjacent to the site to support the Project and the site is adequate in size and shape to encompass residential units plus a small commercial office space. The tenant for the commercial office space hasn't been identified yet; some examples of allowed uses in the OI zone include Business Services and/or Professional Services. Further, the proposed use is consistent with the land use designation and zoning. The Project will include landscaping with numerous trees, shrubs, and groundcover. The visual aesthetics of the area will be enhanced with the proposed landscaping. The proposed building and parking lot landscaping are integrated together as well as outdoor amenities for the residents.

The proposed lighting is dark sky compliant and consistent with Title 17 lighting requirements as the lights are downward-facing and energy efficient. Further, the proposed parking lot lighting will not trespass across property lines.

<u>Inclusionary Housing</u>

The City's Inclusionary Housing program requires development projects with five or more housing units to provide 20% of the units as deed restricted for occupancy by certain income levels (2.5% Extremely Low, 2.5% Very Low, 5% Low, 5% Moderate, and 5% Above Moderate up to 200% of area median income).This project will provide a total of 14 units, but three (3) of the units are exempt from the requirements of the City's Inclusionary Housing chapter, per 17.28.020(B)(8), as they are units "above the allowed density of a Zone District granted through a Density Bonus Agreement pursuant to Chapter 17.27, Density Bonus and Other Incentives." To comply with the requirements of the Inclusionary Housing program, the applicant would need to provide 0.275 Extremely Low income units, 0.275 Very Low income units, 0.55 Low income units, 0.55 Moderate income units, and 0.55 Workforce income units. This requirement could be met by either: building one (1) unit at each income level, or combining the fractional units of each income category to build the whole number of units at the Low-Income level (2 units) and pay the

fractional remainder (0.2) as an Affordable Housing In-Lieu Fee for Low-Income Rental Units. The applicant has selected the latter option. The calculation for the Residential Affordable Housing In-Lieu Fee is in the Conditions of Approval for the Project (Att. 1 Ex. D).

Traffic

In compliance with the California Environmental Quality Act, the project was analyzed using the vehicle miles traveled (VMT) metric. If the project's VMT exceeded an applicable threshold of significance, then either additional analysis or mitigations would be needed. The Project would not result in impacts related to traffic based on the Traffic and Vehicle Miles Traveled Analysis Study prepared by Associated Transportation Engineers, dated June 20, 2022, and accepted by the City of Goleta Public Works Department. Neither the work commute component nor the residential component exceeded the minimum thresholds.

In addition, the Traffic Study identified that the Project would add 11 PM peak hour trips (7 for the dwellings and 4 for Retail Commercial use). It is noted that the proposed project would result in fewer PM peak hour trips than compared to 6,000 square feet of office space that could have been proposed for the site. A 6,000-square foot office building at this location is estimated to generate approximately 13 PM trips.

The Project is consistent with the Transportation Element of the General Plan as the proposal will not degrade principal arterial, collector street, or intersection level LOS in the Project's vicinity according to the Traffic Study (Associated Transportation Engineers June 20, 2022, and Site Distance Analysis September 6, 2022) prepared for the Project. An adequate number of parking spaces are provided based on State Density Bonus law for the residential component and for the commercial component based on Title 17 standards. The Project meets short and long-term bicycle parking requirements.

Additionally, a sight distance analysis for the Project was prepared by Associated Transportation Engineers, dated September 6, 2022, stating that the Project meets the sight distance requirements. Ingress/egress into the Project site would be provided via an existing driveway connection to Calle Real located across the street from the Community Assembly use. The driveway would not be lane-restricted, meaning that people exiting the driveway may turn right or left out of the Project site. Lastly, the City has adopted the Goleta Transportation Improvement Program (GTIP) to fund transportation improvements in the City that are needed to accommodate future development. The Project will be required to pay traffic impact fees (GTIP) to offset its contribution to cumulative traffic growth within the City.

Archaeological Resources

A Phase 1 Archaeological Study has been prepared for the Project by Padre Associates, Inc., dated June 2022 and revised May 2023. The report concludes that the proposed Project will not affect cultural resources and recommends cultural monitoring of Project-related ground disturbance. This monitoring is incorporated as a condition of approval.

The City had the Padre report peer-reviewed by Chantal Cagle, MA, RPA with Applied EarthWorks Inc. In May 2023, Ms. Cagle, Principal Archaeologist, concurred with the findings and conclusion in the Padre report. While a formal Native American consultation is not required as the project has been found to be exempt from CEQA, the project archaeologist conducted outreach with local Chumash tribal representatives as part of the Phase 1 Archaeological Study and the report reflects the input from the Native American groups that participated in the applicant's outreach efforts.

Water

A Preliminary Water Service Determination letter from Goleta Water District, dated March 2, 2022, concludes that the District currently has sufficient water supply available to serve the proposed Project. Therefore, sufficient water service is available for the proposed development.

General Plan Consistency and Zoning Consistency

The Project will provide housing, which is in high demand throughout the State and here locally. It complies with all adopted zoning standards, over which the City has purview.

Further, the Project is consistent with the applicable design policies based on the compatibility of the architectural design, siting, and landscaping of the Project with adjacent development relative to size, bulk, and scale, as recommended by the DRB.

In addition, adequate infrastructure and services are available to serve the Project, including fire protection services, waste disposal, and sanitary services consistent with the General Plan. The Project would not result in any significant effects on recreational facilities nor create any substantial new demand for public amenities.

The Project is located along a scenic corridor and is consistent with the Visual and Historic Resources Element. As previously noted, all lighting will be dark-sky compliant and downward-facing and shielded. No specimen trees exist on site and therefore none are proposed to be removed. The landscaping palette avoids invasive plants and utilizes native and low-water vegetation with a range and appropriate density of plantings, including groundcovers, shrubs, and trees with varying heights and species.

The Project is consistent with the City's adopted 2023-2031 Housing Element. The Project location is identified as site V16 in Table 10A-28 of the Housing Element Technical appendix in the City's adopted 2023-2031 Housing Element. In addition, the site is categorized in the Housing Element at a density of 20 dwelling units per acre, which would yield a maximum of 11 units for the site. This Project is utilizing SDBL and proposes to construct 14 dwelling units, which would be a density of 26.4 units per acre or a 30% density increase. Based on the City's inclusionary requirements, the project is required to provide 2.8 units as affordable units. To satisfy this requirement, 2 of the 14 units will be deed-restricted low-income units and the Applicant will make an in-lieu payment for the remaining fractional requirement.

Conclusion

Staff finds that all the Project components meet the findings and criteria to support adoption of the Conditional Use Permit and approval of the Development Plan for the reasons outlined above and stated in the Resolution. The findings of fact to support approval are outlined in Sections 1-5 of the Resolution provided as Attachment 1.

CALIFORNIA ENVIRONMENTAL QUALITY ACT

City staff prepared a Notice of Exemption: *Infill Development Projects §15332* consistent with CEQA requirements. A §15332 Categorical Exemption consists of projects characterized as in-fill development where the project: (a) is consistent with the applicable General Plan designation and all applicable General Plan policies, as well as with applicable zoning designation and regulations; (b) occurs within City limits on a project site of no more than five acres substantially surrounded by urban uses; (c) has no value as habitat for endangered, rare or threatened species; (d) would not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) is adequately served by all required utilities and public services.

The property is in an urbanized area with a Zoning and General Plan Land Use designation of Office Institutional (OI) and will not result in any changes to the allowable land uses. The proposed use is consistent with the land use designations of the site and has been designed to meet the applicable standards. Additionally, no variances or exceptions are required, and all services and access to the parcel are sufficient and will comply with local standards. The Project is proposed to occur on a parcel that is 0.53 acres, which is less than 5 acres in size. Moreover, the Project site is located on a vacant 0.53-acre lot within the City of Goleta limits and it is substantially surrounded by urban uses. The Project site is located south of Lake Los Carneros Natural and Historical Preserve (Lake Los Carneros) with commercial uses immediately to the southeast, a Community Assembly use to the north across Calle Real, and agricultural use to the west across Los Carneros Road.

The development area does not contain special status species and has no potential value as a habitat area for endangered, rare, or threatened species.

The Project would not result in any significant impacts related to traffic based on the Traffic and Vehicle Miles Traveled Analysis prepared by Associated Transportation Engineers, dated June 20, 2022, and accepted by the City of Goleta Public Works Department.

Moreover, none of the State CEQA Guidelines Section 15300.2's exceptions to the Class 32 exemption apply to the project. The exception set forth in State CEQA Guidelines Section 15300.2(a), by its own terms, does not apply to projects that fall within the Class 32 exemption; moreover, the project will not impact an environmental resource of hazardous or critical concern that is designated, precisely mapped, or officially adopted pursuant to law by federal, state, or local agencies. Section 15300.2(b)'s exception, relating to cumulative impacts, does not apply as technical reports in the record of

proceedings demonstrate that the project would not result in significant cumulative impacts; moreover, there are no other successive projects of the same type in the same place that could result in significant cumulative impacts. Section 15300.2(c)'s exception does not apply because there are no "unusual circumstances" that apply to the project; construction of a mixed-use project as urban infill on commercially zoned land next to existing development does not constitute an unusual circumstance. Section 15300.2(d)'s exception does not apply because the project is not located near any scenic highways. Section 15300.2(e)'s exception does not apply because the project site and off-site improvement locations do not contain hazardous waste and are not on any list compiled pursuant to Section 65962.5 of the Government Code. Finally, Section 15300.2(f)'s exception does not apply because the project has no potential of causing a substantial adverse change in the significance of a historical resource. Additionally, the Project's site does not contain any identified significant cultural resources and will be conditioned to include all mandatory grading best practices.

For all the foregoing reasons, the entirety of the Project falls within the Class 32 Exemption set forth in State CEQA Guidelines Section 15332. The Categorical Exemption is included as Exhibit A of Attachment 1.

PUBLIC NOTICE

On August 31, 2023, the Planning Commission hearing notice was published in the Santa Barbara Independent and mailed to property owners and tenants within 500 feet of the site. The Project site was also posted with signage on July 7, 2023 for the DRB meeting, and updated on August 17, 2023, for the Planning Commission meeting. Story poles were installed on April 7, 2023.

APPEALS PROCEDURE

The Planning Commission is the decision maker on this project. The action of the Planning Commission may be appealed by an applicant or an aggrieved party to the City Council pursuant to Goleta Municipal Code Section 17.52.120. Appeals must be filed and associated fees must be paid within 10 calendar days of the appealable decision.

Legal Review By:

Winnie Cai
Assistant City Attorney

Approved By:

Peter Imhof
Planning and Environmental Review Director

ATTACHMENTS:

1. A Resolution of the Planning Commission of the City of Goleta, California, "A Resolution of the Planning Commission of the City of Goleta, California, Approving the Conditional Use Permit and Development Plan for a Mixed-Use Project to Allow 14 Residential Units and Commercial Space for a total of 14,257 Square Feet and associated Site Improvements to be located at 6491 Calle Real and Finding the Project to be Categorically Exempt from the California Environmental Quality Act under State CEQA Guidelines Section 15332; APN 077-160-066; Case Nos. 22-0005-DP, 22-0001-CUP, 23-0028-DRB"
   Exhibit A – Notice of Exemption
   Exhibit B – General Plan Consistency
   Exhibit C – Zoning Consistency
   Exhibit D – Conditions of Approval (Conditional Use Permit and Development Plan)

2. Project Plans
3. Traffic and VMT Analysis
4. Sight Distance Analysis
5. Staff Presentation

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Attachment 1

PLANNING COMMISSION RESOLUTION NO. 23-___, ENTITLED A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF GOLETA, CALIFORNIA, APPROVING THE CONDITIONAL USE PERMIT AND DEVELOPMENT PLAN FOR A MIXED-USE PROJECT TO ALLOW 14 RESIDENTIAL UNITS AND COMMERCIAL SPACE FOR A TOTAL OF 14,257 SQUARE FEET AND ASSOCIATED SITE IMPROVEMENTS TO BE LOCATED AT 6491 CALLE REAL AND FINDING THE PROJECT TO BE CATEGORICALLY EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT UNDER STATE CEQA GUIDELINES SECTION 15332; APN 077-160-066; CASE NOS. 22-0005-DP, 22-0001-CUP, 23-0028-DRB

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RESOLUTION NO. 23-___

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF GOLETA, CALIFORNIA, APPROVING THE CONDITIONAL USE PERMIT AND DEVELOPMENT PLAN FOR A MIXED-USE PROJECT TO ALLOW 14 RESIDENTIAL UNITS AND COMMERCIAL SPACE FOR A TOTAL OF 14,257 SQUARE FEET AND ASSOCIATED SITE IMPROVEMENTS TO BE LOCATED AT 6491 CALLE REAL AND FINDING THE PROJECT TO BE CATEGORICALLY EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT UNDER STATE CEQA GUIDELINES SECTION 15332; APN 077-160-066; CASE NOS. 22-0005-DP, 22-0001-CUP, 23-0028-DRB

WHEREAS the property is located at 6491 Calle Real in the Inland area of the City of Goleta and has an Office Institutional (OI) General Plan/Coastal Land Use Plan Land Use and Zoning designation; and

WHEREAS, on May 5, 2022, RRM Design Group submitted a request for a Preliminary Housing Development Application (SB 330) and on May 18, 2022, the Preliminary Housing Development Application was deemed complete; and

WHEREAS, on June 21, 2022, RRM Design Group submitted an application for a Conditional Use Permit (22-0001-CUP) and Development Plan (22-0005-DP) to allow the development of a 14-dwelling-unit and commercial space mixed-use project and associated site improvements ("Project") which was eventually deemed complete after a couple of rounds of completeness review on January 13, 2023; and

WHEREAS it has been determined that the proposed environmental impacts of the Project meet the criteria for use of a Categorical Exemption, specifically Section 15332 of CEQA Guidelines, and a Notice of Exemption is recommended for adoption. The NOE has been available for public review since July 20, 2023, when the Design Review Board Agenda was posted; and

WHEREAS, on July 25, 2023, the Design Review Board held a duly noticed public hearing to review the design elements of the Project, at which time all interested persons were given an opportunity to be heard; and

WHEREAS, on July 25, 2023, the Design Review Board recommended Preliminary Approval of the Project with conditions to the Planning Commission; and

WHEREAS, on September 11, 2023, the Planning Commission held a duly noticed public hearing at which time all interested persons were given an opportunity to be heard;

NOW, THEREFORE, BE IT RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF GOLETA, THE PLANNING COMMISSION HEREBY TAKES THE FOLLOWING ACTIONS:

SECTION 1. *Environmental Assessment for the Project.* The Planning Commission makes the following environmental findings:

A. The City has determined that the Project is exempt from further environmental review in accordance with California Environmental Quality Act Guidelines pursuant to Title 14 California Code of Regulations. Specifically, the Project is exempt based on *§15332 (Infill Development Projects).* The Project is exempt due to its consistency with the existing General Plan Land Use designation and Zoning designation applicable to the property, its location is surrounded by substantially developed sites to the north, south, west, and east. The existing parcel will be served by all required utilities and public services and roadways. The new development conforms to the policies of the City of Goleta General Plan and the regulations of Title 17, the Goleta Zoning Ordinance, within the City of Goleta Municipal Code. The Project site has no potential value as a habitat area for endangered, rare, or threatened species, given that the site has been previously graded and is situated adjacent to similar developments within the urban context. Further, none of the exceptions to the exemptions are applicable to the Project or site. The specific analysis relating to why the Project is categorically exempt from CEQA is outlined in the Notice of Exemption provided as Exhibit A to this Resolution.

B. The Planning Commission reviewed the record of the proceedings and considered all information presented at or before all public hearings.

C. The Categorical Exemption reflects the City's independent judgment and analysis. The Planning Commission has independently reviewed and analyzed the Categorical Exemption prepared for the Project. The Categorical Exemption was prepared under the direction of the City of Goleta Planning and Environmental Review Department and reflects its independent judgment and analysis.

SECTION 2. The Planning Commission makes the following findings pursuant to Section 17.52.070 (Common Findings), Section 17.57.050 (Conditional Use Permit Findings), 17.59.030 (Development Plan Findings), and 17.58.080 (Design Review) relating to the Project:

Common Findings

A. There are adequate infrastructure and public services available to serve the proposed development, including water and sewer service, existing or

planned transportation facilities, fire and police protection, schools, parks, and legal access to the lot.

There are adequate public services to serve the property as Goleta Water District, Goleta West Sanitary District, and Santa Barbara County Fire Department have all reviewed the Project and provided preliminary service letters verifying that services are available. Further, the parcel has an existing curb cut onto Calle Real which will be used for ingress/egress for the site, and the adjacent roadways have sufficient capacity to accommodate the anticipated amount of traffic to be generated by this Project as documented in the Traffic, Circulation and VMT Study prepared by Associated Transportation Engineers dated September 6, 2022. This Project is small, so the increase in usage of schools and parks due to the Project will be minimal.

B. The proposed project conforms to the applicable regulations of this Title and any zoning violation enforcement on the subject premises has been resolved as permitted by law.

As demonstrated in the Zoning Consistency Analysis which is attached to this Resolution as Exhibit C, the Project conforms to the applicable regulations of the Goleta Municipal Code. Further, there are no active code compliance cases on the Project site.

C. The proposed development is located on a legally created lot.

The proposed development is located on a legally created lot. It is identified in the City of Goleta, County of Santa Barbara, State of California, according to the map thereof recorded in Book 77, at Page 16, inclusive, of maps in the office of the County recorder of Santa Barbara County, APN 077-160-066.

D. The development is within the project description of an adopted or certified CEQA document or is statutorily or categorically exempt from CEQA.

City staff prepared a Notice of Exemption: Infill Development Projects §15332 consistent with CEQA requirements as provided in Exhibit A to this Resolution. A §15332 Categorical Exemption consists of Projects characterized as in-fill development where the Project: (a) is consistent with the applicable General Plan designation and all applicable General Plan policies, as well as with applicable zoning designation and regulations; (b) occurs within city limits on a Project site of no more than five acres substantially surrounded by urban uses; (c) has no value, as habitat for endangered, rare or threatened species; (d) would not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) is adequately served by all required utilities and public services.

Please refer to Section 1 above for additional information and the Notice of Exemption provided as Exhibit 1 to this Resolution.

SECTION 3. The Planning Commission makes the following findings pursuant to Section 17.57.050 (Conditional Use Permit Findings) relating to the Project:

A. The use as proposed is consistent with the General Plan.

As outlined in Exhibit B to this Resolution, the proposed Conditional Use is consistent with the guiding principles and goals of each of the chapters of the General Plan/Coastal Land Use Plan. The proposal is in compliance with the Land Use Plan Map (LU Element, Figure 2-1) and is consistent with the development standards specified for this designation. The proposed site improvements do not conflict with the land use designation or the zoning ordinance designation of Office Institutional Commercial (OI). Further, the Project design, including landscaping, is appropriate and compatible with the neighborhood. Adequate infrastructure and services are available to serve the Project in keeping with the provisions of the Land Use and the Public Facility Elements.

Further, the Project is consistent with the policies of the Conservation Element (CE 15.3 – Water Conservation and CE 10.1 – 10.3, 10.6 and 10.8-10.9 Watershed Management and Water Quality) as the Project proposes complies with all Stormwater requirements. The proposed landscaping is drought tolerant and the irrigation design complies with local and state conservation requirements.

In addition, the Project is consistent with Transportation Element policies (TE 13.3 Maintenance of LOS Standards, TE 10.4 Pedestrian Facilities in New Development) as the Traffic Study prepared for the Project demonstrated that the Project will not significantly impact LOS levels for roadways or intersections.

The Project location is identified as site V16 in Table 10A-28 of the Housing Element Technical appendix in the City's adopted 2023-2031 and consistent with Housing Element policy HE2.1d encourages well-designed Mixed-use residential/nonresidential developments are encouraged by the City at locations where appropriate, including but not limited to areas designated as Office and Institutional on the Land Use Plan map. The City will continue to encourage Mixed-use development in commercial areas. This Project complies with Housing Element policy HE2.3 (a-h) Housing Design Principles for Multiple-'Unit and Affordable Housing. Inclusionary Housing HE 2.5 is met on the Project by providing 20% affordable units plus an in-lieu fee for fractional unit.

B. The use will not be more injurious to the health, safety, and general welfare of the surrounding neighborhood due to noise, dust, smoke, or vibration than from uses allowed in the district.

The multi-unit residential use and relatively small commercial office space is a less intensive use category as compared to other uses allowed under Office Institutional (OI). Any noise or vibration generated by the proposed use would be similar to or less than uses typically allowed in the OI zone district such as private schools, government buildings, and animal keeping.

Further, standard best management practices for dust control and limitations on idling vehicles and the use of generators will be included as conditions of approval to ensure that the health, safety, and general welfare of the surrounding neighborhood is protected.

SECTION 4. The Planning Commission makes the following findings pursuant to Section 17.59.030 (Development Plan Findings) relating to the Project:

A. The project as proposed is consistent with the General Plan.

The proposed Project is consistent with the purpose and intent of the Office Institutional designation in the General Plan. Mixed-Use Projects are an allowed use in the OI Zone with approval of a Conditional Use Permit. As noted in Exhibit B to this Resolution (General Plan Consistency analysis) and Finding B in Section 3 above, the Project is consistent with all the applicable General Plan policies pertaining to construction and use of the development.

B. The site for the project is adequate in size, shape, location, and physical characteristics to accommodate the density and intensity of development proposed.

The Project site is 23,019 square feet (0.53) acres in size, relatively flat, currently vacant, and it has frontage on both Calle Real and Los Carneros Road. The site is located southwest of Lake Los Carneros Natural and Historical Preserve (Lake Los Carneros) with commercial uses immediately to the south and east, a Community Assembly to the north across Calle Real, and agricultural use to the west across Los Carneros Road. While residential uses are not located directly adjacent to the site, there are/will be residential complexes (Village at Los Carneros and approved Heritage Ridge) located approximately 0.25 miles to the south

across the U.S. 101 freeway and about 0.33 miles to the north on Covington Way. Thus, the Project site is surrounded on multiple sides by existing development and as such, is considered an in-fill parcel. The proposed building footprint is 5,048 square feet, which results in a lot coverage of 21%. This proposed lot coverage is well below the Office Institutional (OI) lot building coverage maximum of 40%. The layout of the Project fits well within the shape of the site: the building, parking, amenities, etc. fit well at their locations, and their usage with the expected pedestrian and automobile traffic appears to be very reasonable. In summary, the site is adequate in size, shape, location, and physical characteristics to accommodate the density and intensity of the proposed development.

C. Any significant environmental impacts are mitigated to the maximum extent feasible.

As indicated in Section 1 above, the Project qualifies for a Class 32 In-fill Exemption under Section 15332 of State CEQA Guidelines. No mitigation measures are needed given the nature of the Project.

D. The project will not conflict with any easements required for public access through, or public use of a portion of the property.

The development of the Project will not conflict with the existing easements that are present on the site. Further, there are no easements that encumber the area to be developed. The easements that exist on site include the City of Goleta and public utilities.

SECTION 5. The Planning Commission makes the following findings pursuant to Section 17.58.080 (Design Review) relating to the Project:

A. The development will be compatible with the neighborhood, and its size, bulk and scale will be appropriate to the site and the neighborhood.

The proposed Project is in a varied-use neighborhood with a 35-foot-high Community Assembly use across Calle Real to the north of the Project, approximately 20-foot-high commercial buildings to the east and south of the Project, and agricultural use across Los Carneros Road to the west of the Project. The proposed mixed-use Project's commercial use is clearly compatible with the existing neighborhood commercial use, and the proposed Project's residential use is acceptable considering that the neighborhood's existing uses are already so varied. Regarding size, bulk, and scale, the proposed Project's height is compatible with the heights of the nearby buildings, and the proposed Project's footprint is also similar to the footprints of the nearby buildings. The proposed Project is located along a Local Scenic Corridor on Figure 6-1 of the General Plan, and the

proposed photo simulations demonstrate that the view to the mountains will alter minimally. In summary, the development will be compatible with the neighborhood, and its size, bulk, and scale will be appropriate to the site and the neighborhood.

B. Site layout, orientation, and location of structures, including any signage and circulation, are in an appropriate and harmonious relationship to one another and the property.

The proposed development's parking and building placements are designed to be consistent with the adjacent traffic circle at the intersection of Calle Real and Los Carneros Road. The property's existing curb cut is used and was constructed by the City when the Calle Real/Los Carneros roundabout was constructed. Further, the applicant has provided a Traffic Study dated June 20, 2022, by Associated Transportation Engineers and a Sight Distance Analysis dated September 6, 2022, by Associated Transportation Engineers which demonstrate that the Project's placement will not affect intersection visibility or the functionality of the roundabout. Additionally, the parking and the building are harmonious with each other, being designed to minimize as much as possible the distances that occupants (or visitors) have to walk from parking places to the building. Thus, these elements of the proposed development are appropriate and harmonious.

Signage will be proposed and reviewed at a later time.

C. The development demonstrates a harmonious relationship with existing adjoining development, avoiding both excessive variety as well as monotonous repetition, but allowing similarity of style, if warranted.

The proposed development is compatible with the existing developments surrounding the site, respecting the existing adjoining developments' architectural styles. Since the proposed development is not identical to the buildings in the existing commercial center nor the Community Assembly to the north, the architectural style of the proposed development avoids monotonous repetition.

D. There is harmony of material, color, and composition on all sides of structures.

The materials and colors for this Project are proposed to match and complement each other as the whole of the Project was considered in the exterior design including roof, windows, doors, exterior finishes, and site improvements. All sides of the structure have been considered and addressed in the design of the proposed development.

E. Any outdoor mechanical or electrical equipment is well integrated in the total design and is screened from public view to the maximum extent practicable.

The outdoor mechanical and electrical equipment is screened from public view. The proposed trash enclosure is architecturally consistent with the design of the building. The transformer is integrated into the landscape design. It is quite explicitly well-integrated into the total design.

F. The site grading is minimized, and the finished topography will be appropriate for the site.

The finished topography will be the same as it is currently, and the grading will be the minimum required to construct the Project. Approximately 140 cubic yards of grading is proposed with 130 cubic yards of fill, 800 cubic yards of overexcavation, and 1,500 cubic yards of recompaction.

G. Adequate landscaping is provided in proportion to the project and the site with due regard to preservation of specimen and protected trees, and existing native vegetation.

The proposed development contains an appropriate amount of landscaping around most of the edges of the proposed development as well as in the parking lot's finger planters between parking spaces. There is no significant existing native vegetation, in particular no trees, so preservation of such is unnecessary.

H. The selection of plant materials is appropriate to the project and its environment, and adequate provisions have been made for long-term maintenance of the plant materials.

The plant palette comprises plant material and trees known to thrive in the local climate and soil conditions. Twenty percent (20%) or less of the plant material will require moderate water, and the remainder will require low to very low water once established. The plant palette coupled with the proposed irrigation system have been designed to meet or exceed the state and local standards for water conservation through water-efficient landscape irrigation design. Low-water shrubs and ground covers comprise a majority of the total landscaped area. Further, the project has been designed to meet the Water Efficient Landscape Ordinance (WELO). The irrigation design will comply with local and state requirements.

I. All exterior lighting, including for signage, is well designed, appropriate in size and location, and dark-sky compliant.

Exterior lighting is designed for the safety of the occupants and visitors. The lights are LEDs and meet the City's dark sky lighting standards. The exterior building lighting meets code requirements at building entrances and exits, and it will be dark sky compliant and angled downward. The photometric grid showing foot-candle readings every 10 feet indicates that the lighting is compliant with the Light Trespass requirement in that lighting does not exceed 0.1 foot-candles. The Project complies with the City's Lighting Ordinance.

No signage is proposed with this Project and will be considered at a later date.

J. The project architecture will respect the privacy of neighbors, is considerate of private views, and is protective of solar access off site.

The proposed Project is in a varied-use neighborhood with a Community Assembly across Calle Real to the north of the Project, agricultural use across Los Carneros Road to the west of the Project, and commercial buildings to the east and south of the Project. Other than a caretaker unit at the Community Assembly, there are no other residential uses in this area, and privacy of the neighbors has been considered with the bulk of the Project's windows, balconies, and outdoor living areas facing the Project's own parking lot, the public street, or the commercial driveway to the south of the Project. The proposed development's building is approximately 130 feet south of the Community Assembly, 100 feet west of the commercial building on the east, and 40 feet from the commercial building on the south. As such, the proposed development is considerate of privacy and private views and protective of solar access off-site, and it will not affect solar access.

K. The proposed development is consistent with any additional design standards as expressly adopted by the City Council. (Ord. 20-03 § 6).

There are no adopted mixed-use design standards adopted that apply to this Project.

SECTION 6. Based on the findings outlined in Sections 1-5 above, the Planning Commission hereby takes the following actions:

A. Find that the Project is Exempt under the California Environmental Quality Act as described in the Notice of Exemption provided as Exhibit A to this Resolution.
B. Approve the Conditional Use Permit and Development Plan based on the General Plan and Zoning Consistency analysis provided as Exhibits B and C respectively to this Resolution subject to the conditions of approval provided as Exhibit D, and grant Preliminary Design Approval.
C. Direct staff to have the Notice of Exemption filed within five (5) business days.

SECTION 7. Reliance on Record

Each and every one of the findings and determinations in this Resolution are based on the competent and substantial evidence, both oral and written, contained in the entire record relating to the Project. The findings and determinations constitute the independent findings and determinations of the Planning Commission in all respects and are fully and completely supported by substantial evidence in the record as a whole.

SECTION 8. Limitations

The Planning Commission analysis and evaluation of the Project, including this Resolution, are based on the entire record, including the best information currently available. This includes competent and substantial evidence, both oral and written. It is inevitable in evaluating a Project that absolute and perfect knowledge of all possible aspects of the Project will not exist. One of the major limitations on analysis of the Project is the Planning Commission's lack of knowledge of future events. In all instances, best efforts have been made to form accurate assumptions. Somewhat related to this situation are the limitations on the City's ability to solve what are in effect regional, state, and national problems and issues. The City must work within the political framework within which it exists and with the limitations inherent in that framework.

SECTION 9. Summaries of Information

All summaries of information in the findings, which precede this section, are based on the substantial evidence in the record. The absence of any particular fact from any such summary is not an indication that a particular finding is not based in part on that fact.

SECTION 10. Duration of the Resolution
This Resolution will remain effective until superseded by a subsequent Resolution.

SECTION 11. Copies of the Resolution
The City Clerk is directed to mail a copy of this Resolution to 6491 Calle Real Partner, LLC, 1933 Cliff Drive Suite 2, Santa Barbara, CA 93109-1585; Tony Tomasello, Principal Planner, RRM Design Group, 10 East Figueroa Street, Suite 200, Santa Barbara, CA 93101; and to any other person requesting a copy.

SECTION 12. Effective date of the Resolution
This Resolution will become effective immediately after adoption.

SECTION 13. Certification of the Resolution
The City Clerk shall certify the passage and adoption of this resolution and enter it into the book of original resolutions.

PASSED, APPROVED AND ADOPTED this ____ day of _____, 2023.

_____
JENNIFER SMITH
PLANNING COMMISSION CHAIR

ATTEST:

_____
DEBORAH LOPEZ
CITY CLERK

APPROVED AS TO FORM:

_____
WINNIE CAI
ASSISTANT CITY ATTORNEY

)
COUNTY OF SANTA BARBARA) ss.
CITY OF GOLETA)

I, DEBORAH LOPEZ, City Clerk of the City of Goleta, California, DO HEREBY CERTIFY that the foregoing Resolution No. 23- __ was duly adopted by the Planning Commission of the City of Goleta at a regular meeting held on the ___ day of _____, 2023 by the following vote of the Planning Commission:

AYES:

NOES:

ABSENT:

(SEAL)

_____
DEBORAH LOPEZ
CITY CLERK

Exhibit A: Notice of Exemption

Exhibit B: General Plan Consistency Analysis

Exhibit C: Zoning Consistency Analysis

Exhibit D: Conditional Use Permit and Development Plan COA

Attachment 1

Exhibit A

Notice of Exemption

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NOTICE OF EXEMPTION (NOE)

To: ☐ Office of Planning and Research
P.O. Box 3044, 1400 Tenth St. Rm. 212
Sacramento, CA 95812-3044

X Clerk of the Board of Supervisors
County of Santa Barbara
105 E. Anapamu Street, Room 407
Santa Barbara, CA 93101

From: City of Goleta
130 Cremona Drive, Suite B
Goleta, CA 93117

Subject: Filing of Notice of Exemption

Project Title:

Calle Real Mixed-Use Project; Case Nos. 22-0005-DP; 22-0001-CUP; 23-0028-DRB.

Project Applicant:

RRM Design Group, for 6491 Calle Real Partners, LLC.

Project Location (Addresses and APN):

6491 Calle Real, Goleta CA, 93117, County of Santa Barbara, APN: 077-160-066.

Description of Nature, Purpose, and Beneficiaries of Project:

The proposed Housing Accountability Act housing development project for a new three-story, mixed-use building on a vacant 0.53-acre lot. The building would total 14,257 square-feet, comprising 1) One commercial office space of 585 square feet plus; 2) 14 one-bedroom residential units totaling 11,520 square feet; and 3) 2,152 square feet of common areas dedicated to residential use. This project is using State density bonus law (SDBL) to increase the density, use SDBL parking requirements, receive a concession from the City's parking lot heat island requirements, and a height waiver to increase the building height by 2.5 feet.

The 14 residential units would range in size from 694 to 979 square feet. Each residential unit will have a private patio or balcony associated with the unit. An approximately 585 square-foot patio would be shared between the commercial and residential tenants and 3,454 square feet of outdoor area would be shared by residents only. Parking includes 20 uncovered automobile parking spaces plus 10 short-term and 16 long-term bicycle parking spaces. The project includes a trash enclosure and other site amenities. The proposed project will be all electric and no gas connection is proposed. The lot is located on the southeast

corner of the Calle Real/Los Carneros roundabout and is zoned Office Institutional (OI).

The entitlements required for this project are Design Review, and a Conditional Use Permit and Development Plan with the Planning Commission serving as the Review Authority.

The Beneficiary of the Project is 6491 Calle Real Partners, LLC., property owner.

Name of Public Agency Approving the Project (Lead Agency):

City of Goleta

Name of Person or Agency Carrying Out the Project:

RRM Design Group

Exempt Status:

X Categorical Exemption: *In-Fill Development Project, State CEQA Guidelines, §15332*

Reason(s) why the project is exempt:

Categorical Exemption: *In-Fill Development Projects §15332*

Class 32 consists of projects characterized as in-fill development where the project is: (a) consistent with the applicable general plan designation and all applicable general plan policies, as well as with applicable zoning designation and regulations; (b) occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses; (c) has no value as habitat for endangered, rare, or threatened species; (d) would not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) is adequately served by all required utilities and public services.

Consistent with the requirements of the Class 32 requirements above, and pursuant to CEQA Guidelines *§15300.2, Exceptions to the Exemption,* the entirety of the project falls within the Class 32 Exemption set forth in State CEQA Guidelines section 15332.

General Plan and Zoning Consistency

The project would conform to the policies within the City of Goleta General Plan, and the regulations of Title 17, the City of Goleta Zoning Ordinance, within the City of Goleta Municipal Code. The property is in an urbanized area with Office Institutional (OI) General Plan and zoning designations, and the uses of multi-unit residential in a mixed-use development is allowed with a Major Conditional Use

Permit by Title 17. The project has been designed to meet the applicable zoning standards. Additionally, the only variances or exceptions are: [i] increase in the building height that is allowed with a Waiver through SDBL; [ii] concession from complying with the City's parking lot Heat Island standards; and [iii] using the parking standards of the SDBL. All services and access to the parcel are sufficient and would comply with local standards.

Further, the Project location is identified as site V16 in Table 10A-28 in the City's adopted 2023-2031 Housing Element. In addition, the site is identified in the Housing Element at a density of 20 dwelling units per acre which would allow a maximum of 11 units. This Project is utilizing SDBL and proposed to construct 14 dwelling units at a density of 26.4 units per acre. Based on the City's inclusionary requirements, the project is required to provide 2.8 units as affordable units. To satisfy this requirement, of the 14 units, 2 of the units will be deed-restricted low-income units and the Applicant will make an in-lieu payment for the remaining fractional requirement.

Infill development on vacant and underutilized land constitutes the most sustainable means to accommodate growth and to redesign cities to be more sustainable. Streamlining this development in the City of Goleta is key, and doing so presents an opportunity for the City to align its housing policies with its sustainability goals while providing more critical housing for its residents.

Project occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.

The Project site is located on a vacant 0.53-acre lot within the City of Goleta limits, and it is substantially surrounded by urban uses. The Project site is located south of Lake Los Carneros Natural and Historical Preserve (Lake Los Carneros) with commercial uses immediately to the southeast, an Islamic Center to the north across Calle Real, and agricultural use to the west across Los Carneros Road.

Biological Resources

Per Figure 4-1, *Special-Status Species and Environmental Sensitive Habitat Areas (ESHA)* of the City of Goleta General Plan, the site does not contain special-status species. Additionally, the project site has no potential value as a habitat area for endangered, rare, or threatened species. The closest Riparian ESHA to the project site is Lake Los Carneros which is approximately 125 feet northeast of the project site, across Calle Real. A Biological study has been prepared for the Project based on its proximity to ESHA. The report prepared by Padre Associates, Inc. dated May 2022 and revised August 2022 concludes that the project will not have an impact on habitat in the area or on wildlife based on the sufficient distance of the Project Site at no less than 100 feet from ESHA, separated by roadways, and at least 125 feet from any Streamside Protection

Areas, no direct impacts to ESHA would result from the Project. All work activities including staging of equipment and materials are proposed within the previously disturbed footprint of the Project Site, which is bordered on all sides by existing roadways or other urban development. No Restoration or Monitoring Plan for ESHA should be required for the Project. The report is incorporated by reference into this NOE and is on file with the PER Department

Visual Resources

The General Plan/Coastal Land Use Plan Visual and Historic Element Policy VH 1.1 designates the nearby Lake Los Carneros, the more distant foothills, and the Santa Ynez Mountains as Scenic Resources. Views of these resources are currently available from the site and public rights-of-way from both Los Carneros Road and Calle Real adjacent to the project site. From the public roadway and sidewalks adjacent to the site, a portion of the views to the Santa Ynez Mountains are obscured by existing tree canopies. No other public views of natural landforms or open space are seen from the vicinity of the project site.

The City's Visual and Historic Resources Element, Policy VH 2.1 identifies Los Carneros Road from Highway 101 to Cathedral Oaks as a Local Scenic Corridor. The proposed building would be located on the east side of the Los Carneros Road Local Scenic Corridor when traveling north as a pedestrian, by bicycle, or by car. Impacts to views northward along this Scenic Corridor would be considered less than significant due to the proposed building's location to the east of the view corridor.

The Applicant has provided a view study of the area and has installed story poles. These tools document that the project will not impact the nearby Scenic Resources and the project can be found consistent with the applicable policies as the development to the north is approximately the same height and the views to the mountains are maintained.

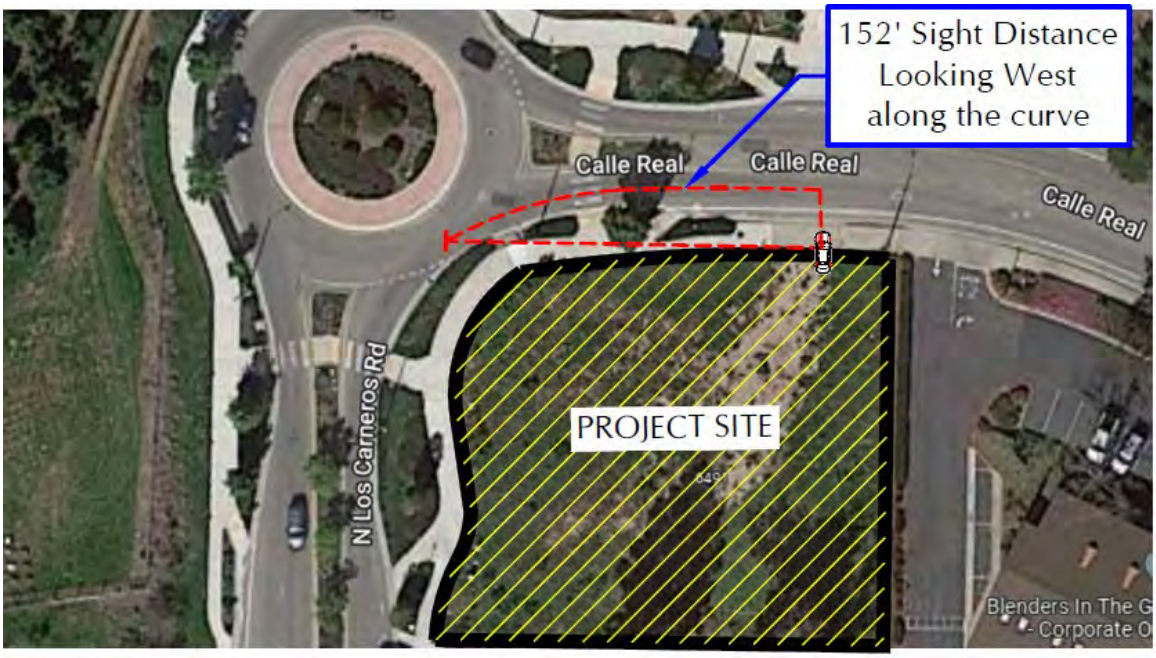
Cultural Resources

A Phase 1 Archaeological Study has been prepared for the Project by Padre Associates, Inc. dated June 2022 and revised May 2023. The report concludes that the proposed Project will not affect cultural resources and recommends cultural monitoring of Project related ground disturbance in response to concerns relayed by the Native Americans which will be incorporated as conditions of approval. In addition, the City had the Padre report peer reviewed by Chantal Cagle, MA, RPA with Applied EarthWorks Inc. Ms. Cagle peer reviewed the report and provided comments February 2023 and again in May 2023 in response to the revised report. Ms. Cagle, Principal Archaeologist, concurred with the findings and conclusion in the Padre report. The report is incorporated by reference into this NOE and is on file with the PER Department

Transportation

The Project would not result in impacts related to traffic based on the Traffic and Vehicle Miles Traveled Analysis Study prepared by Associated Transportation Engineers, dated June 20, 2022, and accepted by the City of Goleta Public Works Department. The City of Goleta has adopted a new set of transportation assessment guidelines, in compliance with Senate Bill 743, which are based on a Vehicle Miles Traveled (VMT) metric rather than the traditional Level of Service (LOS) metric. Per the State's Natural Resource Agency Updated Guidelines for the Implementation of the CEQA adopted in 2018, VMT has been designated as the most appropriate measure of transportation impacts. "Vehicle Miles Traveled" refers to the amount and distance of automobile travel attributable to a project. Other relevant considerations may include the effects of the project on transit and non-motorized travel. For land use projects, vehicle miles traveled exceeding an applicable threshold of significance may indicate a significant impact. The City of Goleta has developed the VMT Calculator ("sketch tool") to assess a Project's potential VMT based on the project's description, location, and attributes. For most residential and commercial development projects, the sketch tool is the approved method to calculate Project VMT. The City provides the sketch tool for use by transportation consultants, developers, and others in assessing VMT for development projects and evaluating TDM plans. The City sketch planning tool indicates that no VMT analysis is required for the Project. The Project trips related to the "Work Commute Component" are below the minimum threshold. The Project trips related to the "Residential Component" are below the minimum threshold. Additionally, a sight distance analysis for the Project was prepared by Associated Transportation Engineers, dated September 6, 2022, stating that the Project meets the sight distance requirements, and it was accepted by the City of Goleta Public Works Department. See the following images:

Site Distance Analysis Looking West:



Site Distance Analysis Looking East:

Further Site Distance Analysis Looking East

Islamic Society of Santa Barbara
N Los Carneros Rd
Calle Real
Calle Real
Calle Real
Calle Real
Calle Real
Calle Real
Keep Vegetation Below 3.5'
Blenders In The Grass Corporate Office
Brass & Silver Traditions
Temporarily closed
LOS CARNEROS BUSINESS Center
GizzMoVest LLC for GARMIN &...
N
NOT TO SCALE

Noise

The project would not result in impacts related to noise. The project site is split across the 60 dBA and 65 dBA noise levels from the US 101, as illustrated in the City's General Plan Figure 9-3: Future Noise Contours-Roadways (2030). As indicated in Figure 9-4: Future Noise Contours – Airport (2025) and Railroad (2030), the project site is not within one of the identified future noise levels associated with these two facilities. The City's Noise Element in Table 9-2: Noise and Land Use Compatibility Criteria demonstrates that a multifamily residential use is conditionally acceptable within the 60-65 dBA noise level range. Consistent with Policy NE 1.5, the new construction proposed at this site shall include appropriate sound attenuation measures, such as insulation, dual paned windows, glazing, etc., to reduce interior noise levels as a condition of construction. Additionally, a fence along N. Los Carneros Road and landscaping shall provide a buffer to roadway traffic, per Policy NE 7.2. The roundabout at the N. Los Carneros Road and Calle Real intersection provides an existing traffic calming measure to help reduce speeds and noise levels adjacent to the project site, pursuant to Policy NE 2.7. The project will be subject to Policies NE 6.4 and NE 6.5 which restrict construction hours and construction equipment operation/specifications.

The proposed project does not exceed standards established in the City's General Plan or noise regulations. The proposed project complies with the criteria and/or requirements outlined in Goleta Municipal Code 17.39.070 Noise.

With respect to noise increase from project related traffic, impacts would be significant if the additional traffic resulted in exposure of sensitive noise receptors to "unacceptable" noise. The Goleta General Plan identifies sensitive noise receptors as residential neighborhoods, schools, libraries, hospitals and rest homes, auditoriums, certain open space areas, and public assembly places. The sensitive noise receptor on site will be the proposed residences themselves, and the closest off-site sensitive noise receptor is the Islamic Society of Santa Barbara which is located directly across Calle Real. The proposed project will not impact the operation of this use as noise levels associated with the proposed project will remain below the 65 dBA level.

According to the 2023 Adopted Airport Land Use Compatibility Plan (ALUCP), the project site lies outside of the Santa Barbara Airport's noise exposure range, as indicated in Figure 4-1 of the ALUCP and is located in Safety Zone 6 as indicated in Figure 4-2 of the ALUCP which means there is no density limitation regarding the number of people on site.

The project will comply with standard City of Goleta construction hours, which are from 8AM to 5PM, Monday through Friday, with no construction allowed on State holidays. Therefore, the project will not expose people to excessive noise levels.

Air Quality

The City references the County of Santa Barbara Environmental Thresholds and Guidelines Manual (2002) which outlines 'Quantitative Emission Thresholds' in Section C of Air Quality Thresholds. Section C stipulates that "if a (proposed) project is smaller in size than the project sizes listed, project-specific emission calculations are not required." This section refers the reader to Table 2: Screening Table to Determine Potentially Significant Long-Term Air Quality Impacts for ozone precursors (nitrogen oxides or reactive organic compounds). The proposed project does not exceed the thresholds identified for a primarily residential land use project of 14 units under the Table 2 screening criteria. As the project does not exceed 800 peak hour trips, as indicated in the traffic analysis prepared by ATE and dated June 20, 2022, carbon monoxide modeling is not required. Therefore, the proposed project would be considered having a less than significant impact regarding long-term air quality impacts.

Short-term air quality impacts, typically related to project construction activities, shall be addressed through standard construction requirements and best practices. The proposed project received a condition letter from the Santa Barbara County Air Pollution Control District (APCD) as part of the development review process, encompassing conditions which the Project shall comply with.

<u>Other Criteria Pollutants</u>
Construction and operation of the Project would not result in emissions that exceed the Santa Barbara County APCD's emission thresholds for any criteria air pollutant, and therefore would not expose sensitive receptors to substantial pollutant concentrations.

As residential and commercial use, the Project will not generate any objectionable odors during operation. The potential for odors is associated with vehicle and equipment exhaust during construction. Given the relatively short duration of construction and the fact that the Project site is not surrounded by a substantial number of people during construction hours, the Project will not create odor impacts considered significant.

The proposed project is consistent with the less than significant threshold criteria, as the project integrates best management practices and will comply with regulatory requirements for short- and long-term impact considerations. The proposed Project does not exceed and/or violate a standard, requirement, and/or plan, and no unique or special circumstances exist that would suggest the project may have a significant impact on air quality. Therefore, as indicated above by the air quality thresholds, the proposed Project can be considered to have a less than significant impact.

Water Quality

The Project will not result in significant water quality impacts as the Project would comply with the City of Goleta Municipal Code Section 15.09.290 during construction. The City's existing NPDES program requires preparation of an Erosion and Sediment Control Plan to outline project-specific Best Management Practices (BMPs) to control erosion, sediment release, and otherwise reduce the potential for discharge of pollutants in stormwater. The Project would not create substantial new impervious surfaces, and new sidewalks and gutters would comply with City of Goleta standards.

The preliminary design of stormwater treatment facilities and other stormwater pollution control measures in the proposed plan are in accordance with the current edition of the Santa Barbara County Project Clean Water's Stormwater Technical Guide. Four bioretention areas have been designed and incorporated into the project to decrease the post-project peak flow of runoff to that of the pre-project value for the 2-year through 100-year storm events. The program HydroCAD was used to determine the peak flow of runoff from the project site for various storm events for both pre- and post-project conditions.

The Project's proposed Stormwater Control Plan (SWCP) was accepted and deemed complete for entitlement application processing per the City's Public Works letter dated January 11, 2023.

The proposed project does not alter the course of a stream or river and/or substantially alter the existing drainage pattern of the site/areas. The proposed Project, through the incorporation of bioretention areas on-site, decreases the post-project peak flow of runoff to that of the pre-project value.

In the Environmental Thresholds Manual, Subsection 16.G.2 Less than Significant Impact indicates that "new development and redevelopment projects that incorporate into the project design construction best management practices (BMP) for erosion, sediment, and construction waste control and incorporate post-construction BMP to protect sensitive riparian or wetland resources, reduce the quantity of runoff, and treat runoff generated by the project to pre-project levels" as a less than significant type of project. The proposed project is consistent with this less than significant threshold criterion, as the project integrates bioretention and BMP to address construction and post-construction measures. With this guidance and threshold, the proposed project is considered to have a less than significant impact.

Utilities and Services

The existing parcel will be served by all required utilities and public services. The Goleta Water District has issued a Preliminary Water Service Determination letter dated March 2, 2022 confirming that the District currently has sufficient water

supply available to serve the proposed Project. The new development conforms to the policies of the City of Goleta General Plan, and the regulations of Title 17, the Goleta Zoning Ordinance, within the City of Goleta Municipal Code.

Exceptions to the Exemption

Moreover, none of State CEQA Guidelines section 15300.2's exceptions to the Class 32 exemption apply to the project. The exception set forth in State CEQA Guidelines section 15300.2(a), by its own terms, does not apply to projects that fall within the Class 32 exemption; moreover, the project will not impact an environmental resource of hazardous or critical concern that is designated, precisely mapped, or officially adopted pursuant to law by federal, state, or local agencies. Section 15300.2(b)'s exception, relating to cumulative impacts, does not apply as technical reports in the record of proceedings demonstrate that the project would not result in significant cumulative impacts; moreover, there are no other successive projects of the same type in the same place that could result in significant cumulative impacts. Section 15300.2(c)'s exception does not apply because there are no "unusual circumstances" that apply to the project; construction of Mixed use as urban infill on commercially zoned land next to existing development does not constitute an unusual circumstance. Section 15300.2(d)'s exception does not apply because the project is not located near any scenic highways. Section 15300.2(e)'s exception does not apply because the project site and off-site improvement locations do not contain hazardous waste and are not on any list compiled pursuant to Section 65962.5 of the Government Code. Finally, Section 15300.2(f)'s exception does not apply because the project has no potential of causing a substantial adverse change in the significance of a historical resource. Additionally, the Project's site does not contain any identified significant cultural resources and will be conditioned to include all mandatory grading best practices.

For all of the foregoing reasons, the entirety of the Project is exempt under the Class 32 exemption.

Conclusion:

Based on this analysis, the entirety of the Calle Real Mixed Use Project meets all criteria for the Class 32 Categorical Exemption pursuant to Section 15332 of the State CEQA Guidelines. Furthermore, exceptions to the applicability of a Categorical Exemption, as specified in Section 15300.2(a) through (f) of the CEQA Guidelines, do not apply to the project. Therefore, the project is exempt from CEQA.

City of Goleta Contact Person and Telephone Number:

Christina McGuire, Associate Planner, (805) 961-7566

Lisa Prasse, AICP	Current Planning Manager	Date

Note: Authority cited: Section 21083 and 211110, Public Resources Code
Reference: Sections 21108, 21152.1, Public Resources Code

Attachment 1

Exhibit B

General Plan Consistency

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EXHIBIT B

GENERAL PLAN/COASTAL LAND USE PLAN CONSISTENCY ANALYSIS FOR CALLE REAL MIXED-USE PROJECT CONDITIONAL USE PERMIT AND DEVELOPMENT PLAN AT 6491 CALLE REAL

The Project (6491 Calle Real Conditional Use Permit and Development Plan, hereinafter "Calle Real Mixed-use Project") is consistent with the Goleta General Plan/Land Use Plan (GP/LUP) as follows:

Land Use Element (LU)

The entirety of the proposed Project and its uses are consistent with the guiding principles and goals of Chapter 2.0, Land Use Element, of the City's General Plan. The proposal is in compliance with the Land Use Plan Map (LU 1.1, Figure 2-1), with designation of Office Institutional (OI), which includes Mixed-use Residential with approval of a Conditional Use Permit. The Project's commercial office component is also one of the allowed uses in the OI zone.

Land Use (LU) Element Policy 4.3 *Office and Institutional* and Table 2-3 specify development standards including a Maximum Residential Density of 20 units/acre, Maximum Structure Height of 35 feet, and Maximum Lot Coverage Ratio of 40%. The physical development associated with the Project meets or exceeds these requirements. The Project's residential density is 26.4 units/acre, which is allowed per the State Density Bonus Law (SDBL) due to the 2 deed-restricted low-income units. Most portions of the Project's roof elements comply with the OI height standard of 35'; however, in a few areas, the roof elements exceed the height limit by 2' 2" (37'2") to accommodate roof features. The applicant has applied for a SDBL concession to accommodate this minor height increase. To deny the concession, the City would have to provide in writing a public health or safety problem or an environmental problem. However, the City has not identified a public health or safety concern, or an environmental problem resulting from the minor height increase and therefore the concession is acceptable. The Project's lot coverage is 21%, which is well under the maximum.

Further, the Office and Institutional category as stated in LU 4.3 is to "provide areas for existing and future office-based uses. […] Mixed-use developments with residential uses on the same site may be permitted at appropriate locations where the residential uses are compatible with adjacent uses and do not break up the continuity of office and institutional uses." The office space is proposed to be either co-working space for tenants or used as an office which is consistent with this policy.

This Project is consistent with LU 1.10 *Multifamily Residential Development* for which the stated purpose is to "provide appropriate locations for Multifamily dwellings as well as allow development standards that allow creativity and diversity in design while protecting health and safety." This project is in an appropriate location due to the zoning district and the varied existing uses on nearby properties. This site is located approximately 0.25 miles from nearby multi-family uses so the additional multiple family complex will not be out of character. The further criteria of LU 1.10(a – f) have been considered as the Project moved through completeness review and those criteria have been satisfied.

Consistent with LU 1.11, various alternative modes of travel, such as public transit, biking, and walking, have been integrated into the Project with the long-term and short-term bike parking and proximity to an MTD bus stop approximately half a mile away. Additionally, walking to Fairview Shopping center requires approximately half an hour, the Project is at the intersection of two Class II Bikeways, and several business parks with major area employers a one-mile walk away.

With respect to LU 1.8 *New Development and Neighborhood Compatibility* and LU 1.9 *Quality Design*, the Design Review Board (DRB) has found the project acceptable with conditions for the Project. The DRB found the Project design including landscaping to be appropriate and compatible with the neighborhood and held that it would contribute in a positive manner to the community's overall design and sense of place and scale. Further, the Calle Real Mixed-use Project circulation, site planning, and design are compatible with the adjacent developments.

Pursuant to LU 1.13, adequate infrastructure and services are available to serve the Project.

Open Space Element (OS)

The proposed project and uses are consistent with the guiding principles and goals of Chapter 3, the Open Space Element of the City's General Plan. In addition, the project is consistent with Open Space Policies OS 8.1, OS 8.3, OS 8.4, OS 8.6, OS 8.7, which are discussed below.

Given the location, scope, and nature of the proposed project, the Project would not create a demand for, nor increase the use of, existing park/recreational facilities or open spaces within the community or affect beach access. As such, the Project would not result in any significant effects on recreational facilities nor create any substantial new demand for public amenities. The project site is not located in the 100-year flood zone.

Open Space Policy OS 8 requires the protection of Native American/Paleontological Resources. As part of the Calle Real Mixed-use Project, a Phase I Archaeological Study was prepared in June 2022 and revised May 2023. Based on the recommendation of the report and meeting with tribal representatives Archaeological Monitoring during over

excavation and the storm water basin will be required during ground disturbance activities out of respect and caution, the requirement for archaeological monitoring has been carried forward as a condition approval for the physical construction at the subject site.

To further ensure consistency with these General Plan policies, conditions of approval are proposed that address the unanticipated discovery of cultural or tribal cultural and paleontological resources during construction. The conditions require ground-disturbing work activities to be stopped if resources are encountered during soil disturbance until the find(s) can be evaluated. As conditioned, the Project is consistent with the Open Space Element policies for cultural and paleontological resources.

Conservation Element (CE)

The proposed Project and uses are consistent with the guiding principles and goals of Chapter 4, Conservation Element of the City's General Plan. The project is also consistent with the following Conservation Element policies CE 10.1, 10.2, 10.3, 10.6, 10.7 and 10.8 and CE 15.3, which are discussed below.

Per Figure 4-1, Special-Status Species and Environmentally Sensitive Habitat Areas (ESHA) of the City of Goleta General Plan, the site does not contain special-status species. Additionally, the project site has no potential value as a habitat area for endangered, rare, or threatened species and is situated adjacent to similar developments within the urban context. The riparian corridor or creek that is closest to the project site is approximately 125 feet to the north, which is separated from the proposed Project by the existing Calle Real roadway. The proposed Project minimizes potential impacts to the maximum extent feasible with implementation of mitigation measures prior to and during construction.

The Project proposes Best Management Practices (BMP) for Stormwater Management consistent with CE 10.1, 10.2, 10.3, 10.6, 10.7 and 10.8 by incorporating bioretention basins, use of permeable materials, revegetation of disturbed areas, planting trees in the proposed parking lot areas, and providing required erosion control plans. The Project has adequate sanitary and waste disposal services, and the stormwater BMP measures will be maintained as required.

The Project is consistent with Policy CE 15.3 *Water Conservation* as the Project will use low-water plumbing fixtures, water-conserving landscaping, and low-flow irrigation.

Safety Element (SE)

The proposed Project and uses are consistent with the guiding principles and goals of Chapter 5, Safety Element of the City's General Plan. The applicable Safety Element policies are SE1.3, SE 7.2, SE 9.8 which are discussed below.

Pursuant to General Plan Safety Element Policy SE 1.3, an Initial Site Assessment screening was conducted to determine the potential presence of hazards, including earthquake hazard zones; areas subject to tsunami run up, landslides, liquefaction, episodic and long-term shoreline retreat (including beach or bluff erosion), high seas, ocean waves, storms, tidal scour, flooding; slopes averaging greater than 25 percent; unstable slopes; and flood hazard areas. No issues were identified that would make the Project a safety concern.

The Project is adequately served by fire protection services, and the Santa Barbara County Fire Department has reviewed and approved the access and circulation plan and has provided Conditions of Approval, pursuant to SE 7.2. The Project would comply with Fire Department standard conditions regarding circulation and access.

The Calle Real Mixed-use Project will not involve the routine transport, use, or disposal of hazardous substances, so the Project is consistent with the policies of SE 9.8.

The Project's site lies outside of the Santa Barbara Airport's noise exposure range as indicated in Figure 4-1 of the 2023 Adopted Airport Land Use Compatibility Plan (ALUCP), and it is located in Safety Zone 6 as indicated in Figure 4-2 of the ALUCP. Therefore, the ALUCP does not prescribe any density limitation regarding the number of people on this project site.

Visual and Historic Resources Element (VH)

The project is consistent with the objectives of section *VH 4.4 Multifamily Residential Areas and 4.7 Office Buildings, Business Parks, Institutional, and Public/Quasi-Public Uses*. The proposed project is located on a Designated Scenic Corridor and is consistent with the Visual and Historic Resources Element, namely Visual and Historic Resources Element Policies VH 1.1, VH 1.4, VH 1.5, VH 2.1, VH 2.3, VH 3.1, VH 3.3, and VH 3.4.

The General Plan/Coastal Land Use Plan Visual and Historic Element Policy VH 1.1 designates the nearby Lake Los Carneros, the more distant foothills, and the Santa Ynez Mountains as Scenic Resources. Views of these resources are currently available from the site and public rights-of-way from both Los Carneros Road and Calle Real adjacent to the project site. From the public roadway and sidewalks adjacent to the site, a portion of the views to the Santa Ynez Mountains are obscured by existing tree canopies. No other public views of natural landforms or open space are seen from the vicinity of the project site.

The City's Visual and Historic Resources Element Policy VH 2.1 identifies Los Carneros Road from Highway 101 to Cathedral Oaks as a Local Scenic Corridor. The proposed building would be located on the east side of the Los Carneros Road Local Scenic Corridor when traveling north as a pedestrian, by bicycle, or by car. Impacts to views

northward along this Scenic Corridor would be considered are minimized due to the proposed building's location to the east of the view corridor.

The Applicant has provided a view study of the area and has installed story poles. These tools document that the project will not impact the nearby Scenic Resources and the project can be found consistent with the applicable policies as the development to the north is approximately the same height and the views to the mountains are maintained.

The project is consistent with the objectives of section VH 4.9 *Landscape Design* as the landscape plan was designed and considered as part of the development and conforms to the natural topography and there are no existing specimen trees. No trees are proposed for removal. The landscaping palette avoids invasive plants and utilizes native and drought tolerant vegetation with a range and density of plantings including groundcovers, shrubs, and trees in varying heights and species.

The project is consistent with Policy VH 4.11 *Parking Lots*, as adequate parking is provided in the quantity specified for the use and at dimensions and widths consistent with City standards, as prescribed by SDBL. Pedestrian circulation is integrated, adequate, and delineated to provide safe and convenient pedestrian links.

The project is consistent with Policy VH 4.12 *Lighting* as the proposed outdoor light fixtures are dark-sky compliant, downward facing, and shielded. The outdoor lighting proposed is minimal in terms of the number of fixtures and level of intensity, preventing light pollution. Further, the fixtures are of appropriate style and compatible with surrounding areas and architectural fabric of the area, which the DRB has reviewed and recommended approval.

Transportation Element (TE)

The proposed Project and uses are consistent with the guiding principles and goals of Chapter 7, Transportation Element, of the City's General Plan. The project is consistent with the following Transportation Element policies TE 3.3, TE 3.5, TE 4.1, TE 9.1, TE 9.2, TE 9.4, TE 9.5, TE 10.4, TE 11.4 and TE 12.1.a which are described below.

General Plan Policies TE 3.3 and TE 3.5 address major Principal Arterials (including Los Carneros Road) and Minor Arterial streets (including Calle Real) which serve the Project's site. Policy TE 4.1 sets Level of Service (LOS) Standard C for roadways and intersections, including the aforementioned Principal and Minor Arterials in the project vicinity. According to the Traffic and Vehicle Miles Traveled Analysis Study prepared for the Project by Associated Transportation Engineers (June 20, 2022), the Project will not degrade Principal or Minor Arterials, or intersection level LOS in the project vicinity and therefore the Project is consistent with the aforementioned Policies.

The Project is consistent with General Plan Policies TE 9.1, 9.2 and 9.4, in that sufficient and adequate off-street parking is provided for the Project in a combination of new on-site parking spaces. The Project is utilizing SDBL for the parking requirement.

Further, General Plan Policies TE 9.5, TE 10.4, TE 11.4 and TE 12.1.a relate to parking lot design, pedestrian facilities in new development, provision of bicycle parking, and the design of driveways and parking lots to minimize pedestrian conflicts with vehicles. The project is using an existing driveway curb cut installed by the City when the roundabout was constructed, and the driveway, parking lot, and buildings' entrances are designed to minimize pedestrian use of the parking lot, thereby reducing potential pedestrian conflicts. The Project provides long-term and short-term bicycle parking. For all these reasons, the siting and design of the Project are consistent with the above referenced Transportation Policies.

Additionally, the proposed project is not in conflict with TE Figure 7-3 (Transportation Improvement Map), Figure 7-4 (Public Transportation System), Figure 7-5 (Pedestrian System Plan), or Figure 7-6 (Bikeways Plan Map).

Public Facilities Element (PF)

The Project is consistent with the Public Facilities Element and Public Facilities Policies PF 3.1 *Fire Protection*, PF 4.1 *Water Facilities and Service*, PF 4.2 *Sewer Facilities and Services*, PF 5.1 *Collaboration with School Districts*, PF 6.1 *Utilities*, and PF 9.7 *Essential Services for New Development.* These policies relate to the timing of new development to sync with the provision of essential public services such as water, sewer, fire, and school capacity. Given that this site is located in an urbanized area of the community, all of the necessary infrastructure exists adjacent to the site. Goleta Water District and Goleta West Sanitation District have indicated that there is sufficient capacity to serve this site. Further, Santa Barbara County Fire Department has indicated that they have capacity to serve the site, and Station 14 is located 0.2 miles to the north of the site so response time to any fire emergency would require only 1 minute of driving. The Project will not impact local schools as 14 new residential units would not significantly increase enrollment.

The proposed project will not result in any significant new demands on public facilities or services. Coordination with agencies providing public services has been performed throughout the review of this project. The project is designed to comply with fire safety design standards identified in the California Fire Code, as referenced by the Goleta Municipal Code, and Fire Department development standards. The Project would not result in any significant new demands on police or fire protection services.

Noise Element (NE)

The proposed project and uses are consistent with the guiding principles and goals of Chapter 9, Noise Element, of the City's General Plan. The project is consistent with Noise Element policies NE 1.5, NE 7.2, NE 2.7, NE 6.4, NE 6.5 which are shared below.

The Project is not expected to generate excessive operational noise based on the proposed use of the property for Multifamily Residential and Commercial Office. The construction of the development may increase ambient noise in the area during the short-term construction period.

The project would not result in impacts related to noise however the tenants may be exposed to noise from U.S. 101 given its proximity. The project site is split across the 60 dBA and 65 dBA noise levels generated by the U.S. 101, as illustrated in the City's General Plan Figure 9-3: Future Noise Contours-Roadways (2030). As indicated in Figure 9-4: Future Noise Contours – Airport (2025) and Railroad (2030), the project site is not within one of the identified future noise levels associated with these two facilities. The City's Noise Element in Table 9-2: Noise and Land Use Compatibility Criteria demonstrates that a multifamily residential use is conditionally acceptable within the 60-65 dBA noise level range.

Consistent with Policy NE 1.5, the new construction proposed at this site shall include appropriate sound attenuation measures, such as insulation, dual paned windows, glazing, etc., to reduce interior noise levels as a condition of construction. Additionally, a fence along N. Los Carneros Road and landscaping shall provide a buffer to roadway traffic, per Policy NE 7.2. The roundabout at the N. Los Carneros Road and Calle Real intersection provides an existing traffic calming measure to help reduce speeds and noise levels adjacent to the project site, pursuant to Policy NE 2.7. The project will be subject to Policies NE 6.4 and NE 6.5 which restrict construction hours and construction equipment operation/specifications.

As discussed above, the proposed project does not exceed standards established in the City's General Plan or noise regulations.

Housing Element (HE)

The Project is consistent with the City's adopted 2023-2031 Housing Element. The Project location is identified as site V16 in Table 10A-28 of the Housing Element Technical appendix in the City's adopted 2023-2031 Housing Element. In addition, the site is categorized in the Housing Element at a density of 20 dwelling units per acre, which would yield a maximum of 11 units for the site. This Project is utilizing SDBL and proposes to construct 14 dwelling units, which would be a density of 26.4 units per acre. Based on the City's inclusionary requirements, the project is required to provide 2.8 units as affordable units. To satisfy this requirement, 2 of the 14 units will be deed-restricted low-income units and the Applicant will make an in-lieu payment for the remaining fractional requirement.

The objective of policy HE 2 *Facilitate New Housing Development to Meet Growth Needs for Persons of All Income Levels* is "to facilitate a variety of residential

development types commensurate with the City's Regional Housing Needs Allocation (RHNA) and needs of the local workforce, designed to be compatible with and enhance Goleta's neighborhoods and the community as a whole." The proposed Project will support this objective due to the addition of housing and the provision for two (2) low-income dwellings.

Attachment 1

Exhibit C

Zoning Consistency Analysis

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ZONING CONSISTENCY ANALYSIS

ORDINANCE REQUIREMENTS	PROPOSED PROJECT	CONSISTENCY WITH STANDARDS
17.09.030 **Front Yard Setback applicable to Calle Real:** Fifteen (15) feet from the property line	26’11”	Yes
17.09.030 **Interior Side Setback (East):** Fifteen (15) feet from the property line **Trash enclosure may be located within interior side setback (17.24.140 (B) (2) (b)**	74’10” for building 6’ for trash enclosure.	Yes
17.09.030 **Street Side Setback applicable to Los Carneros:** Fifteen (15) feet from the property line	21’6”	Yes
17.090.030 **Rear Yard Structure Setback (South):** Fifteen (15) feet from the property line	15’	Yes
17.09.030 **Maximum Lot Coverage: 40%**	21%	Yes
17.09.030 **Maximum Dwelling Unit Density (units/acre):** 20 Dwelling units per acre	26 dwelling units/ acre	Yes* 30% density bonus per State Density Bonus Law G.C. 65915 providing two (2) low income units
17.09.030 Maximum Building Height: Maximum height of 35’	37’2”	Yes * Utilizing State Density Bonus Law Concession to increase maximum height

17.09.030 Minimum Landscaping: 10% required	22%	Yes
17.07.050(B)(1) Common Open Space: 100 square feet per unit 14 units = 1,400 square feet	1,400 square feet total	Yes
17.07.050(B)(2)(a)and(b) Restricted Open Space: 200 s.f. for units on ground; and 60 s.f. for units without floor area on the ground story	4 ground floor units: 200 s.f. 10 second story units : s.f.	Yes
17.38 **Parking:** Total Auto Spaces: State Density Bonus Law Gov Code §65915(p) mandates no more than one parking space for one-bedroom units. One space per 300 SF for commercial use.	Total Provided: 20 spaces (18 for residential and 2 for commercial)	Yes*
EV Charging Spaces: 2	Provided: 2 spaces	Yes
ADA Spaces: 2	Provided: 2 spaces	Yes
Van Spaces: 1	Provided: 1 space	Yes
Long-term Bicycle Spaces: 14	Provided: 16	Yes
Short-Term Bicycle Spaces: 6	Provided: 10	Yes

17.090.030 **Limitations on Curb Cuts:** One per parcel unless a need is demonstrated	Using the 1 existing curb cut	Yes
17.24.210 **Vision Clearance:** No structure or vegetation in excess of 3 feet within the "triangle" as defined in Section 17.24.210	No wall, fence or landscaping will exceed a height of 3 feet with the defined triangle.	Yes
17.24.170 **Screening of Mechanical Equipment:** All mechanical equipment must be screened from public view.	Mechanical equipment is located on the roof of the buildings and is surrounded by a screen wall on all sides that is integrated into the building.	Yes
17.24.140 **Trash Enclosures:** Must be located outside of front & street setbacks and required parking & landscape areas.	The trash enclosure is located in the northeastern corner of the property where trash collection is easily accessible. They will be within a solid enclosure structure. Per 17.24.140(B)(2)(b) since not abutting an "R" Zone District parcel trash enclosure may be located within interior side setback.	Yes
17.35 **Parking Lot Lighting:** Must not exceed 20 feet in height. The light level at property lines must not exceed 0.1 foot candles.	Proposed parking lot lights are on 12 Foot poles. The foot candle at the property lines does not exceed 0.1 foot candles.	Yes
17.30.070 **ESHA:** The Streamside Protection Buffer is 100 feet	The closest structure to Riparian Habitat is 125 feet.	Yes

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Attachment 1

Exhibit D

Conditions of Approval

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Conditions of Approval
6491 Calle Real Mixed-Use Project
Conditional Use Permit 22-0001-CUP
Development Plan Case No. 22-0005-DP
Design Review Board Case No. 23-0028-DRB

In addition to all applicable provisions of the Goleta Municipal Code ("GMC"), 6491 Calle Real Partners, LLC, ("Property Owner" and RRM Design Group, applicant (collectively hereinafter referred to as "Permittee") agrees to the following conditions for the City's approval of Case Nos. 22-0001-CUP, 22-0005-DP, and 23-0028-DRB ("Project Conditions").

Unless the contrary is stated or clearly appears from the context, the construction of words and phrases used in these Project Conditions use the definitions set forth in the GMC. For purposes of these Project Conditions, the term "Director" refers to the Planning and Environmental Review Director.

A. **AUTHORIZATION**

1. This Conditional Use Permit, Case No. 22-0001-CUP, this Development Plan, Case No. 22-0005-DP, this Design Review Board, Case No. 23-0028-DRB, authorizes implementation of plans stamped "APPROVED", dated _____, and attached/subject to these Conditions of Approval set forth below, including specified plan sheets and agreements included by reference, as well as all applicable City rules and regulations.

   The Project description is as follows:

   Proposal to construct a new three-story mixed-use building on a vacant 0.53-acre lot proposed as a Housing Accountability Act housing development project. The building would total 14,257 square-feet, comprising 1) one commercial office space of 585 square feet; plus 2) 14 one-bedroom residential units totaling 11,520 square feet; and 3) 2,152 square feet of common areas dedicated to residential use. This project is using State density bonus law (SDBL) to increase the density, to reduce parking requirements (adhering to parking numbers per SDBL), to receive a concession from the City's parking lot heat island requirements, and seek a height waiver to increase the building height by 2.5 feet.

   The 14 residential units would range in size from 694 to 979 square feet. Each residential unit will have a private patio or balcony associated with the unit. An approximately 585-square-foot patio would be shared between the commercial and residential tenants and 3,454 square feet of outdoor area would be shared by

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residents only. Parking includes 20 uncovered automobile parking spaces plus 10 short-term and 16 long-term bicycle parking spaces. The project includes a trash enclosure and other site amenities. The project will be all-electric, and no gas connection is proposed.

2. All construction, improvements, implementation, and/or any other actions taken pursuant to this permit shall be in substantial conformance with the Project. Any deviations from the Project must be reviewed and approved by the City of Goleta (City). The City shall determine whether any deviation substantially conforms to the Project. Any deviation determined to not be in substantial conformance with the Project requires the Applicant/Permittee to seek additional approval, permits, or other action by the City. Any deviation from the Project made without the above-described review and approval of the City is a violation of this permit.

3. Approval of the Development Plan will expire five (5) years after approval, unless before the expiration, substantial physical construction has been completed on the Development Plan or a time extension has been applied for by the Permittee. The decision-maker with jurisdiction over the Project may, upon good cause shown, grant a time extension as specified by City regulations. If the Applicant/Permittee requests a Time Extension, the Project may be revised to include updated language to standard conditions and/or may include revised/additional conditions which reflect changed circumstances or additional identified Project impacts. Any new fees imposed, and existing fees will be those in effect at the time of the extension request.

4. Should the Project construction be phased, all unbuilt portions of the approved Development Plan shall expire at the same time as the Development Plan or two years from the date of issuance of the Building Permits, whichever occurs later, unless prior to the expiration date, substantial physical construction has been completed on the unbuilt portion of the development.

5. This Development Plan shall become effective upon the date of Planning Commission action on the Development Plan.

6. This permit runs with the land and the rights and obligations thereof, including the responsibility to comply with Development Plan, are binding upon successors in interest. The Development Plan may be modified, terminated, or abandoned in accordance with applicable law including, without limitation, the GMC.

7. On the date that a subsequent Development Plan and/or Conditional Use Permit is approved for this site, any previously approved but unbuilt plans become null and void.

8. The City will only issue permits for development, including grading, when the construction documents (e.g., grading plans, building plans, etc.) are in substantial compliance with the approved Development Plan. The size, shape arrangement, use, and location of buildings, walkways, parking areas, drainage facilities, and

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landscaped areas must be developed in substantial conformity with the approved plans. Substantial conformity must be determined by the Planning and Environmental Review Director.

9. Any proposed deviations from the Project Plans, Project description, or Project Conditions must be submitted to the Planning and Environmental Review Director for review and approval by the appropriate decision maker. Any unapproved deviations from the Project approval will constitute a violation of the permit approval. The plans labeled Project Plans with date July 5, 2023 are herein incorporated by reference.

10. When exhibits and/or written Project Conditions are in conflict, the written Project Conditions must prevail.

11. The Project site and any portions thereof shall be sold, leased or financed in compliance with the exhibit(s), Project description and the conditions of approval including all related covenants and agreements.

12. No signs of any type are approved with this action unless otherwise specified. All future signage must be reviewed and permitted in compliance with the City's zoning code.

13. This approval does not confer legal status on any existing structures or uses on the property unless specifically reviewed and authorized within the Project description of this Development Plan.

14. Permittees agrees to indemnify, hold the City harmless from and defend the City against any and all claims, actions, appeals, damages, costs (including, without limitation, attorney's fees), injuries, or liability (each, a "Claim"), arising from or related to the City's approval of the Development Plan ("DP") and Conditional Use Permit ("CUP") and project design attendant Conditions of Approval or any associated post-discretionary approvals, approval and condition clearance of the Development Plan and associated post-discretionary approvals ("Project Approvals"). The obligation to indemnify, hold harmless and defend the City shall arise when the City is named in or subject to any Claim, including without limitation a lawsuit, claim, or other legal proceeding regardless of whether such Claim is meritless, meritorious, or otherwise arising out of related to any of the Project Approvals. Permittees shall pay all litigation costs, attorney's fees, expert fees, and other costs associated with any Claim related to or arising out of the Project Approvals against the City ("Defense Costs"), and agrees that the City may select counsel of its own choosing to defend against such Claim(s). Permittees shall remit payment of the City's Defense Costs within thirty (30) days of request for payment. For purposes of this section "the City" includes the City of Goleta's elected officials, appointed officials, officers, employees, consultants, contractors, and agents. Permittees are jointly and severally liable for this condition.

15. In the event than any conditions imposing a fee, exaction, dedication or other mitigation measure is challenged by the Applicant/Permittee in action filed in a court

DRAFT

of competent jurisdiction or threatened to be filed, this approval must be suspended pending dismissal of such action, the expiration of the limitation period applicable to such action, or final resolution of such action. If any Project Condition is invalidated by a court of competent jurisdiction, the Project must be reviewed by the City and substitute conditions may be imposed to validate the Development Plan.

B. AGENCY REQUIREMENTS

16. **Santa Barbara County Fire Department (SBCFD)**: Comply with all conditions and requirements outlined in the letter dated August 22, 2022, Santa Barbara County Fire Department letter to the satisfaction of the SBCFD prior to issuance of a building permit.

17. **Goleta Water District (GWD).** Comply with all conditions and requirements outlined in the letter dated March 2, 2022, of GWD to the satisfaction of the GWD, including securing water service connections prior to the issuance of grading permits.

18. **Goleta West Sanitary District (GWSD).** Comply with all of the conditions and requirements outlined in the letter dated September 22, 2022, of Goleta West Sanitary District to the satisfaction of the GWSD, including securing sewer service connections prior to issuance of grading permits.

19. **Air Pollution Control District (APCD).** Comply with all suggested Conditions and requirements in the letter dated July 27, 2022, including District Authority to Construct permits.

C. CITY DEPARTMENT- PUBLIC WORKS

Prior to the Issuance of the Zoning Clearance the following requirements must be met unless otherwise specified the Applicant/Permittee must:

<u>DRAINAGE</u>

20. Comply with the requirements of the Santa Barbara County Flood Control District Standard Conditions of Project Plan Approval dated January 2011, available on the City's web site.

21. Obtain approval of a Hydrology and Hydraulics Study from the Public Works Director or designee. The study must

    Use the Santa Barbara County Urban Hydrograph method or approved equal, provide Hydrology and Hydraulics calculations for the 2, 5, 10, 25, and 100-year storm events for both pre and post construction and mitigate any increase in peak flow for the 2,5-,10-, 25-, and 100-year storm events over existing conditions.

DRAFT

A. Indicate drainage flows to be anticipated from the entire watershed within which the development location.

B. Show drainage across property lines shall not exceed that which existed prior to grading unless the Property Owner agreed and signed a Drainage Easement. Excess or concentrated drainage shall be contained on site or directed to an approved drainage facility.

C. Indicate that all off tract drainage and flood control facilities and installations must be installed and completed prior to any grading of the subject development.

D. Be prepared, signed, and stamped by the Registered Engineer in California.

Prior to Zoning Clearance, the Applicant/Permittee must:

22. Secure approval of a **Stormwater Control Plan (SWCP)** from the Public Works Director or designee**.** The Owner/Applicant shall submit a SWCP for regulated projects that create or replace 2,500 square feet or more of impervious surface and utilizes Low Impact Development (LID) measures to detain, retain and treat runoff for review and approval by the Public Works Director or designee. The SWCP shall be prepared in accordance with the California Regional Water Quality Control Board, Central Coast Region, Resolution R3-2013-0032, Post-Construction Stormwater Management Requirements (PCR) for Developmental Projects in the Central Coast Region (PCRs Resolution) and shall use the "Stormwater Technical Guide for Low Impact Development" as set forth by the County of Santa Barbara for guidance in complying with the PCRs Resolution. The following are design considerations when proposing" Stormwater Control Measures (SCMs):

    A. Surfaced based SCMs including but are not limited to, self-retaining areas, grassy swales, or biofilters should be maximized to the extent practicable to achieve runoff retention before considering SCMs listed within item b of this section.
    B. Surface Basin, Subsurface Well, Fluid Distribution System/Galleries and/or Infiltration Trench may require registration as an Environmental Protection Agency (EPA) Class V Injection Well.
    C. Clearly identify and design a non-erosive emergency overland escape route from any proposed SCMs as needed.

23. Secure approval of a **Stormwater Facilities Operation and Maintenance (O&M) Plan** for all parcels from the Public Works Director or designee. The Owner/Applicant/Owner shall submit a O&M Plan that will be used to plan, direct, and record the maintenance of all SCMs on-site for review and approval by the Public Works Director or designee. The O&M plan will default to the final O&M Plan if no

amendments are needed prior to Owner/Applicant request for Final Occupancy Clearance(s).

24. Submit the **Stormwater Data Sheet Application** (Page 1 and/or 2 as applicable) with the SWCP and O&M Plan.

25. Submit and secure approval of an **Erosion and Sediment Control (E&SC) Plan** from the Public Works Director or designee for all parcels if project is disturbing less than < 1 acre of soil area and is not part of a common plan of development, and therefore not subject to the General Construction Permit's SWPPP requirement. The E&SC Plan shall be prepared in accordance with the City approved E&SC Plan template. The E&SC Plan shall include BMPs to control the discharge of sediment and other pollutants from the site and are not picked up by irrigation runoff or rain and transported to the nearest storm drain and into our waterways. Grading activities should be reduced or avoided to the extent feasible during the rainy season (October to May) to minimize soil disturbance at any one time.

26. Secure approval of a **Drainage and Stormwater Facilities Agreement (Maintenance Agreement)** for all parcels concurrently from the Public Works Director or designee and execute the Maintenance Agreement, in a form approved by the City Attorney. The Maintenance Agreement shall include, but not be limited to, insurance, security, and other measures to ensure that Permittee properly installs and maintains the Project's stormwater facilities in perpetuity. The Maintenance Agreement shall include all SCMs that will be inspected and maintained during construction and phased Occupancy Clearances. The Maintenance Agreement will be based on procedures and information outlined in the O&M Plan. The Maintenance Agreement shall include a legal description of the project and project location, and the party responsible for O&M Plan implementation. The Maintenance Agreement shall be signed by the Property Owner accepting responsibility of O&M of the installed onsite and/or offsite treatment and flow control SCMs until such responsibility is legally transferred to another entity in accordance with the requirements specified within the PCRs Resolution. The Maintenance Agreement shall be recorded with the County of Santa Barbara. The Property Owner shall provide a signed, and notarized Maintenance Agreement to the City for final review and acceptance by the City prior to recordation. The Property Owner is responsible for all of the costs associated with the preparation and recordation of said Maintenance Agreement. Supporting documentation for the Maintenance Agreement may require updates and revisions to reflect 'Record Drawing/As-Built' conditions, and the 'As-Built' information will be

DRAFT

submitted to the City for review prior for acceptance prior to inclusion in said Maintenance Agreement.

Prior to obtaining Grading/Building Permit, the Owner/Applicant must:

27. Secure approval of a Construction and Demolition (C&D) Debris Recycling from the Public Works Director or designee**.** Provide a copy of the signed Certificate of Implementation of State Law: Construction and Demolition (C&D) Debris Recycling Program ensuring compliance with the Green Building Code. This form indicates who will haul all material and an account number for the C&D project from the hauler. This will facilitate the recycling of all construction recoverable/recyclable materials. The project will be required to meet the CalGreen minimum diversion requirement of 65% of the project's solid waste to be diverted from the landfill.

28. The following shall be identified on the Building or Grading Plans:

    A. Show all existing survey monuments to be preserved and/or tied out in coordination with the County of Santa Barbara's Surveyor's Office. Survey monuments that control the location of subdivisions, tract boundaries, roads, streets, or highways or provide horizontal or vertical survey control shall be surveyed and documented in accordance with Section 8771 of the Land Surveyor's At. Preliminary copies of pre- and post- construction Corner Records shall be provided to the City as evidence of compliance with the Land Surveyor's Act.
    B. Indicate all Rights-of-Way Easements and Monuments
    C. Provide official documentation approving use of an easement from all utilities that have easement rights for the use of the applicable easements.
    D. Reset survey monuments if damaged during construction. If survey monuments are damaged or affected by the construction activities, the City will require a security for the resetting of the survey monuments disturbed by construction. The Owner/Applicant shall submit an estimate, signed, and stamped by a Licensed Surveyor in the State of California for monument preservation. This estimate will be used to determine the amount of the security.
    E. To ensure compliance with City Engineering Design Standards and the current version of ADA standard/ regulations, the construction of the new

DRAFT

curb opening/driveway approach, curb outlet storm drain, and sidewalk improvement on Calle Real must adhere to these requirements.

F. The landscape along the Calle real frontage of the Project site should be maintained at a height of 3.5 feet in order to meet the sight distance looking west along the curve.

Prior to Encroachment Permit(s) Issuance, the Applicant/Permittee must:

29. Comply with City Resolution No. 15-46, Construction and Major Maintenance Limitations in the Public Right-of-Way for construction working hours and lane closure limitations.

30. If necessary, submit for and secure approval of a Haul Permit (50 Cubic Yards or more) from the Public Works Director or designee. All applicable permits for the placement of exported material at off-site location(s) within the City limits must be provided to the Public Works Director or designee. The Haul Permit must clearly identify:

A. The proposed haul routes
B. The proposed location for placement of export material.
C. Measures to ensure that all haul trucks hauling debris, sand soil and/or other loose materials shall be covered and/or maintain a minimum 2 feet freeboard.
D. Measures to ensure that construction vehicles only use the City's designated Truck Routes, as clearly indicated on the Haul Route Exhibit. All other routes are prohibited.
E. Measures to ensure that construction parking is implemented in a manner that will minimize the potential for traffic interference. Include construction parking designated area(s) on Haul Route exhibit.
F. Clearly identify the proposed area for construction vehicle staging and location(s) for construction vehicle ingress and egress. The ingress/egress pattern shall be identified on the Haul Route Exhibit.

Pre-Construction, the Applicant/Permittee must:

31. Hold a Pre-Construction Meeting with Public Works staff to discuss stormwater BMPs and to coordinate inspection of all Stormwater Control Measures (SCMs) including Underground Injection Control (UICs) installed during construction.

32. Provide the Public Works with a schedule 2 weeks in advance of the SCMs including

DRAFT

Surface Basin, Subsurface Well, Fluid Distribution System/Galleries including UICs and/or Infiltration Trench installations, so Public Works can inspect and verify subgrade and subsequent construction details for all SCMs.

<u>During Construction, the Project Applicant/Permittee must</u>:

33. Ensure ongoing compliance with the E&SC Plan and shall perform inspections and maintenance on all installed BMPs, including work performed within the Road right-of-way, and the SCMs as identified in the Maintenance Agreement. Maintenance Reports shall be submitted to the Public Works Department.

34. Ensure ongoing implementation of BMP Requirements, including dust control of soil off-site. The Owner/Applicant shall identify appropriate BMPs to control the volume, rate, and potential pollutant load of stormwater runoff; and ensure that BMPs are installed, implemented, and maintained through the duration of the project (construction, new or redevelopment) to minimize the potential discharge of pollutants to the Storm Drain System. These requirements may include a combination of structural and non-structural BMPs that are consistent with the California Storm Water Quality Association (CASQA) Best Management Practice Handbook, most current edition (or equivalent), and shall include requirements to ensure the proper long-term operation and maintenance of these BMPs.

35. Ensure the use of the SCMs during active construction should be documented within the E&SC Plan as well as shown on the grading and drainage plans. The following measures must be implemented during active construction to protect post-construction SCMs and while disconnected to ensure that they function properly:

    A. SCMs such as UICs or bioretention basins must be protected from sedimentation during construction activities or until the site surface conditions are stabilized. Erosion and Sediment Control BMPs must be inspected and maintained to ensure effectiveness so that inflow of sediment from runoff from nearby stockpiles and disturbed areas, etc. does not occur that could cause SCM to clog;
    B. SCMs must also be protected from construction wastewater (concrete, stucco, paint, etc.), construction debris or other materials during construction activities that can cause the SCMs to clog;
    C. SCMs should be kept off-line until the surrounding tributary areas are fully stabilized to prevent clogging;
    D. Minimize compaction of soils and restrict heavy equipment in the area of SCMs to ensure infiltration rates are not affected. If compacted, the

DRAFT

contractor will restore the native soil infiltration rates through ripping or disking.

Prior to Issuance of the Final Certificate of Occupancy , the Applicant/Permittee must:

36. Pay all required City Development Impact Fees as specified in Conditions.

37. Schedule a Final Construction Inspection and PCR Field Verification Inspection:

A. Final Construction Inspection – Following the completion of active construction and stabilization of disturbed areas the Public Works Director or designee will conduct an inspection in accordance with the Construction Site Inspection Form, to verify all temporary erosion and sediment control measures and BMPs have been removed and completed work is in compliance with the approved Plans and 'Record Drawing/As-Built' Plans. The City shall note that any outstanding issues have been resolved in a manner acceptable to the City.

B. Final PCR Field Verification Inspection – The PCR field verification inspection must include a visual inspection of each of the SCMs to ensure the SCMs are installed according to the SWCP and As-Builts. The Owner/Applicant shall submit all images, dimensions, and elevations of constructed SCMs prior to the request for Occupancy Clearance to verify the installation of all SCMs that are subgrade or otherwise unable to be verified by Final PCR Field Verification Inspection. If design changes were implemented, 'Record Drawing/As-Built' Plans shall be submitted prior to the request for PCR Field Verification Inspection.

38. Complete the SWCP Certification of Approval and submit the following support documentation:

A. Record Drawings/As-Built Site Plans – The Plans submission shall include CAD drawing files and GIS files of as-bult conditions including all storm drain infrastructure installation and/or improvements.
B. Quality Assurance/Quality Control Summary Checklist
   i. SCM Installation Elevations by Licensed Surveyor as determined on plan;
   ii. SCM Basin Dimensions and Depths;
   iii. Photo Documentation of SCMs throughout the installation of the project (including installation of Liner and Geotextile Materials)
   iv. Material Submittals for Liner and Geotextile Materials which may be invoices;

DRAFT

v. Soil Compaction Report by Licensed Soils Engineer documenting compliance with SCMs and project plans

C. Stormwater Control Plan (Resubmittal, if As-Builts)
D. Operation & Maintenance Plan (Resubmittal, if As-Builts)
E. If the requirements of this condition are not completed at time of Certificate of Occupancy, then Applicant/Owner shall furnish to the City a $5,000 cash deposit, to the Public Works Director, guaranteeing the completion of this condition. The cash deposit will be returned once this condition has been met or the cash deposit will be used to complete this condition.

39. Submit a Waste Reduction and Recycling Summary (WRRS) to the Public Works Department for review and approval. The Report shall substantiate how a minimum 65% diversion goal was met by the project during construction, provides the actual amounts of material generated and what the final diversion rate was, along with either scale house receipts or a summary from the diversion facility used substantiating each load brought to the facility, the tonnage, and the diversion achieved

40. Submit a Private Improvement Certification for parking lots, roads, sidewalks, etc. by the Engineer of Record. The Certificate shall include Record Drawing/As-Built Plans and Quality Assurance testing information and results.

41. Complete all encroachment permits for public improvement and associated As-Built Plans shall be submitted electronically to the Public Works Director or designee. The As-Built Plans shall incorporate all changes and/ or revisions.

D. CITY DEPARTMENT CONDITIONS – PLANNING AND ENVIRONMENTAL REVIEW DEPARTMENT

AFFORDABLE HOUSING CONDITIONS

42. Of the fourteen (14) housing units in the project, two (2) one-bedroom units must be designated and operated as Below-Market Rate (BMR) Units for a period of fifty-five (55) years, in compliance with the City's most current Affordable Housing Policies and Procedures Manual and with the following:

    a. The BMR Units must be reserved for occupancy by Low Income households with gross household incomes which do not exceed the Low Income limit, adjusted by household size, updated and published annually by the California Department of Housing and Community Development (HCD).

    b. The BMR Units must be leased and occupied by qualified tenant households no later than six (6) months after the issuance of the Certificate of Occupancy for the last non-BMR Unit.

DRAFT

c. For the initial leasing only, a public housing lottery will be used to select the occupant households of the BMR Units.

d. The maximum monthly rent amounts will be set initially in compliance with the most current Affordable Housing Policies and Procedures Manual and the Maximum Rent Amounts published on the City's website. Annually thereafter, the maximum monthly rent amounts will be adjusted with the annual publication of Area Median Income (AMI) by HCD.

43. Prior to building-permit issuance for construction of the proposed buildings, the Applicant/Permittee shall prepare an Inclusionary Housing Plan, which shall include a description and site plan of the project, identification of the BMR Units, and floor plans and specific information for the BMR Units (i.e., income category, square footage, bedroom count, amenities). The plan shall be submitted to Housing Program staff for review and must be approved before the building permit(s) for the non-BMR Units can be finalized.

    Prior to building-permit issuance for construction of the proposed buildings and at least six (6) months prior to the commencement of any advertisement or marketing of the BMR Units, the Applicant/Permittee shall prepare a Marketing Plan and Tenant Selection Plan, in compliance with the City's Affordable Housing Policies and Procedures Manual, and submit both documents to Housing Program staff for review. Housing Program staff shall review the Marketing Plan and Tenant Selection Plan, and the plans must be revised as necessary, resubmitted, and approved by Housing Program staff before advertisement of the BMR Units may begin.Prior to issuance of the first Zoning Clearance (either grading or construction of the proposed buildings), the Applicant/Permittee must execute and record on property title a "Regulatory Agreement and Declaration of Restrictive Covenants" in a form prepared by the City to formalize and govern the operation and maintenance of the BMR Units throughout the 55-year term of affordability, which shall not commence until both BMR Units are occupied. The costs associated with the review of the regulatory agreement will be at the Applicant/Permittee's sole expense, and the agreement must be approved by the City Manager and City Attorney.

GENERAL CONDITIONS

44. The Applicant/Permittee is responsible for ascertaining and paying all City Development fees, including without limitation, fire facility fees, library fees, park fees, police services fees, public administration fees, and transportation fees as required by the GMC, in effect at time of payment. In addition, the impact fees established by the Goleta Union/Santa Barbara Unified School Districts (School Fees) shall also be paid in accordance with the requirements of those entities. This condition also serves as notice pursuant to Government Code Section 66020 (d) that the City of Goleta is imposing development impact fees ("DIFs") and the Applicant/Permittee has 90 days after the imposition of the fees to protest fees.

DRAFT

The Applicant/Permittee must pay all applicable development impact fees and other contributions for the Project in full no later than at the time specified in the below chart. The amount of the actual fee may be different than that listed below, as the Applicant/Permittee must pay the most current rate of the fee in effect at the time of payment and on the latest edition of the Institute of Traffic Engineers Trip Manual. At the Permittee's discretion, the Applicant/permittee may choose to pay the below listed fees at any time, but no later than the time frame specified.

FEE 14 Dwelling Units	RATE	ESTIMATED FEE	TIME DUE	AGENCY
Fire Facility	$951/DU	$13,314	CO/FI	City
Library	$788/DU	$11,032	CO/FI	City
Park fee	$8,979/DU	$125,706	CO/FI	
Public Administration	$2,531/DU	$35,434	CO/FI	City
Transportation (7 PM Peak Hour Trips, residential only)	$14,068	$196,952	CO/FI	City
Storm Drain	$2,949/DU	$41,286	CO/FI	City
School Fees	Set by School Districts	--	BP	Goleta Union & SB Unified School Districts
Bicycle and Pedestrian	$2,536/DU	$35,504	CO/FI	City
TOTAL	__	$459,228.00	__	__

*14 Dwelling units proposed
DU = Dwelling Unit BP = Building Permit KSF = 1,000 Square Feet CO = Certificate of Occupancy FI = Final Inspection

FEE 585 SF of Office	RATE	ESTIMATED FEE	TIME DUE	AGENCY
Fire Facility	$1,199/ KSF	$701.42	CO/FI	City

DRAFT

Library	$231/KSF	$135.14	CO/FI	City
Park fee	$2,857/KSF	$1,671.35	CO/FI	
Public Administration	$742/KSF	$434.07	CO/FI	City
Transportation (4 PM Peak Hour Trips, Office only)	$14,068	$56,272	CO/FI	City
Storm Drain	$3,019/KSF	$1,766.12	CO/FI	City
School Fees	Set by School Districts	--	BP	Goleta Union & SB Unified School Districts
Affordable Housing Development Fee Program (Office)	$8,283/KSF	$4,845.56	CO/FI	City
Bicycle and Pedestrian	$743/KSF	$434.66	CO/FI	City
TOTAL	__	$66,260.32	__	__

*585 Square feet of Office,
DU = Dwelling Unit BP = Building Permit KSF = 1,000 Square Feet CO = Certificate of Occupancy FI = Final Inspection

45. Residential Affordable Housing In-Lieu Fees need to be paid by CO; however, they may not be paid any earlier than Building Permit Issuance.

    The applicant will satisfy the City's Inclusionary Housing requirement for the project by providing two (2) Low-Income units onsite with a deed restriction of 55 years and paying the fractional remainder (0.2) as an In-Lieu Fee equal to its percentage of the project's total Inclusionary requirement (2.2) times $11.60 per square foot of living space. The Residential Affordable Housing In-Lieu Fee is $11,329.

46. Any modifications to the approved plans including but not limited to site plan, floor plans, elevations, landscaping, colors and materials, cannot be executed without the Planning and Environmental Review Director's approval.

47. The Applicant/Permittee shall maintain all landscaping and irrigation, per the approved plan, for the life of the Project.

DRAFT

48. Before using any land or structure, or commencing any work pertaining to the erection, moving, alteration, demolition, enlarging or rebuilding of any building structure, or improvement, the Applicant/Permittee must obtain a grading and/or building permit from the Planning and Environmental Review Director. Both the grading and the building plans must denote that any portion of the site with archaeological sensitivity cannot be used as parking/storage of construction workers vehicles, construction equipment, stockpiling, or construction materials storage, etc., to the satisfaction of the Planning and Environmental Review Director.

49. All plans submitted for permit issuance (e.g., grading, building permit, etc.) must include all applicable conditions of Project approval. As the Project meets the threshold outlined in Chapter 17.34 of the GMC regarding Water Efficient Landscaping, the Applicant/Permittee must secure approval of an irrigation plan from the Building Official or designee before issuance of a building permit. The landscape and irrigation shall be installed per plan prior to Final Building Inspection Clearance.

50. Before the start of any work on-site, the Applicant/Permittee must conduct a pre-construction meeting that includes the Applicant/Permittee, Project superintendent, architect, contractors, subcontractor, as well as City representatives from the Planning and Environmental Review and Public Works Departments and including all elements included in the Mitigation Monitoring and Reporting Program.

51. All work within the public right-of-way, including without limitation, utilities and grading, must be explicitly noted on the building plans. The Applicant/Permittee must obtain all necessary encroachment permits from the Public Works Director or designee, before commencing work within or over the public right-of-way including without limitation, water meters, backflow devices, signs, and curb/gutter/sidewalk improvements.

52. Any temporary building, trailer, commercial coach etc. installed or used in connection with the construction of this Project must comply with the requirements of Section 35-281 Article III of the City's Inland Zoning Ordinance.

53. The Applicant/Permittee is responsible for informing all contractors, subcontractors, consultants, engineers, or other business entities providing services related to the Project of their responsibilities to comply with these conditions including, without limitation, the GMC. This includes the requirements that a business license be obtained to perform work within the City as well as the City's construction hour limitations.

54. The Applicant/Permittee, including all contractors and subcontractors, shall limit construction activity and equipment maintenance is limited to the hours between 8 AM and 5 PM, Monday through Friday. Exceptions to these restrictions may be made for good cause at the sole discretion of the Planning and Environmental Review Director, for onsite work. Exceptions to these restrictions may be made for good cause at the sole discretion of the Public Works Director or designee, for work in the

DRAFT

City Right-of-Way only. Any subsequent amendment to the General Plan noise standard upon which these construction hours are based shall supersede the hours stated herein. No construction can occur on State holidays (e.g., Thanksgiving, Labor Day). Non-noise generating construction activities such as interior plumbing, electrical, drywall and painting (depending on compressor noise levels), are not subject to these restrictions.

55. The 585 square foot office component of the Mixed-Use Development is required to be provided and maintained on the site and is not allowed to be converted to a residential use unless the standards of the OI land use designation are changed. The residential component is only allowed through the approval of the Conditional Use Permit because of the office component.

56. Prior to the issuance of a Zoning Clearance, the Applicant/Permittee must:

A. Provide evidence that they have recorded a signed Agreement to Comply with Conditions that specifies that the Applicant/Permittee and Property Owner agrees to comply with the Project description, approved exhibits and all conditions of approval. All costs associated with the preparation, review and recordation of the Agreement to Comply with Conditions are the sole responsibility of the Applicant/Permittee.

B. Secure Final Design Approval from the Design Review Board (DRB) regarding building design, landscaping and irrigation, lighting, site improvements including utilities, and address the conditions for approval from the DRB meeting of July 25, 2023 as feasible. The items for DRB review include:

i. Revise the architectural, site, and/ or landscaping plans to address the July 25, 2023 DRB conditions regarding:

a. Relocating or screening infrastructure items at northwest corner of the site.
b. Reconsider color palette of the project.
c. Restudy pitched roofs.
d. Study South elevation.
e. Integrate the downspouts into building and painted to match wall color.
f. Better integrate the stair on north elevation with the building.
g. Revisit stormwater connection.
h. Consider removing bocce ball court and moving bike parking to south of building, and options for an access gate near southwest corner.
i. Reconsider specific plant selections.
j. Better integrate bike storage.

ii. The landscaping plan must consist of at least 75% drought-tolerant native or Mediterranean type plants which adequately complement the Project design

and integrate the site with surrounding land use. The plant material used in the landscape palette must be compatible with the Goleta climate pursuant to Sunset Western Garden Book Zone 24 published by Sunset Books, Inc. Revised and Updated 2012 edition or a more current edition.

The irrigation plan must demonstrate compliance with the City's Water Conservation regulations and Guidelines for Water Conservation in Landscaping. Use reclaimed water to irrigate landscaped areas if the recycled waterline is extended to serve the site. If that occurs, then dual water connections must be installed to allow for landscaping to be irrigated by reclaimed water, if feasible.

Utilize efficient irrigation systems which minimize runoff and evaporation and maximize the water which will reach plant roots (e.g., drip irrigation, automatic sprinklers equipped with moisture sensors, etc.). Utilize automatic sprinkler systems that must be set to irrigate landscaping during early morning hours or during the evening to reduce water losses from evaporation. Sprinklers must also be reset to water less often in cooler months and during the rainfall season so that water is not wasted by excessive landscaping irrigation.

C. Applicant/Permittee must enter into a Landscape Maintenance Agreement with performance securities, in a form approved by the City Attorney, to maintain required landscaping and water-conserving irrigation systems for at least a 3-year period. All expenses associated with the development, review and recordation of the Landscape Maintenance Agreement are the sole responsibility of the Applicant/Permittee.

D. Enter into an agreement with the City regarding compliance monitoring and submit the fees cover full costs of compliance monitoring. All costs associated with the development, review, and execution of the compliance monitoring contract is the sole responsibility of the Applicant/Permittee.

E. Submit a composite utility plan to be approved by the Director of Planning and Environmental Review, or designee. All external/roof mounted mechanical equipment (including HVAC condensers, switch boxes, etc.) must be included on all building plans and designing this equipment to be integrated into the structure and/or screened in its entirety from public view.

F. Provide trash/recycling enclosures that are compatible with the architectural design of the Project, of adequate size for trash and recycling containers (at least 50 square feet), and accessible by tenants and for removal. The trash/recycling areas must be enclosed with a solid wall of sufficient height to screen the area, with a solid gate and a roof, to be maintained in good repair in perpetuity and must be included on Building Project plans. The enclosures are required to be approved by the City Design Review Board (DRB).

DRAFT

G. Construction Monitoring Plan: The Applicant/Permittee, at its sole expense, must retain a City-qualified archaeologist and local Chumash Native American observer to monitor all ground disturbing construction activities. Filed notes generated by the local Chumash Native American observer shall be made available upon request to the extent that the information is not considered confidential under applicable law. A construction Monitoring Treatment Plan shall be developed and implemented to ensure that any new discoveries are adequately recorded, evaluated, and, if significant, mitigated. The Construction Monitoring treatment plan shall describe the following:

   a. Specifications that all ground disturbances shall be monitored by a City-qualified archaeologist and a Chumash Native American observer;

   b. Qualifications and organization of monitoring personnel;

   c. Procedures for notifying the City and other involved or interested parties in case of a new discovery;

   d. Procedures that would be used to record, evaluate, and mitigate new discoveries with minimum of delay;

   e. Procedures that would be followed in case of discovery of disturbed as well as intact human burials and burial -associated artifacts. The City-qualified archaeologist and Chumash Native American observer shall have the authority to temporarily halt or redirect construction in the vicinity of any potentially significant discovery to allow for adequate Phase 3 data recovery recordation, evaluation, and mitigation. Evaluation and mitigation could require additional archaeological testing and data recovery. In the highly unlikely event that isolated human remains are encountered, consultation with the most likely Native American descendant, pursuant to Public Resources Code section 5097.97 and 5097.98, would apply; and

   f. Results of the monitoring program shall be documented in a report after completion of all ground disturbing activities.

A contract for the Construction Monitoring Plan, including identification of the City-qualified archaeologist and Chumash Native American observer, shall be approved by the City prior to the issuance of any Zoning Clearance issued for the project.

The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Zoning Clearance.

H. Discovery of Cultural Resources: If archaeological resources are encountered during grading, work must be stopped immediately or redirected until the City -approved archaeologist and local Chumash Native American observer can

DRAFT

evaluate the significance of the find pursuant to the investigation standards set forth in the City Archaeological Guidelines. The appropriate actions and Reports would be required and funded funded by the Applicant/Permittee.

The identification of the City-qualified archaeologist and Chumash Native American monitor shall be approved by the City prior to the issuance of the Zoning Clearance. The monitors must be onsite during all project excavation, grading or other soil disturbance.

The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Zoning Clearance.

I. Design the buildings to comply with all GMC standards and conditions for the attenuation of interior noise to the satisfaction of the Planning and Environmental Review Director.

57. Prior to construction and ongoing during grading and construction activities, the Applicant/Permittee must:

A. Promptly remove any graffiti at the Project site.

B. Obtain all the necessary approvals, licenses, and permits and pay all of the appropriate fees as required by the City. Before any permit may be issued by the City of Goleta, the Applicant/Permittee must obtain written clearance for each development phase from all Departments/Agencies having conditions or Project approval. Such clearance must indicate that the Applicant/Permittee has satisfied all pre-construction conditions.

C. Secure the construction site with a minimum 6-foot high fence. The fence must be covered with a material approved by the Planning and Environmental Review Director to minimize dust leaving the site.

D. Contact PER compliance monitoring staff at least two weeks prior to commencement of construction activities to schedule an on-site pre-construction meeting to be led by PER compliance monitoring staff and attended by all parties deemed necessary by the City, including the permit issuing planner, grading and/or building inspectors, other agency staff, and key construction personnel: contractors, sub-contractors and contracted monitors among others.

E. Meet the following standards:

i. Prevent construction and/or employee trash from blowing offsite by:

ii. Provide covered receptacles on-site before commencement of any grading or construction activities;

iii. Coordinate trash pick up waste weekly or more frequently as directed by the City;

iv. Designate and provide to the Planning and Environmental Review Director the name and contact information of the Project foreman who will monitor construction trash/waste. Additional covered receptacles must be provided as determined necessary by Planning and Environmental Review Director.

v. Ensure that public sidewalks remain open at all times.

vi. Ensure that all haul trucks, hauling soil, sand, and other loose materials, are either covered or maintain two feet of freeboard.

vii. Reduce NO_X emissions during construction by limiting the operation of heavy duty construction equipment to no more than 5 pieces of equipment at any one time if possible.

viii. Maintain equipment and vehicles engines in good condition and in proper tune as per manufacturer's specifications and per APCD rules to minimize dust emissions.

ix. Use electricity from temporary power poles rather than temporary diesel or gasoline powered generators if possible.

x. Ensure that construction vehicles only use the City's designated Truck Routes to the satisfaction of the Public Works Director. All other routes are prohibited. Construction traffic must be routed away from congested streets.

xi. Configure construction parking to minimize traffic interference to the satisfaction of Public Works Director.

xii. Provide temporary traffic controls during all phases of construction activities to maintain traffic flow (e.g. flag persons) to the satisfaction of the Public Works Director.

xiii. Secure approval of the construction vehicle staging and location of vehicle ingress/egress location and the use of temporary construction driveways from the Public Works Director or designee.

xiv. Use electric equipment if feasible to replace diesel-powered equipment such as booster pumps or generators.

xv. Install catalytic converters on equipment if feasible.

xvi. Equip equipment with two to four -degree engine time retard or pre-combustion chamber engines if feasible.

DRAFT

xvii. Use methanol or natural gas powered mobile equipment and pile drivers instead of diesel equipment if feasible.

58. Prior issuance of a Certificate of Occupancy/Final Building Inspection clearance:

A. Install all required Trash enclosures, landscaping and irrigation, lightings, and building elements in accordance with approved plans.

B. Screen all mechanical equipment in accordance with approved plans

C. Screen all new utility service connections and above-ground mounted equipment such as backflow devices, etc. from public view and/or painted in a soft earth tone color so as to blend in with the Project (red is prohibited) in accordance with approved plans.

D. Pay all adopted impacts fees for the Project including but not limited to Fire, Parks and Recreation, Transportation, Library, Public Administration, Storm Drain, Bicycle and Pedestrian and Affordable Housing Development Fee Program in effect at the time unless previously pad.

E. Remove temporary construction driveway if one is installed.

F. Pay all required development compliance monitoring fees and complete construction in accordance with approved plans including design, colors and materials.

By signing this document, the undersigned certifies that he has read, understood, and agrees to the Project Conditions listed in this document.

_____

Benjamin Williams, Partner of 6491 Calle Real Partners, LLC Date

-End of Development Plan and Conditional Use Permit Conditions -

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Attachment 2

Project Plans

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CALLE REAL MIXED USE

6491 CALLE REAL, GOLETA, CA 93117

VICINITY MAP

PROJECT DESCRIPTION

THE PROPOSED PROJECT IS A MIXED USE DEVELOPMENT INCLUDING WITH 14 ONE-BEDROOM RENTAL APARTMENTS AND ONE COMMERCIAL TENANT SPACE. THE RESIDENTIAL UNITS VARY IN SIZE, RANGING FROM 694 SF TO 979 SF. THE RESIDENTIAL SF TOTALS APPROXIMATELY 13,672 SF AND THE TOTAL COMMERCIAL SF TOTALS 585 SF. EACH RESIDENTIAL UNIT WILL HAVE A PATIO OR BALCONY ASSOCIATED WITH THE UNIT(REFER TO SHEET A2), UNIT PLANS FOR PATIO/BALCONY DIMENSIONS). AN APPROXIMATE 585 SF PATIO IS LOCATED ADJACENT TO THE COMMERCIAL TENANT SPACE. 20 PARKING SPACES ARE PROVIDED ON-SITE. A TRASH ENCLOSURE WILL BE PROVIDED ON-SITE WITHIN THE PARKING AREA. THE PROPOSED PROJECT WILL BE ALL ELECTRIC AND NO GAS CONNECTION IS PROPOSED. THE PROJECT INCLUDES TWO (2) DEED RESTRICTED AFFORDABLE UNITS AND A DENSITY BONUS AND CONCESSIONS PURSUANT TO CAL GOV CODE 65915. THE PROPOSED PROJECT CASE FILES ASSOCIATED WITH THE DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT ARE **22-0005-DP** AND **22-0001-CUP**.

DISCRETIONARY APPROVALS:
BASED ON THE PROPOSED PROGRAM AND DESIGN, THE ANTICIPATED DISCRETIONARY APPROVAL FOR A MIXED USE PROPOSAL INCLUDES:

- MAJOR CONDITIONAL USE PERMIT AND DEVELOPMENT PLAN (PLANNING COMMISSION, APPLEABLE TO CITY COUNCIL)
- DESIGN REVIEW BY CITY'S DESIGN REVIEW BOARD
- DENSITY BONUS AND ELIGIBLE CONCESSIONS

ANTICIPATED CONCESSIONS (PER GOVERNMENT CODE 65915):

1) ELIMINATION OF HEAT ISLAND EFFECT REDUCTION/SHADING REQUIREMENTS, PER GMC 17.38.110.J

WAIVERS (PER GOVERNMENT CODE 65915(e))

1) INCREASE OF THE MAXIMUM HEIGHT TO 37'-2"

PROJECT DIRECTORY

APPLICANT
6491 CALLE REAL PARTNERS, LLC
ADDRESS: 21 EAST CARRILLO STREET, SUITE 270
SANTA BARBARA, CA 93101
CONTACT: JASON JAEGER
EMAIL: JASON@JAEGERPARTNERS.NET
P:805.845.9300

ARCHITECT
RRM DESIGN GROUP
ADDRESS: 10 EAST FIGUEROA ST, STE. 200
SANTA BARBARA, CA 93101
CONTACT: ELIJAH PEARCE
EMAIL: EMPEARCE@RRMDESIGN.COM
P:805.963.8283

LANDSCAPE ARCHITECT
RRM DESIGN GROUP
ADDRESS: 3765 S. HIGUERA ST., SUITE 102
SAN LUIS OBISPO, CA 93401
CONTACT: LANCE WIERSCHEM
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P:805.543.1794

CIVIL ENGINEER
RRM DESIGN GROUP
ADDRESS: 10 EAST FIGUEROA ST, STE. 200
SANTA BARBARA, CA 93101
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EMAIL: MCHAMILTON@RRMDESIGN.COM
P:805.963.8283

PLANNING
RRM DESIGN GROUP
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SANTA BARBARA, CA 93101
CONTACT: TONY TOMASELLO
EMAIL: ACTOMASELLO@RRMDESIGN.COM
P:805.883.5204

ELECTRICAL ENGINEER
JMPE ELECTRICAL ENGINEERING
ADDRESS: 627 OLIVE STREET
SANTA BARBARA, CA 93101
CONTACT: JOHN MALONEY
EMAIL: MALONEY@JMPE.NET
P:805.569.9216

PROJECT DATA

PROJECT ADDRESS:	6491 CALLE REAL, GOLETA, CA 93117
APN:	077-160-066
ZONING:	OI (OFFICE/INSTITUTIONAL)
LAND USE:	OI (OFFICE/INSTITUTIONAL)
PARCEL SIZE:	23,019 SF (0.53 ACRES)
EXISTING USE:	VACANT
PROPOSED USE:	MIXED USE
ALLOWED DENSITY:	20 DU/AC; 10.6 UNITS
PROPOSED DENSITY:	26.4 DU/AC; 14 UNITS WITH DENSITY BONUS

REQUIRED SITE AREAS	QUANTITY(SF)	% OF NET
MAX LOT BUILDING COVERAGE:		40%
LANDSCAPING REQUIRED:		10%
REQUIRED PRIVATE OPEN SPACE: *GROUND FLOOR UNITS: MIN. 200 SF/UNIT* *UPPER FLOOR UNITS: MIN. 60 SF/UNIT*		
REQUIRED COMMON OPEN SPACE:	1,400 SF (100 SF PER UNIT x 14 UNITS)	

PROPOSED SITE AREAS	QUANTITY (SF)	% OF NET
BUILDING FOOTPRINT:	5,048 SF	21%
PERVIOUS LANDSCAPE:	5,110 SF	22%
HARDSCAPE:	10,797 SF	46.9%

*REFERENCE SHEET L1 FOR FURTHER DETAILS

PROVIDED PRIVATE OPEN SPACE: 1,466 SF
GROUND FLOOR UNITS: 200 SF/UNIT
UPPER FLOOR UNITS: 81 SF/UNIT

*REFERENCE SHEET A.06 FOR UNIT PLANS

PROVIDED COMMON OPEN SPACE: 3,454 SF
*REFERENCE SHEET A.02 FOR SITE PLAN

MAX. ALLOWED HEIGHT: 35'-0"

MAX. PROPOSED HEIGHT: 37'-2"

BUILDING SETBACKS:	REQUIRED	PROPOSED
FRONT (CALLE REAL) :	15'-0"	26'-11"
INTERIOR SIDE(EAST):	15'-0"	74'-10"
STREET SIDE (LOS CARNEROS):	15'-0"	21'-6"
REAR(SOUTH):	15'-0"	15'-0"

*REFERENCE SHEET A.02 FOR FURTHER DETAILS

PARKING DATA

TOTAL PARKING REQUIRED: 16 UNCOVERED
TOTAL PARKING PROVIDED: 20 UNCOVERED

(1) COMMERCIAL OFFICE ADA SPACE
(1) RESIDENTIAL ADA SPACE
(1) FUTURE ADA EV SPACE
(1) FUTURE EV STANDARD SPACE

TOTAL BIKE PARKING REQUIRED: 6 SHORT TERM/14 LONG TERM
TOTAL BIKE PARKING PROVIDED: 10 SHORT TERM/16 LONG TERM

REFERENCE SHEET A.02 SITE PLAN FOR FURTHER DETAILS

UNIT MIX

UNIT BREAKDOWN:

(6) - 1 BEDROOM UNITS 694 SF EACH (UNIT TYPE A)
(2) - 1 BEDROOM UNITS 741 SF EACH (UNIT TYPE B)
(6) - 1 BEDROOM UNITS 979 SF EACH (UNIT TYPE C)

14 RESIDENTIAL UNITS TOTAL

FLOOR AREA CALCS

FLOOR AREA BREAKDOWN:

RESIDENTIAL UNITS:	11,520	SF
RESIDENTIAL COMMON SPACE:	2,152	SF
COMMERCIAL OFFICE:	585	SF
TOTAL FLOOR AREA SF:	**14,257**	**SF**
FIRE RISER CLOSET & ELEC. RM:	215	SF*
FLOOR	**SQ FT**	
GROUND FLOOR	5,013	SF
SECOND FLOOR	5,319	SF
THIRD FLOOR	3,925	SF
TOTAL SF:	**14,257**	**SF**

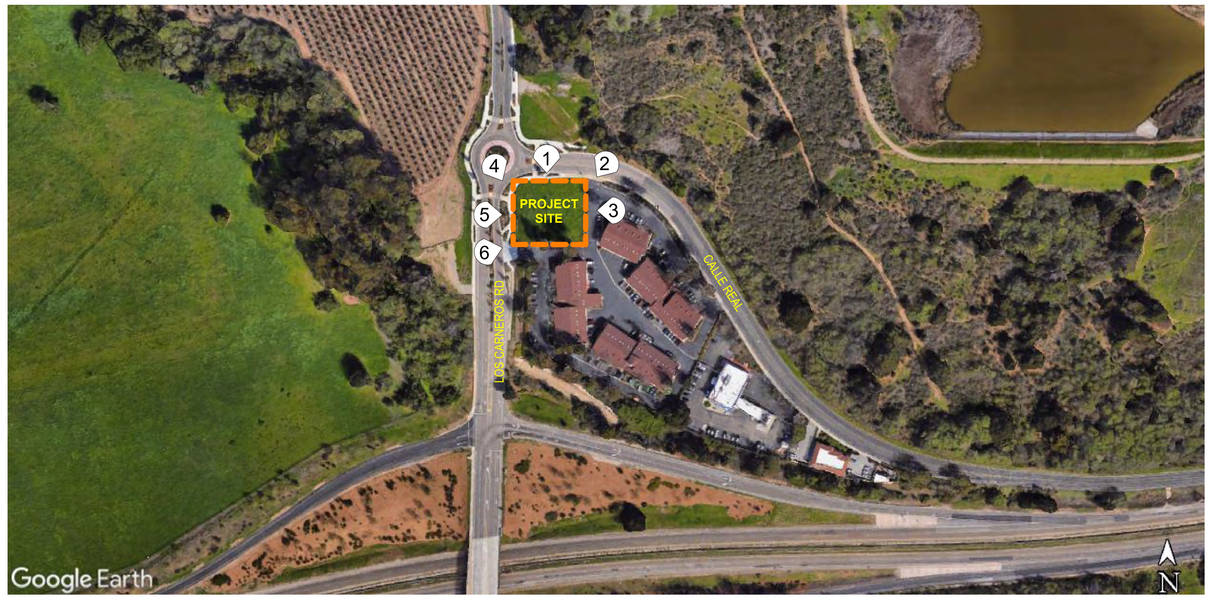
FLOOR AREA CALCS BASED ON RULES OF MEASUREMENT PER GOLETA ZONING CODE 17.03.070.

*NOTE: THIS SQUARE FOOTAGE IS EXCLUDED FROM THE FLOOR AREA PER GOLETA ZONING CODE 17.03.070B

SHEET INDEX

A.00	TITLE SHEET
A.01	EXISTING SITE PHOTOS
A.02	SITE PLAN
A.03	FLOOR PLANS
A.04	FLOOR & ROOF PLAN
A.05	SCHEMATIC ELEVATIONS
A.06	TYPICAL UNIT PLANS
A.07	COLOR & MATERIALS
A.08	PERSPECTIVE VIEW
A.09	STREET PERSPECTIVE VIEW
A.10	VIEW CORRIDOR PHOTOSIM
C1	PRELIMINARY IMPROVEMENTS PLAN
C2	PRELIMINARY SECTIONS AND DETAILS
C3	EROSION AND SEDIMENT CONTROL PLAN
C4	EROSION AND SEDIMENT CONTROL DETAILS
L1	LANDSCAPE PLAN
SL-1	SITE PHOTOMETRICS PLAN
SL-2	DATA SHEETS AND FIXTURES SCHEDULE
1	SURVEY

TOTAL SHEETS: 19



PROJECT SITE

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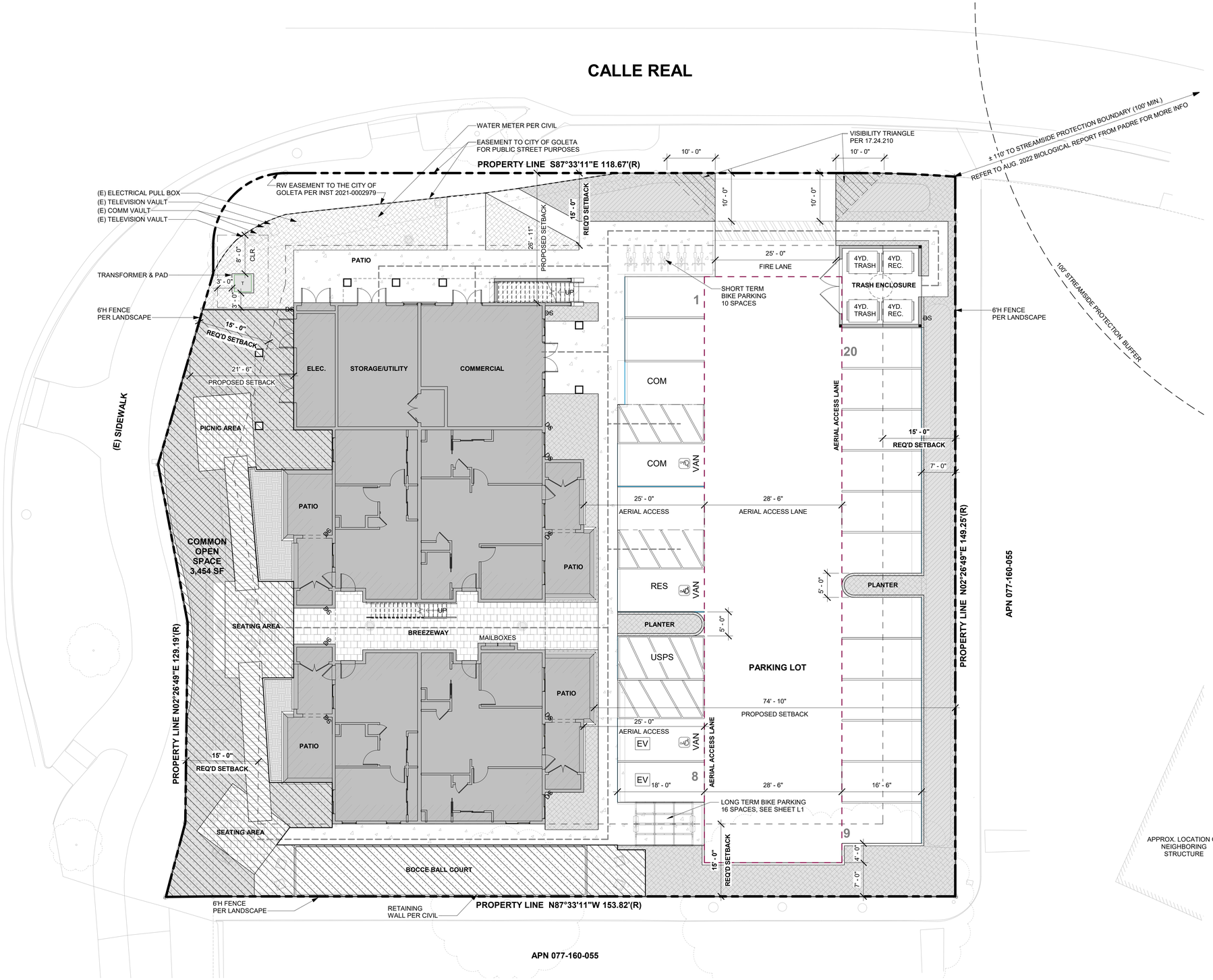
CALLE REAL MIXED USE (22-0005-DP & 22-0001-CUP)

6491 CALLE REAL, GOLETA, CA 93117

EXISTING SITE PHOTOS A.01

11/30/2022



PARKING DATA

TOTAL PARKING REQUIRED: 16 UNCOVERED*

TOTAL PARKING PROVIDED: 20 UNCOVERED

VISITOR PARKING FACTORED INTO DENSITY BONUS PARKING RATIOS

*REQUIRED RESIDENTIAL PARKING PER STATE DENSITY BONUS CODE CAL GOV CODE 65915 (p) (1)

RESIDENTIAL:
PARKING REQUIRED: 14 UNCOVERED*
(14) 1-BEDROOM UNITS: 14 SPACES
ADA STALLS REQUIRED: 1 TOTAL (1 VAN)
FUTURE EV STALLS REQUIRED: 1 VAN STALL, 1 STANDARD (2 TOTAL)

PARKING PROVIDED: 18 UNCOVERED
ADA STALLS PROVIDED: 1 TOTAL (1 VAN)
FUTURE EV STALLS PROVIDED: 1 VAN STALL, 1 STANDARD (2 TOTAL)

COMMERCIAL:
PARKING REQUIRED: 2 UNCOVERED
1 SPACE PER 300 SF
ADA STALLS REQUIRED: 1 TOTAL (1 VAN)

PARKING PROVIDED: 2 UNCOVERED
ADA STALLS PROVIDED: 1 TOTAL (1 VAN)

BICYCLE PARKING:
SHORT TERM REQUIRED: 6 SPACES (3 RES. / 3 COMM.)
**MIN. 10% OF REQUIRED AUTOMOBILE SPACES WITH A MIN. OF 3 SPACES PER ESTABLISHMENT*

SHORT TERM PROVIDED: 10 SPACES

LONG TERM REQUIRED: 14 SPACES (50% COVERED)
**MINIMUM OF 1 LONG TERM SPACE PER RESIDENTIAL UNIT*

LONG TERM PROVIDED: 16 COVERED SPACES

PROJECT TREES

TOTAL EXISTING TREES: 0 ON PROJECT SITE

TOTAL REQUIRED TREES: 5 REQUIRED
1 TREE PER 4 PARKING SPACES

TOTAL PROPOSED TREES: 11 PROPOSED
REFER TO LANDSCAPE SHEET L1 FOR MORE INFORMATION

SITE ACCESSIBILITY NOTES

ACCESSIBLE PATH OF TRAVEL:
1. THE ACCESSIBLE ROUTES OF TRAVEL AS INDICATED ON PLAN IS A BARRIER FREE ACCESS ROUTE WITHOUT ANY ABRUPT LEVEL CHANGES, SURFACE IS STABLE AND SLIP RESISTANT. THIS PATH SHALL BE MAINTAINED FREE OF OVERHANGING OBSTRUCTIONS TO 80" MIN AND PROTRUDING OBJECTS GREATER THAN A 4" PROJECTION FROM WALL AND ABOVE 27" AND LESS THAN 80" [2019 CBC 11B-402]

2. ACCESSIBLE ROUTES OF TRAVEL ARE TO AND FROM THE FOLLOWING: "SITE ARRIVAL POINTS" ON RAYMOND AVE. AND PACKER PLACE, RESIDENTIAL ENTRANCES TO UNIT, ACCESSIBLE PARKING STALLS,TRASH ENCLOSURE, EV-A CHARGING STALL, BICYCLE PARKING, TOT LOT, AND COMMUNITY CENTER.

3. FLOOR AND GROUND SURFACES: FLOOR AND GROUND SURFACES SHALL BE STABLE, FIRM, AND SLIP RESISTANT. [2019 CBC 11B-302.1]

4. CHANGES IN LEVELS: VERTICAL CHANGES IN LEVEL FOR FLOOR OR GROUND SURFACES MAY BE ¼ INCH HIGH MAXIMUM AND WITHOUT EDGE TREATMENT. CHANGES IN LEVEL GREATER THAN ¼ INCH AND NOT EXCEEDING ½ INCH IN HEIGHT SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 1:2. [2019 CBC 11B-303, FIGURES 11B-303.2 & 11B-303.3]

5. PROTRUDING OBJECTS: PROTRUDING OBJECTS ON CIRCULATION PATHS SHALL COMPLY WITH CBC 11B-307. OBJECTS WITH LEADING EDGES MORE THAN 27" AND NOT MORE THAN 80" AFF OR GROUND SHALL PROTRUDE 4" MAXIMUM HORIZONTALLY INTO THE CIRCULATION PATH. HANDRAILS SHALL BE PERMITTED TO PROTRUDE 4 1/2" MAXIMUM. [2019 CBC11B-307.2]

6. FREE STANDING OBJECTS MOUNTED ON POSTS SHALL OVERHANG CIRCULATION PATHS 12" MAX WHEN LOCATED 27" MIN AND 80" MAX AFF. WHERE A SIGN OR OBSTRUCTION IS MOUNTED BETWEEN POSTS AND THE CLEAR DISTANCE BETWEEN POSTS IS GREATER THAN 12", THE LOWEST EDGE OF SUCH SIGN OR OBSTRUCTION SHALL BE 27" MAX OR 80" MIN AFF.

SITE PLAN LEGEND

- PROPERTY LINE
- SETBACK
- EASEMENT
- ACCESSIBLE PATH OF TRAVEL (SHALL BE 48" MIN. CBC 11B-403.5)
- STANDARD ACCESSIBLE STALL
- VAN — VAN ACCESSIBLE STALL
- EV — FUTURE ELECTRIC VEHICLE CHARGING STALL
- COMMON OPEN SPACE AREA

NORTH

0' 5' 10' 20'

1 SITE PLAN
SCALE: 1" = 10'-0"

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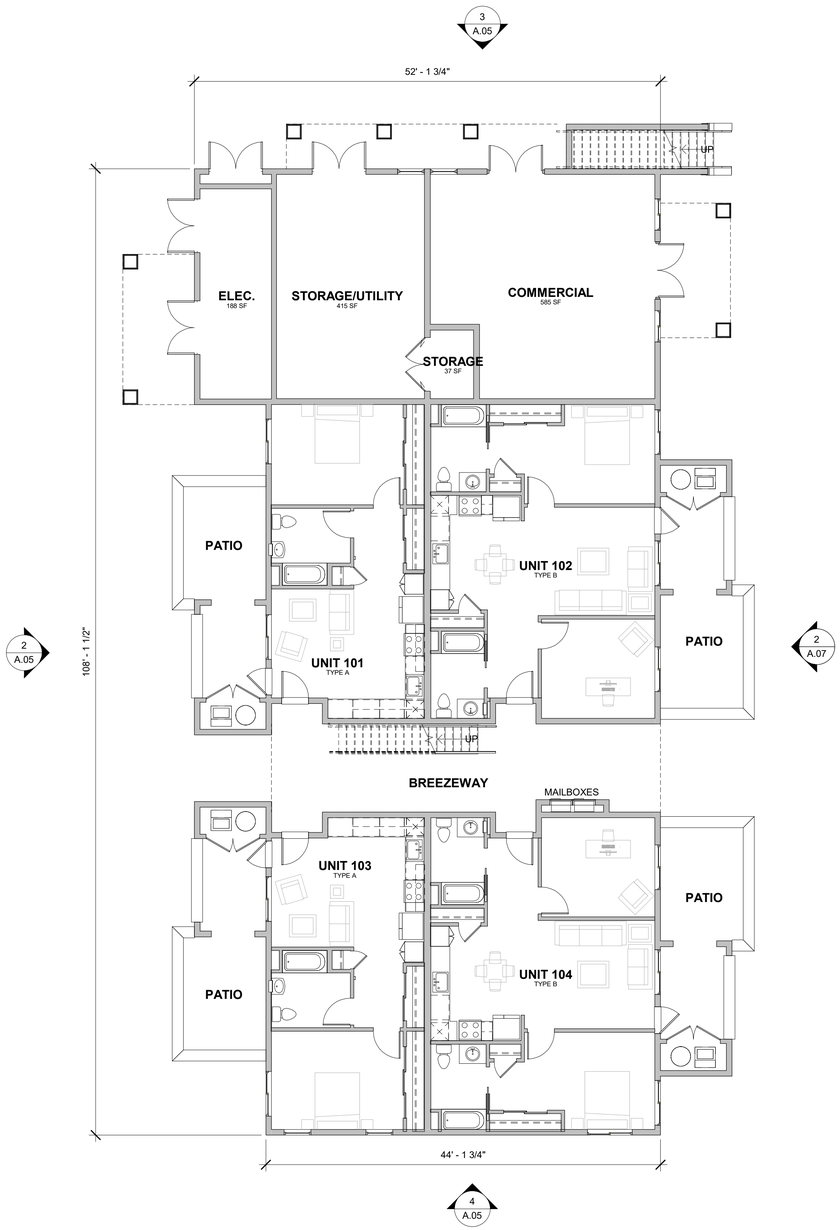
CALLE REAL MIXED USE (22-0005-DP & 22-0001-CUP)

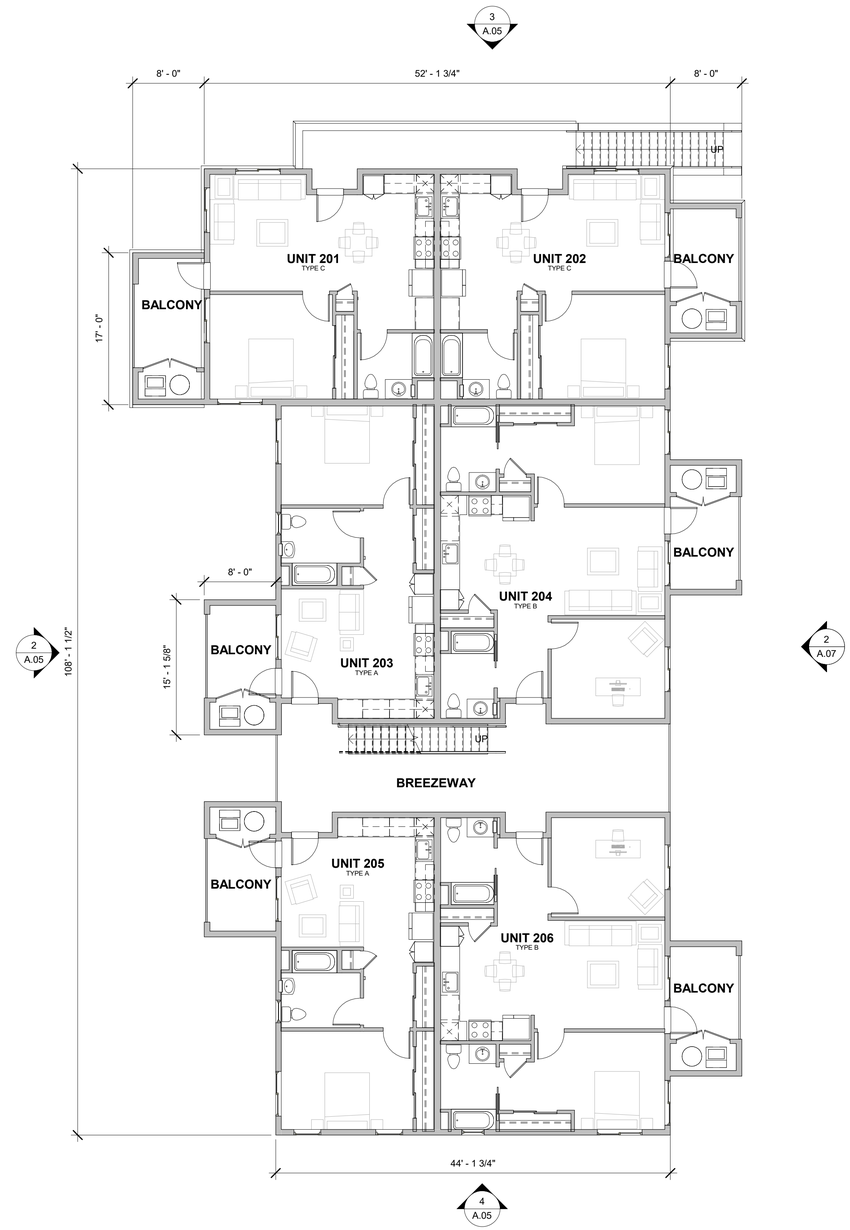
6491 CALLE REAL, GOLETA, CA 93117

SITE PLAN A.02

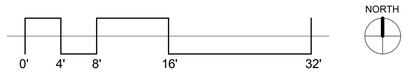
01/17/2023



1 GROUND FLOOR PLAN
SCALE: 1/8" = 1'-0"



2 SECOND FLOOR PLAN
SCALE: 1/8" = 1'-0"



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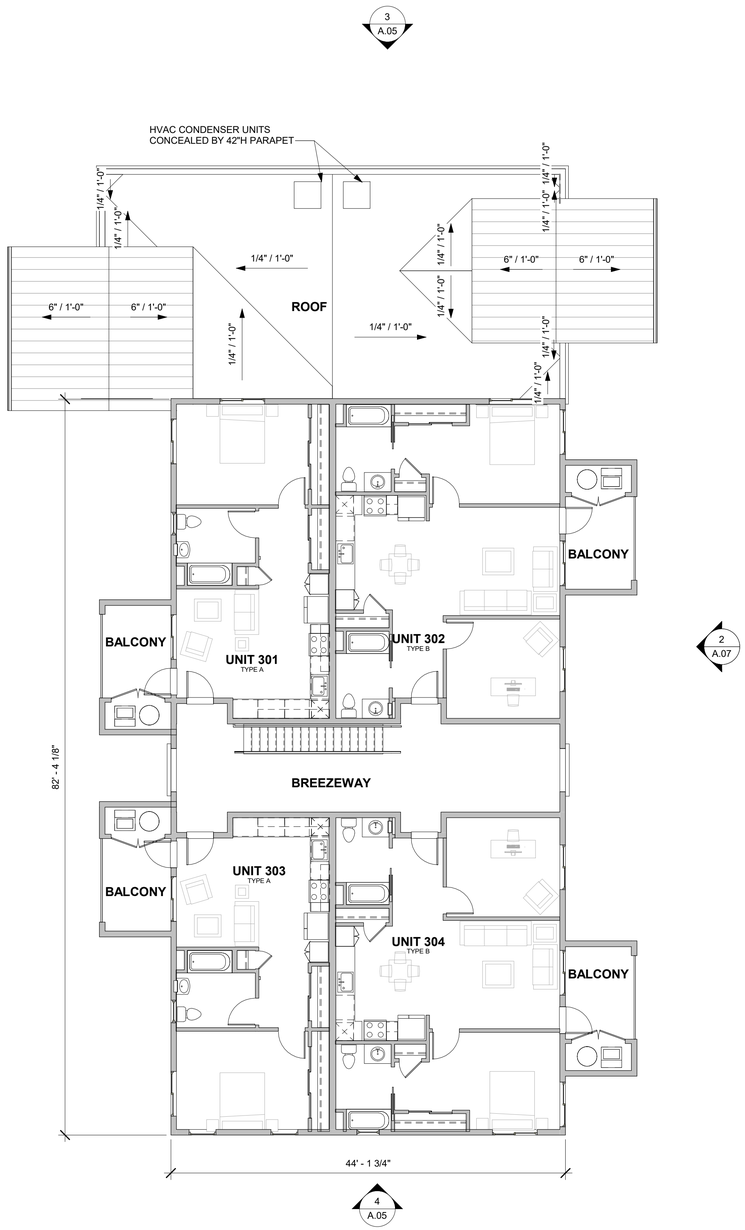
CALLE REAL MIXED USE (22-0005-DP & 22-0001-CUP)

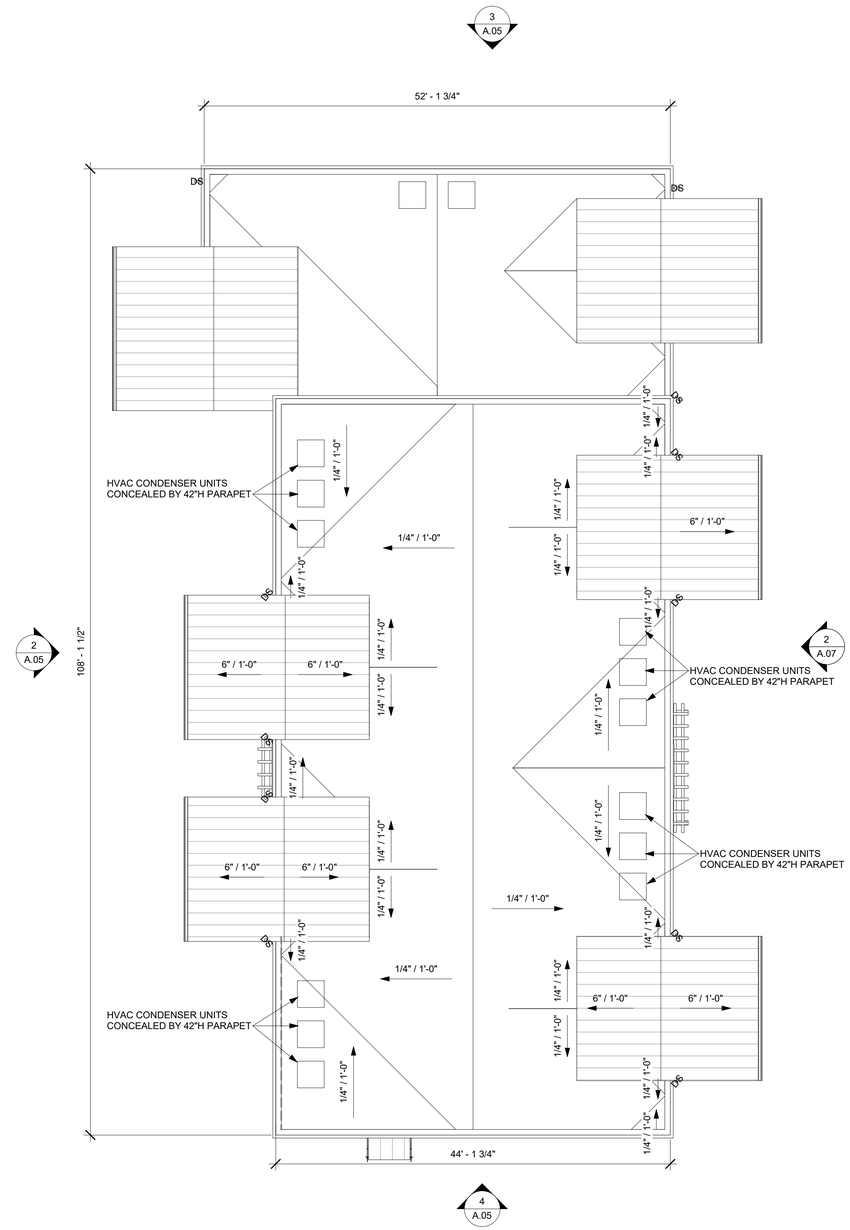
6491 CALLE REAL, GOLETA, CA 93117

FLOOR PLANS A.03

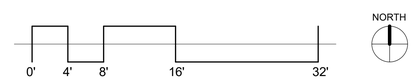
11/30/2022



1 THIRD FLOOR PLAN
SCALE: 1/8" = 1'-0"



2 ROOF PLAN
SCALE: 1/8" = 1'-0"



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CALLE REAL MIXED USE (22-0005-DP & 22-0001-CUP)

6491 CALLE REAL, GOLETA, CA 93117

FLOOR & ROOF PLAN A.04

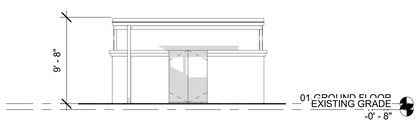
11/30/2022

1 **EAST ELEVATION**
SCALE: 1/8" = 1'-0"

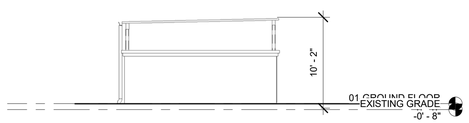
2 **WEST ELEVATION**
SCALE: 1/8" = 1'-0"

3 **NORTH ELEVATION**
SCALE: 1/8" = 1'-0"

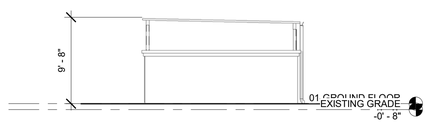
4 **SOUTH ELEVATION**
SCALE: 1/8" = 1'-0"



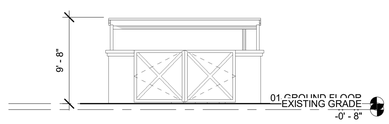
5 **TRASH EAST ELEVATION**
SCALE: 1/8" = 1'-0"



6 **TRASH NORTH ELEVATION**
SCALE: 1/8" = 1'-0"



7 **TRASH SOUTH ELEVATION**
SCALE: 1/8" = 1'-0"



8 **TRASH WEST ELEVATION**
SCALE: 1/8" = 1'-0"

GENERAL NOTES

1. REFER TO SHEET A.6 FOR COLOR & MATERIAL BOARD

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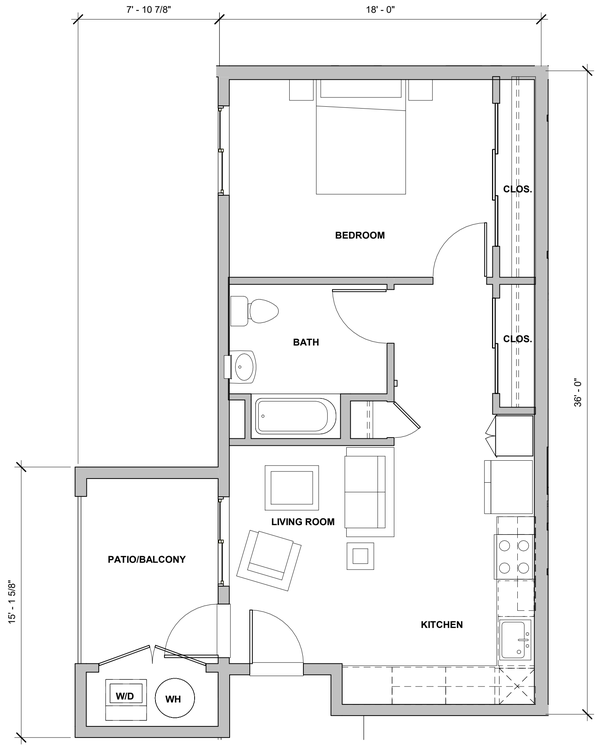
CALLE REAL MIXED USE (22-0005-DP & 22-0001-CUP)

6491 CALLE REAL, GOLETA, CA 93117

SCHEMATIC ELEVATIONS A.05

11/30/2022

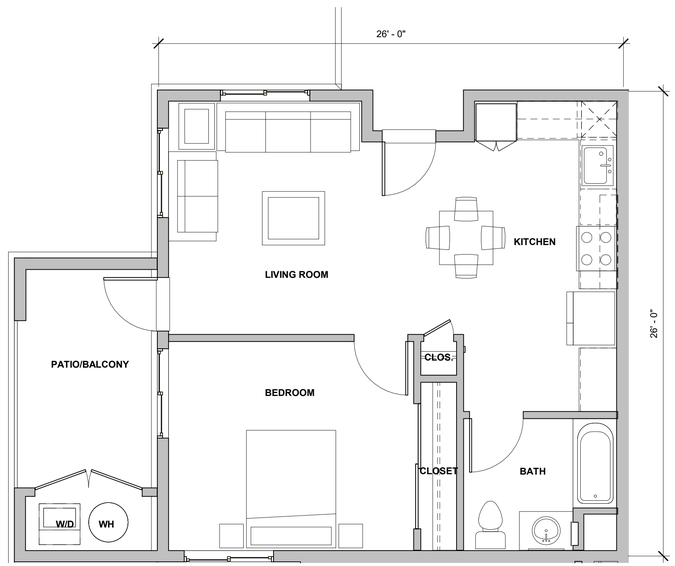


1 1 BEDROOM UNIT - TYPE A (694 SF)

SCALE: 1/4" = 1'-0"

PATIO / BALCONY: GROUND FLOOR (200 SF)

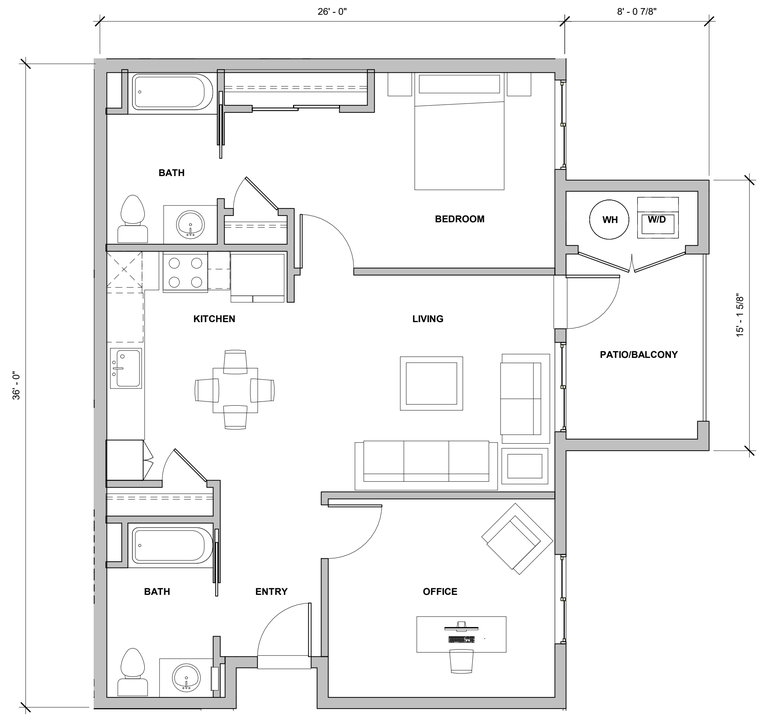
PATIO / BALCONY: SECOND & THIRD FLOOR (81 SF)



2 1 BEDROOM UNIT - TYPE B (741 SF)

SCALE: 1/4" = 1'-0"

PATIO / BALCONY: SECOND & THIRD FLOOR (81 SF)

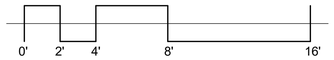


3 1 BEDROOM UNIT - TYPE C (979 SF)

SCALE: 1/4" = 1'-0"

PATIO / BALCONY: GROUND FLOOR (200 SF)

PATIO / BALCONY: SECOND & THIRD FLOOR (81 SF)



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TYPICAL UNIT PLANS A.06

07/05/2023

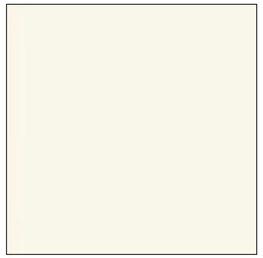
A

STANDING SEAM ROOFING
WALNUT-CREEK METALS
DARK BRONZE

B

STUCCO (1) & TRIM
STEEL TROWEL FINISH
OMEGA 432 MILKY QUARTZ

C



BOARD & BATTEN
LP SIDING SMOOTH
MATCH DUNN EDWARDS
SWISS COFFEE DEW341

D

STONE CLADDING
CULTURED STONE
COBBLEFIELD - GRAY

E

WINDOW
MILGUARD - VINYL
BRONZE

F

EXTERIOR DOOR
DUNN EDWARDS
RED REVIVAL DEA154

G

LIGHTING FIXTURE*
HINKLEY LIGHTING
FOUNDRY# 10443TK

H

WROUGHT IRON
PAINTED DUNN EDWARDS
WEATHERED BROWN DEW344

***NOTE:** LIGHTING FIXTURES WILL BE DARK SKY COMPLIANT AND WILL COMPLY WITH GMC CHAPTER 17.35.

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COLOR & MATERIALS A.07

11/30/2022

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CALLE REAL MIXED USE (22-0005-DP & 22-0001-CUP)

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PERSPECTIVE VIEW A.08

11/30/2022

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STREET PERSPECTIVE VIEW **A.09**

11/30/2022

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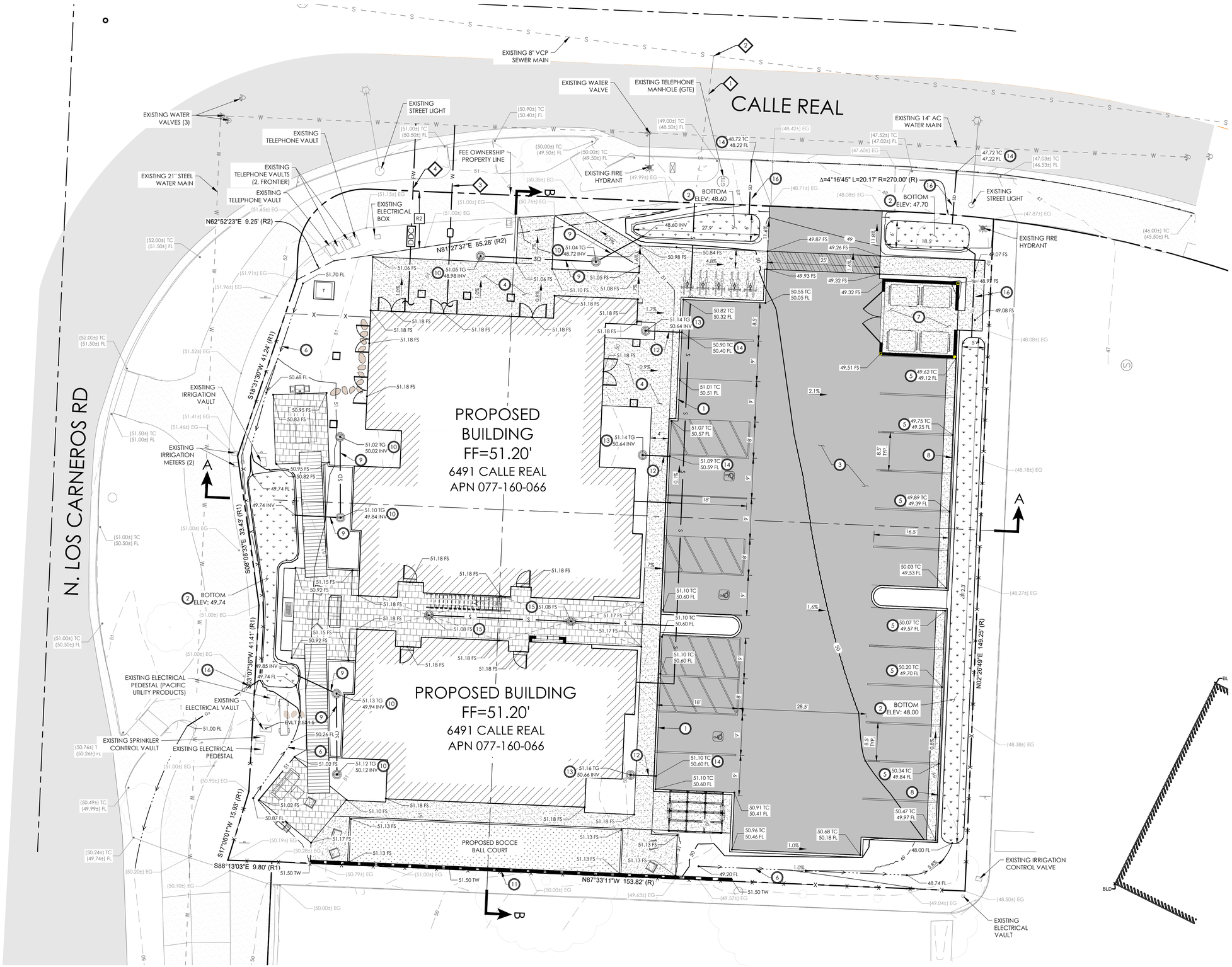
VIEW CORRIDOR PHOTOSIM **A.10**

11/30/2022



GENERAL NOTES:

1. SITE GRADING SHALL CONFORM TO THE RECOMMENDATIONS MADE BY THE GEOTECHNICAL ENGINEER IN THE APPROVED SOILS REPORT.
2. EXTERIOR FINISH GRADES SHALL SLOPE AWAY FROM STRUCTURE AT 2% MINIMUM.

EARTH QUANTITIES

AREA OF DISTURBANCE 0.53 AC±

RAW CUT:	140 CY	OVEREXCAVATION:	800 CY
RAW FILL:	130 CY	RECOMPACTION:	1,500 CY

QUANTITY ESTIMATES ON THESE PLANS ARE TO BE USED FOR PERMIT PURPOSES ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ACTUAL QUANTITIES FOR THE PURPOSE OF CONSTRUCTION.

THE RAW EARTHWORK QUANTITIES SHOWN HEREON REPRESENT THE ESTIMATED VOLUMETRIC DIFFERENCE BETWEEN THE PROPOSED ROUGH GRADE AND THE LIMITED TOPOGRAPHIC EXISTING GRADES. THESE ESTIMATES DO NOT MAKE CONSIDERATIONS FOR LOSSES OR BULKING DUE TO: SHRINKAGE, SOIL AMENDMENTS, STABILIZATION, CONSTRUCTION TECHNIQUE, FOOTING & TRENCHING SPOILS, ETC. THESE, IN ADDITION TO ACTUAL FIELD CONDITIONS, CONSTRUCTION TECHNIQUE AND THE FINAL RECOMMENDATIONS OF THE SOILS ENGINEER MAY SIGNIFICANTLY EFFECT THE FINAL IMPORT/EXPORT QUANTITIES.

LEGEND

- PROPERTY LINE/R.O.W.
- 250 — PROPOSED CONT. MAJOR
- 251 — PROPOSED CONT. MINOR
- 250 — EXISTING CONT. MAJOR
- 251 — EXISTING CONT. MINOR
- SD — EXISTING STORM DRAIN
- SD — PROPOSED STORM DRAIN
- PROPOSED INLET
- FLOWLINE
- EXISTING A.C. PAVEMENT
- EXISTING CONCRETE PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- PROPOSED A.C PAVEMENT
- PROPOSED BIORETENTION
- 73.30 FS — PROPOSED SPOT GRADE
- (73.30 FS) — EXISTING SPOT GRADE
- S — PROPOSED SEWER LINE
- W — PROPOSED WATER LINE
- FW — PROPOSED FIRE WATER LINE
- S — EXISTING SEWER LINE
- W — EXISTING WATER LINE
- ROW EASEMENT LINE
- (R) PM 10717 BK 03 PG 42
- (R1) INST 2021-0002978 RW VACATION
- (R2) INST 2021-0002979 PUBLIC RW EASEMENT TO CITY OF GOLETA

CONSTRUCTION NOTES:

1. PROPOSED 6" CONCRETE CURB ONLY PER COUNTY OF SANTA BARBARA STANDARD DETAIL H-02.0.
2. PROPOSED BIORETENTION FACILITY PER DETAIL C, SHEET C2.
3. PROPOSED 4" ASPHALT CONCRETE PAVEMENT WITH 8" AGGREGATE BASE.
4. PROPOSED 4" CONCRETE PAVEMENT.
5. PROPOSED CURB CUT PER DETAIL E, SHEET C2.
6. PROPOSED EARTHEN SWALE PER DETAIL D, SHEET C2.
7. PROPOSED TRASH ENCLOSURE PER ARCHITECTURE. CONCRETE SECTION WILL CONSIST OF 4" HICK CALTRANS CLASS 2 BASE LAYER, 6" THICK REINFORCED CONCRETE LAYER WITH CONCRETE HAVING A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 3,000 PSI, AND REINFORCEMENT CONSISTING OF #4 BARS AT 18-INCH SPACING EACH WAY SUSPENDED IN THE MIDDLE OF THE SLAB WITH CHAIRS OR OTHER APPROVED DEVICES PER GEOTECHNICAL REPORT.
8. PROPOSED 6" CONCRETE CURB AND 24" GUTTER PER COUNTY OF SANTA BARBARA STANDARD DETAIL H-02.0. DETAIL MODIFIED TO PROVIDE DEEPENED RETAINING CURB.
9. PROPOSED 6" SDR35 PVC DRAIN PIPE.
10. PROPOSED 6" DOME GRATE.
11. PROPOSED RETAINING CURB/GARDEN WALL.
12. PROPOSED 4" SDR35 PVC DRAIN PIPE.
13. PROPOSED 4" DOME GRATE.
14. OUTLET THROUGH FACE OF CURB.
15. PROPOSED 6" GRATE
16. PROPOSED 3" SDR35 PVC DRAIN PIPE.

UTILITY CONSTRUCTION NOTES:

1. EXISTING 4" SEWER LATERAL TO BE REPLACED WITH 6" LATERAL FOR PROPOSED CONSTRUCTION.
2. EXISTING 8" X 4" SEWER WYE TO BE REPLACED WITH 8" X 6" WYE FOR PROPOSED CONSTRUCTION.
3. PROPOSED 2-INCH, TYPE K, COPPER WATER LATERAL CONNECTION PER GOLETA WATER DISTRICT STANDARD DETAIL 3-03 WITH MASTER METER PER GOLETA WATER DISTRICT STANDARD DETAIL 3-05. SEE PLUMBING PLANS FOR CONTINUATION.
4. PROPOSED 6-INCH, TYPE K, COPPER FIRE WATER LINE WITH 6-INCH DCDA PER GOLETA WATER DISTRICT STANDARD DETAIL 4-05.

0 10 20 30 feet
1"=10'

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PRELIMINARY IMPROVEMENTS PLAN

JANUARY 17, 2023

6491 CALLE REAL

C1

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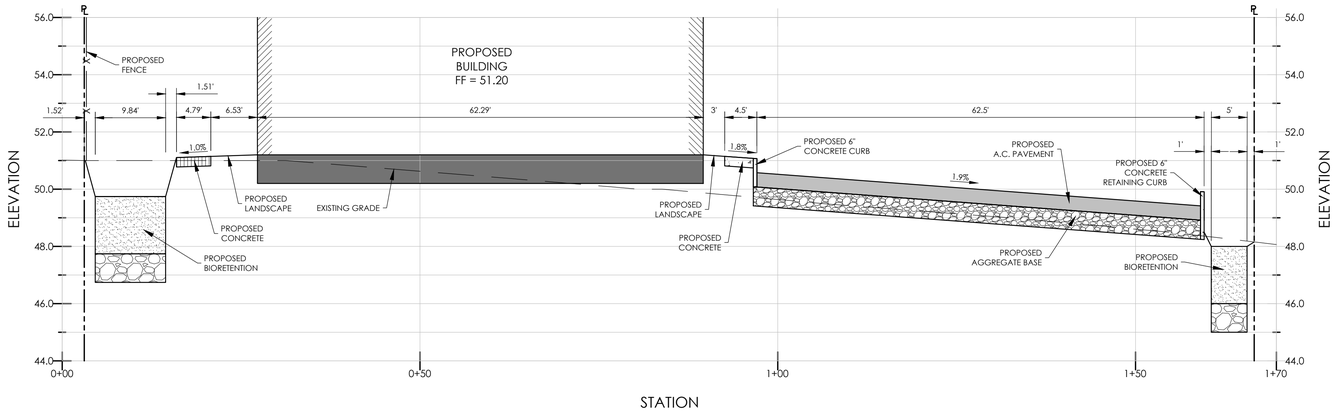
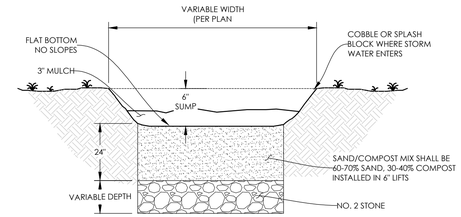
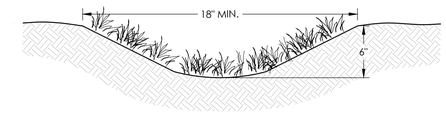


A-A SITE SECTION
SCALE: H: 1"=2.5' V:1"=10'



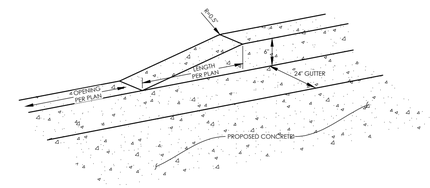
NOTES:
1. BIORETENTION SOIL MIX SHALL BE 60 TO 70% SAND, 15 TO 25% COMPOST, AND 10 TO 20% CLEAN TOPSOIL. THE ORGANIC CONTENT SHALL BE 8 TO 12% AND PH RANGE SHALL BE 5.5 TO 7.5.
2. STONE MATERIAL SHALL BE WASHED, OPEN-GRADED (NO SAND), CRUSHED (ANGULAR) AGGREGATE.
3. NATIVE SOIL BENEATH INFILTRATION BED SHALL BE OVER-EXCAVATED A MINIMUM OF 1.0' AND REPLACED UNIFORMLY WITHOUT COMPACTION.

C BIORETENTION CROSS-SECTION
SCALE: N.T.S.

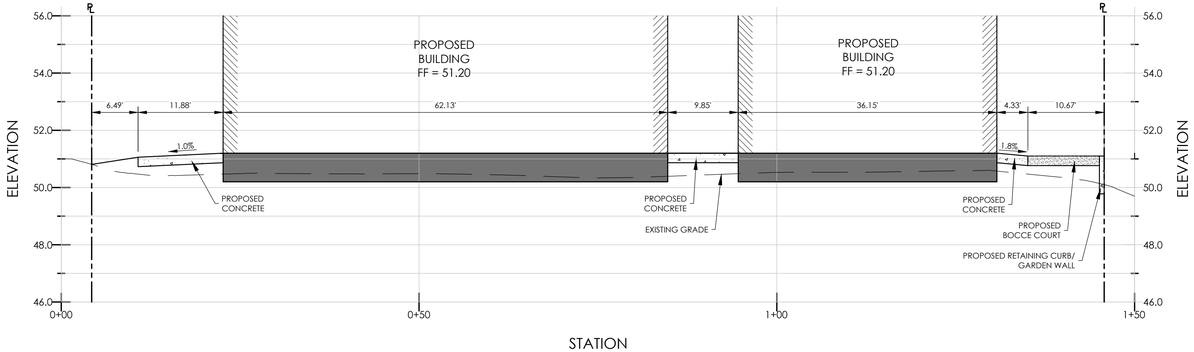


NOTES:
1. SOIL AMENDMENTS SHALL BE AMENDED WITH 2 INCHES OF WELL-ROTTED COMPOST, UNLESS THE ORGANIC CONTENT IS ALREADY GREATER THAN 10%. THE COMPOST SHALL BE MIXED INTO THE NATIVE SOILS TO A DEPTH OF 6 INCHES TO PREVENT SOIL LAYERING AND WASHOUT OF COMPOST. THE COMPOST WILL CONTAIN NO SAWDUST, GREEN OR UNDER-COMPOSTED MATERIAL, OR ANY OTHER TOXIC OR HARMFUL SUBSTANCE. IT SHALL CONTAIN NO UN-STERILIZED MANURE, WHICH CAN LEAD TO HIGH LEVELS OF PATHOGEN INDICTORS (COLIFORM BACTERIA) IN THE RUNOFF.

D EARTHEN SWALE CROSS-SECTION
SCALE: N.T.S.



E TRANSITION TO 0" CURB OPENING
SCALE: N.T.S.



B-B SITE SECTION
SCALE: H: 1"=2.5' V:1"=10'

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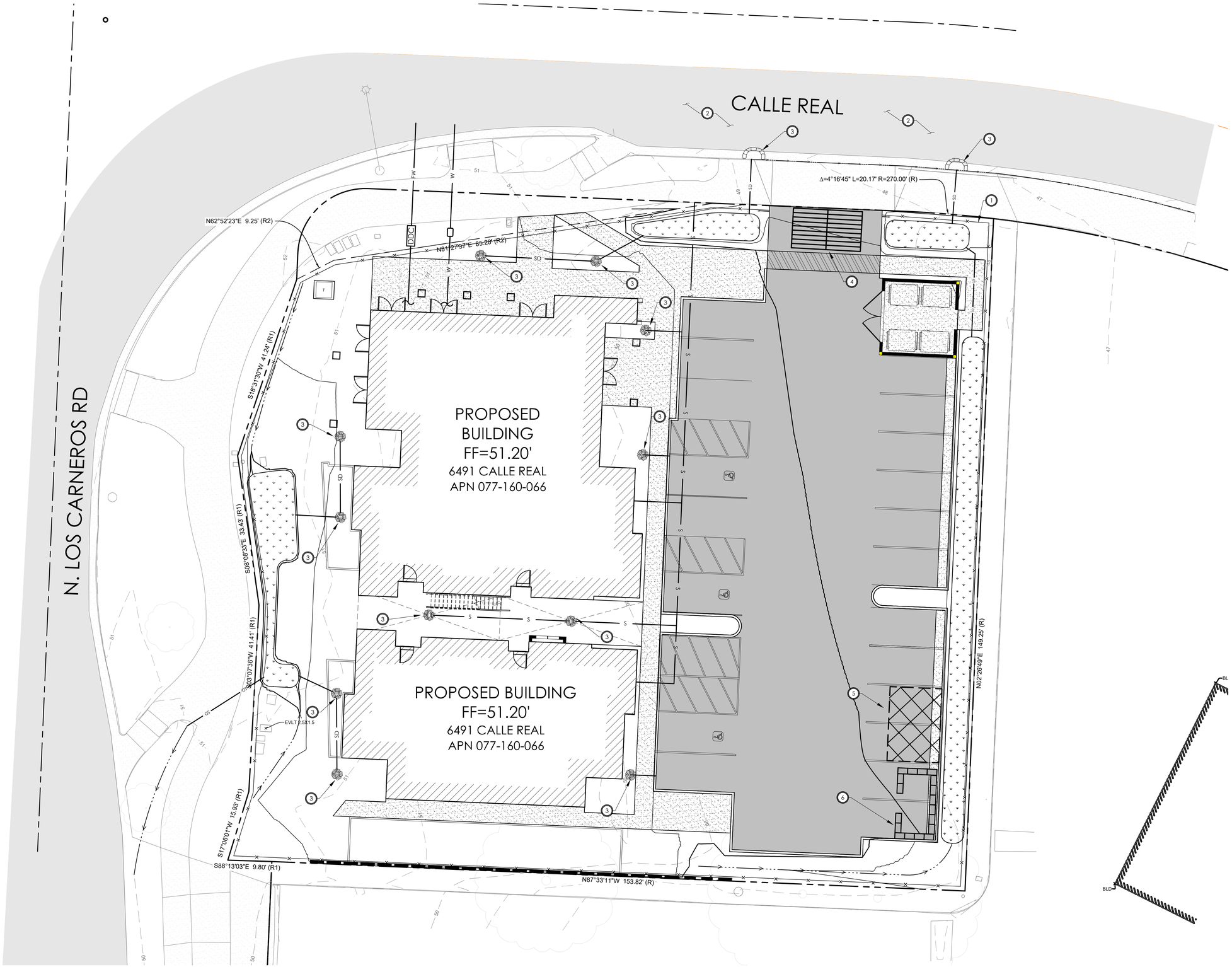
PRELIMINARY SECTIONS AND DETAILS

JANUARY 17, 2023

6491 CALLE REAL

C2



LEGEND

PROPERTY LINE/R.O.W.	
250	PROPOSED CONT. MAJOR
251	PROPOSED CONT. MINOR
250	EXISTING CONT. MAJOR
251	EXISTING CONT. MINOR
SD	EXISTING STORM DRAIN
SD	PROPOSED STORM DRAIN
	PROPOSED INLET
	FLOWLINE
	EXISTING A.C. PAVEMENT
	EXISTING CONCRETE PAVEMENT
	PROPOSED CONCRETE PAVEMENT
	PROPOSED A.C PAVEMENT
	PROPOSED BIORETENTION
73.30 FS	PROPOSED SPOT GRADE
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S	PROPOSED SEWER LINE
W	PROPOSED WATER LINE
FW	PROPOSED FIRE WATER LINE
S	EXISTING SEWER LINE
W	EXISTING WATER LINE
	ROW EASEMENT LINE

STORM WATER POLLUTION PREVENTION BMP'S

	BMP*	NAME	SYMBOL
1	SE-1	SILT FENCE	
2	SE-7	STREET SWEEPING & VACUUMING	
3	SE-10	STORM DRAIN INLET PROTECTION	
4	TC-1	STABILIZED CONSTRUCTION ENTRANCE	
5	WM-1	MATERIAL DELIVERY & STORAGE AREA CONSTRUCTION STAGING AREA	
6	WM-8	CONCRETE WASTE MANAGEMENT	

* REFERS TO BMP DESIGNATION GIVEN IN THE CASQA STORMWATER BEST MANAGEMENT PRACTICE CONSTRUCTION HANDBOOK. SEE HANDBOOK FOR BMP DETAILS AND IMPLEMENTATION STRATEGIES.

SITE SPECIFIC EROSION CONTROL NOTES:

1. PERIMETER CONTROL BMP'S AND STABILIZED CONSTRUCTION ENTRANCES SHALL BE IN PLACE PRIOR TO ANY GROUND DISTURBANCE.
2. THESE PLANS ARE INTENDED TO REPRESENT DIFFERENT PHASES DURING CONSTRUCTION. THE CONTRACTOR SHALL IMPLEMENT THE BMP'S SHOWN AND/OR ANY OTHER MEASURES NECESSARY DURING CONSTRUCTION TO BE IN COMPLIANCE WITH THE GENERAL PERMIT. IMPLEMENTATION OF THE BMP'S SHOWN ON THESE PLANS DO NOT RELIEVE THE OWNER OR HIS/HER REPRESENTATIVE FROM RESPONSIBILITY OF IMPLEMENTING ALL MEASURES NEEDED TO BE IN COMPLIANCE.
3. THE CONTRACTOR SHALL USE CLASS II BASE FOR THE STABILIZED CONSTRUCTION ROADWAY OR ALTERNATE METHODS THAT ACHIEVE THE DESIRED RESULTS. THIS BMP SHALL BE IMPLEMENTED TO ALL BUILDING PADS PRIOR TO VERTICAL CONSTRUCTION, OR AS SOON AS PRACTICAL.
4. THE CONTRACTOR MAY UTILIZE RUMBLE PLATES IN LIEU OF RIP RAP AT THE CONSTRUCTION ENTRANCES AS LONG AS THEY ACCOMPLISH THE DESIRED RESULTS.
5. ANY SEDIMENTS TRACKED OFFSITE SHALL BE CLEANED DAILY BY MEANS OF MOBILE STREET SWEEPERS.
6. ANY GRADED AREAS THAT ARE GOING TO SIT IDLE FOR MORE THAN TWO WEEKS, SHALL HAVE AN APPROPRIATE GROUND COVER BMP APPLIED.
7. THE LOCATIONS SHOWN FOR THE EQUIPMENT AND MATERIAL DELIVERY STORAGE AREAS AND CONCRETE WASTE CLEANOUT MAY BE RELOCATED DURING CONSTRUCTION.

0 10 20 30 feet
1"=10'

rrm design group

EROSION AND SEDIMENT CONTROL PLAN

JANUARY 17, 2023

6491 CALLE REAL

C3

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CONTRACTOR'S NOTES:

1. CONTRACTOR SHALL PREVENT WATER CONTAMINATION DURING CONSTRUCTION BY IMPLEMENTING THE FOLLOWING CONSTRUCTION SITE MEASURES:

A. ALL ENTRANCES/EXITS TO THE CONSTRUCTION SITE SHALL BE STABILIZED USING METHODS DESIGNED TO REDUCE TRANSPORT OF SEDIMENT OFF SITE.
B. STABILIZING MEASURES MAY INCLUDE BUT ARE NOT LIMITED TO USE OF GRAVEL PADS, STEEL RUMBLE PLATES, TEMPORARY PAVING, ETC.
C. ANY SEDIMENT OR OTHER MATERIALS TRACKED OFF SITE SHALL BE REMOVED THE SAME DAY AS THEY ARE TRACKED USING DRY CLEANING METHODS.
D. ENTRANCES/EXITS SHALL BE MAINTAINED UNTIL GRADED AREAS HAVE BEEN STABILIZED BY STRUCTURES, LONG-TERM EROSION CONTROL MEASURES OR LANDSCAPING.
E. APPLY CONCRETE, ASPHALT, AND SEAL COAT ONLY DURING DRY WEATHER.
F. COVER STORM DRAINS AND MANHOLES WITHIN THE CONSTRUCTION AREA WHEN PAVING OR APPLYING SEAL COAT, SLURRY, FOG SEAL, ETC.
G. STORE, HANDLE AND DISPOSE OF CONSTRUCTION MATERIALS AND WASTE SUCH AS PAINT, MORTAR, CONCRETE SLURRY, FUELS, ETC. IN A MANNER WHICH MINIMIZES THE POTENTIAL FOR STORM WATER CONTAMINATION.

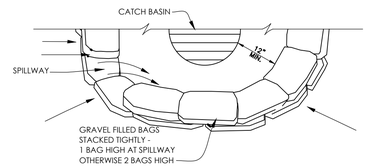
2. WITHIN 30 DAYS OF COMPLETION OF GRADING ACTIVITIES, CONTRACTOR SHALL USE HYDRO-SEED, STRAW BLANKETS, GEOTEXTILE BINDING FABRICS OR OTHER P&D APPROVED METHODS AS NECESSARY TO HOLD SLOPE SOILS UNTIL LANDSCAPE VEGETATION IS ESTABLISHED. P&D MAY REQUIRE THE RESEEDING OF SURFACES GRADED FOR THE PLACEMENT OF STRUCTURES IF CONSTRUCTION DOES NOT COMMENCE WITHIN 30 DAYS OF GRADING.

3. CONTRACTOR SHALL DESIGNATE A CONSTRUCTION EQUIPMENT FILLING AND STORAGE AREA(S) TO CONTAIN SPILLS, FACILITATE CLEAN-UP AND PROPER DISPOSAL AND PREVENT CONTAMINATION FROM DISCHARGING TO THE STORM DRAINS, STREET, DRAINAGE DITCHES, CREEKS, OR WETLANDS. THE AREAS SHALL BE NO LARGER THAN 50 X 50 FOOT UNLESS OTHERWISE APPROVED BY P&D AND SHALL BE LOCATED AT LEAST 100 FEET FROM ANY STORM DRAIN, WATER BODY OR SENSITIVE BIOLOGICAL RESOURCES.

4. GRADING AND EROSION AND SEDIMENT CONTROL PLANS SHALL BE DESIGNED TO MINIMIZE EROSION DURING CONSTRUCTION AND SHALL BE IMPLEMENTED FOR THE DURATION OF THE GRADING PERIOD AND UNTIL RE-GRADED AREAS HAVE BEEN STABILIZED BY STRUCTURES, LONG-TERM EROSION CONTROL MEASURES OR PERMANENT LANDSCAPING.

5. THESE MEASURES ARE REQUIRED FOR ALL PROJECTS INVOLVING EARTHMOVING ACTIVITIES REGARDLESS OF THE PROJECT SIZE OR DURATION. PROPER IMPLEMENTATION OF THESE MEASURES IS ASSUMED TO FULLY MITIGATE FUGITIVE DUST EMISSIONS. DURING CONSTRUCTION, USE WATER TRUCKS OR SPRINKLER SYSTEMS TO KEEP ALL AREAS OF VEHICLE MOVEMENT DAMP ENOUGH TO PREVENT DUST FROM LEAVING THE SITE. AT A MINIMUM, THIS SHOULD INCLUDE WETTING DOWN SUCH AREAS IN THE LATE MORNING AND AFTER WORK IS COMPLETED FOR THE DAY. INCREASED WATERING FREQUENCY SHOULD BE REQUIRED WHENEVER THE WIND SPEED EXCEEDS 15 MPH. RECLAIMED WATER SHOULD BE USED WHENEVER POSSIBLE. HOWEVER, RECLAIMED WATER SHOULD NOT BE USED IN OR AROUND CROPS FOR HUMAN CONSUMPTION. MINIMIZE AMOUNT OF DISTURBED AREA AND REDUCE ON SITE VEHICLE SPEEDS TO 15 MPH OR LESS. IF IMPORTATION, EXPORTATION AND STOCKPILING OF FILL MATERIAL IS INVOLVED, SOIL STOCKPILED FOR MORE THAN TWO DAYS SHALL BE COVERED, KEPT MOIST, OR TREATED WITH SOIL BINDERS TO PREVENT DUST GENERATION. TRUCKS TRANSPORTING FILL MATERIAL TO AND FROM THE SITE SHALL BE TARPED FROM THE POINT OF ORIGIN. GRAVEL PADS SHALL BE INSTALLED AT ALL ACCESS POINTS TO PREVENT TRACKING OF MUD ONTO PUBLIC ROADS. AFTER CLEARING, GRADING, EARTH MOVING OR EXCAVATION IS COMPLETED, TREAT THE DISTURBED AREA BY WATERING, OR REVEGETATING, OR BY SPREADING SOIL BINDERS UNTIL THE AREA IS PAVED OR OTHERWISE DEVELOPED SO THAT DUST GENERATION WILL NOT OCCUR.

6. THE CONTRACTOR OR BUILDER SHALL DESIGNATE A PERSON OR PERSONS TO MONITOR THE DUST CONTROL PROGRAM AND TO ORDER INCREASED WATERING, AS NECESSARY, TO PREVENT TRANSPORT OF DUST OFFSITE. THEIR DUTIES SHALL INCLUDED HOLIDAY AND WEEKEND PERIODS WHEN WORK MAY NOT BE IN PROGRESS. THE NAME AND TELEPHONE NUMBER OF SUCH PERSONS SHALL BE PROVIDED TO THE AIR POLLUTION CONTROL DISTRICT PRIOR TO LAND USE CLEARANCE FOR MAP RECORDATION AND LAND USE CLEARANCE FOR FINISH GRADING OF THE STRUCTURE.



CATCH BASIN INLET SEDIMENT BARRIER

SCALE: N.T.S.

NOTES:

1. ALL CONCRETE TRUCKS AND FINISHING TOOLS SHALL BE WASHED AT THE WASH OFF AREA.
2. ALL CONCRETE WASTE COLLECTED IN WASH OFF AREA SHALL BE RECYCLED OR APPROPRIATELY DISPOSED OF OFF-SITE.
3. LOCATION AND SIZE OF WASH-OFF AREA MAY BE ADJUSTED TO ACCOMMODATE SITE CONDITIONS.

TYPICAL CONCRETE WASH-OFF AREA SECTION

SCALE: N.T.S.

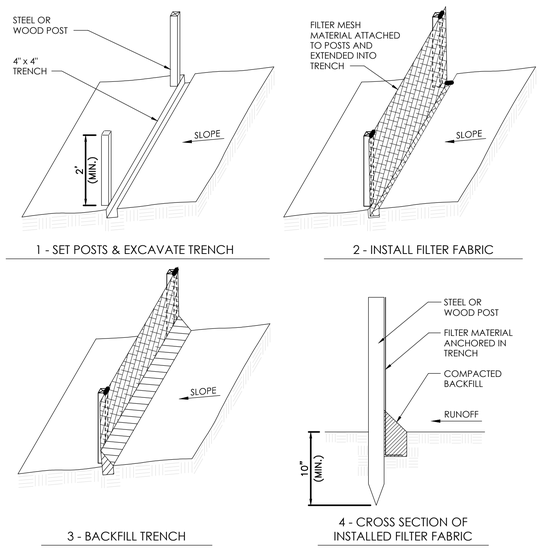
SILT FENCE

CONSTRUCTION SPECIFICATIONS:

- THE HEIGHT OF A SILT FENCE SHALL NOT EXCEED 36 INCHES (0.9 M). STORAGE HEIGHT AND PONDING HEIGHT SHALL NEVER EXCEED 18 INCHES (0.5 M).
- THE FENCE LINE SHALL FOLLOW THE CONTOUR AS CLOSELY AS POSSIBLE. IF POSSIBLE, THE FILTER FABRIC SHALL BE CUT FROM A CONTINUOUS ROLL TO AVOID THE USE OF JOINTS.
- JOINTS, WHEN NECESSARY, SHALL BE SPLICED ONLY AT A SUPPORT POST, WITH A MINIMUM 6 INCH (0.2 M) OVERLAP AND BOTH ENDS SECURELY FASTENED TO THE POST.
- POSTS SHALL BE SPACED A MAXIMUM OF 10 FEET (3.1 M) APART AND DRIVEN SECURELY INTO THE GROUND (MINIMUM OF 12 INCHES (0.3M)). WHEN EXTRA-STRENGTH FABRIC IS USED WITHOUT THE WIRE SUPPORT FENCE, POST SPACING SHALL NOT EXCEED 6 FEET (1.8 M).
- TURN THE ENDS OF THE FENCE UPHILL.
- A TRENCH SHALL BE EXCAVATED APPROXIMATELY 4 INCHES (101 MM) WIDE AND 6 INCHES (0.2 M) DEEP ALONG THE LINE OF POSTS AND UP-SLOPE FROM THE BARRIER.
- WHEN STANDARD-STRENGTH FILTER FABRIC IS USED, A WIRE MESH SUPPORT FENCE SHALL BE FASTENED SECURELY TO THE UP-SLOPE SIDE OF THE POSTS USING HEAVY DUTY WIRE STAPLES AT LEAST 1 INCH (25.4 MM) LONG, TIE WIRES OR HOG RINGS. THE WIRE SHALL EXTEND INTO THE TRENCH A MINIMUM OF 2 INCHES (51MM) ABOVE THE ORIGINAL GROUND SURFACE. FILTER FABRIC SHALL NOT BE STAPLED TO EXISTING TREES.
- WHEN EXTRA-STRENGTH FILTER FABRIC AND CLOSER POST SPACING ARE USED, THE WIRE MESH SUPPORT FENCE MAY BE ELIMINATED. IN SUCH A CASE, THE FILTER FABRIC IS STAPLED OR WIRED DIRECTLY TO THE POSTS.
- THE TRENCH SHALL BE BACKFILLED AND THE SOIL COMPACTED OVER THE TOE OF THE FILTER FABRIC.
- SILT FENCES PLACED AT THE TOE OF A SLOPE SHALL BE SET AT LEAST 6 FEET (1.8 M) FROM THE TOE IN ORDER TO INCREASE PONDING VOLUME.
- SILT FENCES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFUL PURPOSE, BUT NOT BEFORE THE UPSLOPE AREA HAS BEEN PERMANENTLY STABILIZED AND ANY SEDIMENT STORED BEHIND THE SILT FENCE HAS BEEN REMOVED.

INSPECTION AND MAINTENANCE:

- SILT FENCES AND FILTER BARRIERS SHALL BE INSPECTED WEEKLY AFTER EACH SIGNIFICANT STORM (1 INCH (25.4 MM) IN 24 HOURS). ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.
- SEDIMENT SHOULD BE REMOVED WHEN IT REACHES ½ HEIGHT OF THE FENCE OR 9 INCHES (0.3 M) MAXIMUM.
- THE REMOVED SEDIMENT SHALL CONFORM WITH THE EXISTING GRADE AND BE VEGETATED OR OTHERWISE STABILIZED.



1 - SET POSTS & EXCAVATE TRENCH

2 - INSTALL FILTER FABRIC

3 - BACKFILL TRENCH

4 - CROSS SECTION OF INSTALLED FILTER FABRIC

SILT FENCE INSTALLATION

N.T.S.

GRAVEL BAG INSTALLATION

SCALE: N.T.S.

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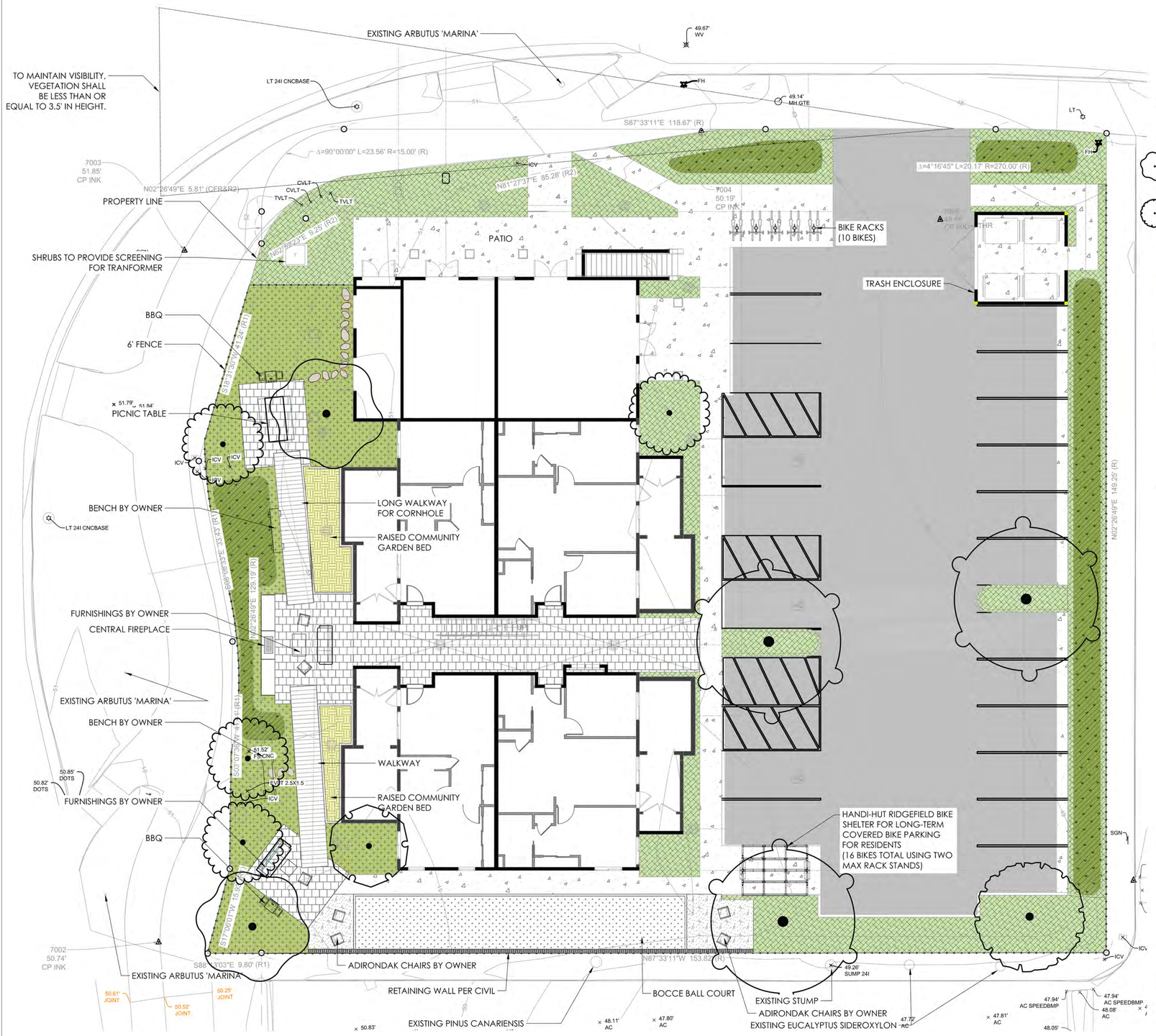
EROSION AND SEDIMENT CONTROL DETAILS

JANUARY 17, 2023

6491 CALLE REAL

C4

H:\1800\1956-02 RP21-6491 Calle Real-Mixed-Use Entitlements\Engineering\DataDev\Sheet Files\C-ESC Plan.dwg, C3, Jan 17, 2023, 9:42am, brabbock1



PLANT_SCHEDULE

TREES	BOTANICAL NAME	COMMON NAME	CONT	WUCOLS
	ARBUTUS X 'MARINA'	MARINA STRAWBERRY TREE MULTI-TRUNK	24"BOX	LOW
	CERCIDIUM X 'DESERT MUSEUM'	DESERT MUSEUM PALO VERDE	24"BOX	LOW
	CERCIS OCCIDENTALIS	WESTERN REDBUD	24"BOX	LOW
	HETEROMELES ARBUTIFOLIA `DAVIS GOLD`	DAVIS GOLD TOYON	24"BOX	VERY LOW
	QUERCUS TOMENTELLA	ISLAND OAK	24"BOX	LOW

BIOSWALE	BOTANICAL NAME	COMMON NAME	CONT	WUCOLS
	ACHILLEA X 'MOONSHINE'	MOONSHINE YARROW	5 GAL	LOW
	CHONDROPETALUM TECTORUM	SMALL CAPE RUSH	5 GAL	LOW
	JUNCUS PATENS 'ELK BLUE'	SPREADING RUSH	5 GAL	LOW
	LEYMUS CONDENSATUS 'CANYON PRINCE'	CANYON PRINCE GIANT WILD RYE	5 GAL	LOW
	MIMULUS AURANTIACUS	STICKY MONKEYFLOWER	1 GAL	VERY LOW
	MIMULUS AURANTIACUS VAR. PUNICEUS	RED BUSH STICKY MONKEYFLOWER	1 GAL	VERY LOW
	MUHLENBERGIA RIGENS	DEER GRASS	1 GAL	LOW

NATIVE ACCENT	BOTANICAL NAME	COMMON NAME	CONT	WUCOLS
	AGAVE DESERTI	DESERT AGAVE	5 GAL	LOW
	AGAVE SHAWII	COASTAL AGAVE	5 GAL	LOW
	ARCTOSTAPHYLOS MANZANITA 'DR. HURD'	DR. HURD COMMON MANZANITA	5 GAL	LOW
	ARCTOSTAPHYLOS X 'EMERALD CARPET'	EMERALD CARPET MANZANITA	5 GAL	LOW
	ARTEMISIA CALIFORNICA	CALIFORNIA SAGEBRUSH	1 GAL	LOW
	BACCHARIS PILULARIS	COYOTE BRUSH	1 GAL	LOW
	BACCHARIS PILULARIS 'PIGEON POINT'	PIGEON POINT COYOTE BRUSH	1 GAL	LOW
	BACCHARIS X 'CENTENNIAL'	CENTENNIAL COYOTE BRUSH	5 GAL	LOW
	CARPENTERIA CALIFORNICA	BUSH ANEMONE	5 GAL	LOW
	FRANGULA CALIFORNICA 'MOUND SAN BRUNO'	MOUND SAN BRUNO COFFEEBERRY	5 GAL	VERY LOW
	KOELERIA MACRANTHA	PRAIRIE JUNEGRASS	1 GAL	LOW
	ROMNEYA COULTERI 'WHITE CLOUD'	WHITE CLOUD MATILIJA POPPY	5 GAL	VERY LOW
	ROSA CALIFORNICA	CALIFORNIA WILD ROSE	5 GAL	LOW
	SALVIA CLEVELANDII 'WINNIFRED GILMAN'	WINNIFRED GILMAN CLEVELAND SAGE	5 GAL	VERY LOW
	SALVIA LEUCOPHYLLA	PURPLE SAGE	5 GAL	LOW
	SALVIA MELLIFERA	BLACK SAGE	5 GAL	VERY LOW
	SALVIA MELLIFERA 'PT. MUGU'	PT. MUGU BLACK SAGE	5 GAL	LOW

ON-SITE EXISTING TREES

THERE ARE NO EXISTING TREES ON THE PROPERTY

OFF-SITE EXISTING TREES	BOTANICAL NAME	COMMON NAME	QUANTITY
	ARBUTUS 'MARINA'	MARINA ARBUTUS	3
	EUCALYPTUS SIDEROXYLON	RED IRON BARK	2
	PINUS CANARIENSIS	CANARY ISLAND PINE	1

CONSTRUCTION LEGEND

REFER TO SPECIFICATIONS FOR COLOR, TYPE, AND FINISH

SYMBOL	DESCRIPTION
	AC PAVEMENT
	CONCRETE PAVEMENT
	CONCRETE WITH STAMP
	BOCCE COURT OYSTER SHELL
	DECOMPOSED GRANITE
	CONCRETE WITH WOOD PATTERN STAMP
	BIOSWALE PLANTING
	PATIO AREA PLANTING PALETTE
	NATIVE PLANT WITH ROCK MULCH
	COMMUNITY GARDEN BOXES
	6' FENCE

PLANTING DESIGN CRITERIA:

THE PLANT PALETTE IS COMPRISED OF PLANT MATERIAL AND TREES KNOWN TO THRIVE IN THE LOCAL CLIMATE AND SOIL CONDITIONS. 20% OR LESS OF THE PLANT MATERIAL WILL REQUIRE MODERATE WATER, AND THE REMAINDER WILL REQUIRE LOW TO VERY LOW WATER ONCE ESTABLISHED. THIS PLANT PALETTE COUPLED WITH THE IRRIGATION SYSTEM DESCRIBED, HAS BEEN DESIGNED TO MEET OR EXCEED THE STATE AND LOCAL STANDARDS FOR WATER CONSERVATION THROUGH WATER EFFICIENT LANDSCAPE IRRIGATION DESIGN. LOW WATER SHRUBS AND GROUNDCOVERS COMPRISE A MAJORITY OF THE TOTAL LANDSCAPED AREA.

THIS PLAN WILL COMPLY WITH MUNICIPAL CODE REQUIREMENTS AND STATE WATER EFFICIENT LANDSCAPE ORDINANCES.

IRRIGATION DESIGN CRITERIA:

THE IRRIGATION DESIGN WILL COMPLY WITH LOCAL AND STATE WATER CONSERVATION REQUIREMENTS. THE WATER CONSERVATION METHOD FOR NEW ORNAMENTAL LANDSCAPE PLANT MATERIAL HAS LOW TO MEDIUM WATER USE. A WEATHER SENSING, 'SMART CONTROLLER' WILL BE USED TO MONITOR THE IRRIGATION WATER AND MANAGE DAILY WATER CONSUMPTION TO THE MINIMUM REQUIREMENTS FOR EACH HYDROZONE. ALL TREES, SHRUBS, AND GROUNDCOVER AREAS WILL BE IRRIGATED ON SEPARATE HYDROZONES SO THAT ONCE ESTABLISHED, WATER CAN BE REGULATED IN A MORE EFFICIENT MANNER. TREES WILL BE IRRIGATED BY BUBBLERS. ALL ORNAMENTAL PLANTING WILL RECEIVE DRIP IRRIGATION OR OTHER HIGHLY EFFICIENT IRRIGATION.

TOTAL PERVIOUS LANDSCAPE AREA: 5,110SF

NOTE: PLANTING AND IRRIGATION WILL ONLY BE PROVIDED FOR APPROXIMATELY HALF THE PERVIOUS LANDSCAPE AREA TO MEET WATER RESTRICTIONS.

WATER EFFICIENT LANDSCAPE WORKSHEET

This worksheet is filled out by the project applicant and it is a required element of the Landscape Document Package

Reference Evapotranspiration (Eto) 48.1

Hydrozone # /Planting Description*	Plant Factor (PF)	Irrigation Method	Irrigation Efficiency (IE)	ETAF (PF/IE)	Landscape Area (Sq, ft)	ETAF x Area	Estimated Total Water Use (ETWU)
Regular Landscape Areas							
1 - DT Shrubs	0.25	Drip	0.91	0.27	5,510	1513.74	45,143
2-DT Trees	0.25	Bubbler	0.8	0.31	192	60.00	1,789
				Totals	5702	1573.74	46,932
Special Landscape Areas							
Play Field				1	0		0
Edibles				1	0		0
Other				1	0		0
				Totals	0		0
						ETWU Total	46,932
				Maximum Allowed Water Allowance (MAWA)			93,525

Hydrozone #/Planting Description
E.G.
1.) front lawn
2.) low water use plantings
3.) medium water use planting

Irrigation Method
overhead spray or drip

Irrigation Efficiency
0.75 for spray head
0.81 for drip

ETWU (Annual Gallons Required)
Eto x 0.62 x (ETAF x Area)

where 0.62 is a conversion factor that converts acre-inches per acre per year to gallons per square foot per year.

*MAWA (Annual Gallons Allowed) = (Eto) (0.62) [(ETAF x LA)]+[(1-ETAF) x SLA)]

where 0.62 is a conversion factor that converts acre-inches per acre per year to gallons per square foot per year. LA is the total landscape area in square feet. SLA is the total special landscape area in square feet, and ETAF is .55 for residential areas and 0.45 for non-residential areas.

ETAF Calculations

Regular Landscape Areas

Total ETAF x Area	(B)
Total Area	(A)
Average ETAF	B / A

All Landscape Areas

Total ETAF x Area	(B+D)
Total Area	(A+C)
Sitewide ETAF	(B+D) / (A+C)

Average ETAF for Regular Landscape Areas must be 0.55 or below for residential areas, and 0.45 or below for non-residential areas.

0 10 20 30 feet

1"=10'

rrm design group

CALLE REAL MIXED USE (22-0005-DP & 22-0001-CUP)

6491 CALLE REAL, GOLETA, CA 93117

LANDSCAPE PLAN L1

11/30/2022

FIXTURE SCHEDULE

Symbol	Label	Image	Quantity	Manufacturer	Catalog Number	Description	Source	Lumens Per LED	LLF	Input Power	Distribution	Notes
	S1		2	SUN VALLEY LIGHTING	DS6D-PLED-IV-FT-20LED-525mA-30K UNV 1 RAL-XXXX/XPKMRTA 1443 - 125 PT23 RAL XXXX	FABRICATED METAL HOUSING, 1 LED MODULE CONSISTING OF:	20 WHITE LIGHT EMITTING DIODES (LEDS), BASE UP.	215	0.9	35.5		
	S2		2	SUN VALLEY LIGHTING	DS6D-PLED-II-FR-20LED-525mA-30K-UNV 1 RAL-XXXX - HS/XPKMRTA 1443 - 125 PT23 RAL XXXX	FABRICATED METAL HOUSING, 1 LED MODULE CONSISTING OF:	20 WHITE LIGHT EMITTING DIODES (LEDS), BASE UP.	174	0.9	35.5		
	S3		6	BEGA	77221 K3	CUTOFF BOLLARD	LED 7.8W 3000K	643	0.92	10		
	S4		3	ANP LIGHTING	W516 M016LDD W 30K SRTCW E3 FINISH	W516:16" WAREHOUSE SHADE, 16W CREE LED Module - Dome Lens, 3000K CCT		1852	0.92	17.45		

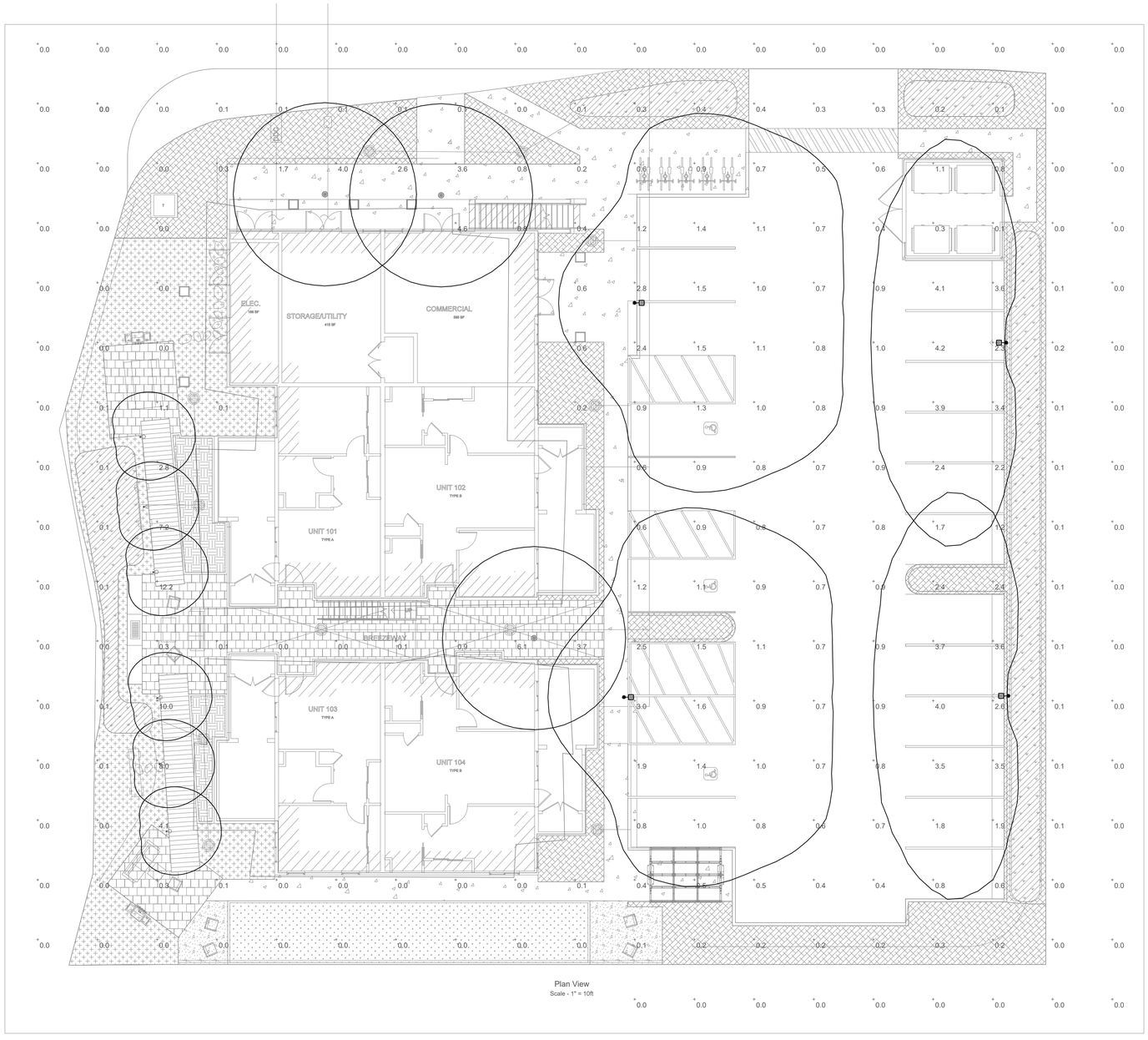
STATISTICS

Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min	UG
SITE LIGHTING	+	1.0 fc	12.2 fc	0.0 fc	N/A	N/A	-1.0

LUMINAIRE LOCATIONS

		Location				
No.	Label	X	Y	Z	MH	Tilt
3	S1	22.00	-11.10	15.00	15.00	0.00
1	S2	86.04	48.26	15.00	15.00	0.00
2	S2	86.40	-10.88	15.00	15.00	0.00
1	S3	-58.97	32.47	3.50	3.50	0.00
2	S3	-56.80	9.83	3.50	3.50	0.00
3	S3	-56.16	-11.24	3.50	3.50	0.00
4	S3	-54.60	-33.57	3.50	3.50	0.00
5	S3	-58.40	20.78	3.50	3.50	0.00
6	S3	-55.73	-22.27	3.50	3.50	0.00
2	S4	-8.61	72.94	10.00	10.00	0.00
3	S4	-28.07	73.12	10.00	10.00	0.00
4	S4	6.95	-1.22	10.00	10.00	0.00
1	S1	23.79	54.94	15.00	15.00	0.00

THIS SITE PHOTOMETRIC PLAN IS CONSISTENT WITH CHAPTER 17.35 OF THE GOLETA ZONING ORDINANCE.



Plan View
Scale - 1" = 10ft

rrm design group
rrmdesign.com | (805) 543-1794
3765 S. Higuera, San Luis Obispo, CA 93401

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CONSULTANT

JMPE
ELECTRICAL ENGINEERING
LIGHTING DESIGN
CA REGISTRATION NO E13083
23411
REGISTERED PROFESSIONAL ENGINEER
E013083
EXP. 09/30/2023
ELECTRICAL
STATE OF CALIFORNIA
627 OLIVE STREET
SANTA BARBARA CA 93101
(805) 569-0216
FAX (805) 569-2405
email : maloney@jmpe.net
www.jmpe.net

AGENCY

CALLE REAL MIXED USE

6491 CALLE REAL, GOLETA, CA 93117

SITE PHOTOMETRICS PLAN

NO.	REVISION	DATE

PROJECT MANAGER

DRAWN BY | CHECKED BY

DATE
06-27-2023

PROJECT NUMBER

SHEET
SL-1

AREA & ROADWAY LIGHTING

PROJECT NAME:

PROJECT TYPE:

DEZĪNER SERIES 6

Small Neoclassical, Domed Bell Luminaire

Features
The NEW Dezīner Series is a flexible, configurable pedestrian scale decorative pendant luminaire with an 6.7" diameter upper housing of 0.125" thick formed aluminum with a large assortment of spun aluminum shades and ornamental options. Each lower housing is comprised of a 0.080" thick spun aluminum reflector with an integrated LED module seat, thermal management for long LED life and a thermally isolated solid state power supply chamber. Trulevel ™ ball coupling.

PLED™ Optics
Emitters (LED's) are arrayed on a metal core PCB panel with each emitter located on a copper thermal transfer pad and enclosed by an LED refractor. LED optics completely seal each individual emitter to meet an IP66 rating. In asymmetric distributions, a micro-reflector inside the refractor re-directs the house side emitter output towards the street side and functions as a house side shielding element. Refractors are injection molded H12 acrylic. Each LED refractor is sealed to the PCB over an emitter and all refractors are retained by an aluminum frame. Any one Panel, or group of Panels in a luminaire, have the same optical pattern. LED refractors produce standard site/area distributions. All optics are U0, Zero Uplight and are Dark Sky compliant. Panels are field replaceable and field rotatable in 90° increments.

LED Emitters
High Power White LED's are driven between 350mA and 875mA for a maximum output of 2.5 Watts nominal. LED's are available in standard Warm White (2700K & 3000K), Neutral White (4000K), or Cool White (5000K). All Standard LED's have a minimum of 70 CRI. Consult Factory for other LED options. Lumen Maintenance of L93 at 100,000 hours (TM-21 calculated at 6x Test Time) for all LED options.

True Amber LED's TRA-True Amber LED's emit light in the amber spectral bandwidth centered on 585-590nm. True Amber has negligible blue light and is suitable for wildlife.

LED Driver
Constant current electronic with a power factor of >.90 and a minimum operating temperature of -40°F/-40°C. Driver(s) is/are UL and cUL recognized and mounted directly against the Electrical Housing to facilitate thermal transfer, held down by universal clamps to facilitate easy removal. In-line terminal blocks facilitate wiring between the driver and optical arrays. Drivers accept an input of 120-277V (UNV), 50/60Hz or 347V & 480V, 50,60Hz. 0 - 10V dimmable driver is standard. Driver has a minimum of 3KV internal surge protection. Luminaire supplied with 20KV surge protector for field accessible installation.)

Finish
Super TGIC polyester powder coating is applied onto a metal substrate this has been pretreated with a four-stage process for maximum adhesion and color retention. The top coat is baked at 400° F for maximum hardness and exterior durability.

DS6A
Angled

DS6C
Concave/Flared

DS6D
Deep Bell

DS6S
Skirted

DS6T
Tiered

The smallest models within the Dezīner Series, offer 5 customizable styles available with optional Ambience™ Low Luminance Lens and unlit Bands.

U.L. Listed for Wet Location

2023062

U.S. Pole Company Inc | 660 West Avenue O, Palmdale, CA 93551
An Employee Owned Company | Phone (661) 233-2000 www.usaltg.com

MADE IN THE USA

SUN VALLEY LIGHTING & U.S. ARCHITECTURAL LIGHTING

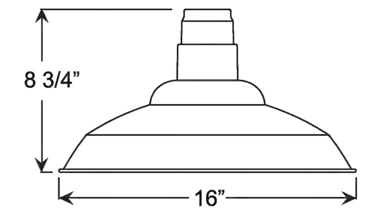
ANP Lighting

Specifications W516

Project:

Fixture Type: Quantity:

Customer:



Specifications

Material:
RLM shades are constructed of heavy duty spun aluminum. Wall back plate and driver housing are cast aluminum. All fasteners are stainless steel. Inside of shade is reflective white finish for all colors except galvanized paint finish. Screw hardware may not match paint.

Glass:
Choice of clear, frosted or prismatic glass up to **24w Max**.

Electrical:
Universal voltage 120-277 is standard. 0-10V, TRIAC and ELV dimming to 1% protocols are standard for LED modules. (12w is 120v and TRIAC dimming only)

See page 2 table for LED module and driver specs, voltage and dimming protocols

Certifications:
Cord mounts are UL Listed for dry locations. Arm mount, stem mount and wall mount are UL Listed for wet locations.

Finish:
A polyester powder coat high quality finish is electro-statically applied and baked at 430° for exceptional durability and color retention. Products undergo an intensive five-step cleansing and pretreatment process for maximum paint adhesion.

Marine grade finish provides superior salt, humidity and UV protection. This coating withstands up to 3000 hours of continuous salt spray, comes with a 5-year warranty and is available in either a textured or gloss surface.

Modifications:
Consult factory for custom or modified designs.

W516
LED
Dark Sky Friendly

Weight: 1.5 lbs.
BUG: B1-U0-G1

Catalog Logic

W516 - M024LDD W 40K - RTCW - E6 - 100GLCL - PC - 41 - UNV

RLM Style | Light Source, Wattage & Dimming | Distribution | CCT | Driver Housing | Mounting | Glass & Guard | Accessories | Finish | Voltage

Catalog Number

	1	2	3	4	5	6	7	8	9
W516									UNV

1 LIGHT SOURCE & WATTAGES	
M012LDN	(12w, 750 lumen, Cree module) Integral driver, 120V, TRIAC dimming & narrow distribution only.
M009LDD	(9w, 850 lumen, Cree module)
M010LDD	(10w, 1250 lumen, Cree module)
M016LDD	(16w, 2000 lumen, Cree module)
M024LDD	(24w, 3000 lumen, Cree module)

2 DISTRIBUTION
W (T5 Wide Distribution with Dome LED Lens)
N* (T5 Narrow Distribution with Flat LED Lens)
*12w is narrow only, select "N".

3 COLOR TEMPERATURE (CCT)	
27K	(2700K)
30K	(3000K)
35K	(3500K)
40K	(4000K)

4 DRIVER HOUSINGS*	
NA	(Housing not required for 12w)
Wall Mount Options Choose Arm in Box 5	
RTCW	(Driver Housing for Wall only (E-arms); 6 1/2" OD x 5 5/8" H)
SRTCW	(Smooth Driver Housing for Wall only (E-arms); 6" OD x 2 7/8" H)
RTCNCW	(Driver Housing/No Spun Cover for Wall only (E-arms); 5 7/8" OD x 2 5/8" H)
Cord Mount Options Choose Cord Style in Box 5	
CRD-RTCC	(Driver Housing for Cord only; 6 1/2" OD x 5 5/8" H)
CRD-SRTCC	(Smooth Driver Housing for Cord only; 6" OD x 2 7/8" H)
CRD-RTCNCC	(Driver Housing/No Spun Cover for Cord only; 5 7/8" OD x 2 5/8" H)
Cable Mount Options Choose Cord Style in Box 5	
SSC-RTCC	(Driver Housing for SS Cable/Cord only; 6 1/2" OD x 5 5/8" H)
SSC-SRTCC	(Smooth Driver Housing for SS Cable/Cord only; 6" OD x 2 7/8" H)
SSC-RTCNCC	(Driver Housing/No Spun Cover for SS Cable/Cord only; 5 7/8" OD x 2 5/8" H)
Stem Mount Options Choose Stem Size in Box 5	
ST-RTCC	(Driver Housing for Stem only; 6 1/2" OD x 5 5/8" H)
ST-SRTCC	(Smooth Driver Housing for Stem only; 6" OD x 2 7/8" H)
ST-RTCNCC	(Driver Housing/No Spun Cover for Stem only; 5 7/8" OD x 2 5/8" H)
Emergency Backup Housing Options Choose EMG Driver in Box 7	
CRD-DCCEM	(Emergency Ballast Housing for Cord only; 12" OD x 1 1/2" H) **Choose Cord Style in Box 5.**
ST-DCCEM	(Emergency Ballast Housing for Stem only; 12" OD x 1 1/2" H) **Choose Stem Size in Box 5.**
SSC-DCCEM	(Emergency Ballast Housing for SS Cable/Cord only; 12" OD x 1 1/2" H) **Choose Cord Style in Box 5.**
*Driver Housing finish will match fixture finish.	

5 MOUNTING SOURCES*	
Arm Mounts	
E3 E4 E6 E8 E9 E10 E11 E12 E15 E18 E24 E25	
Wall Mounts	
WM40 WM54 WM74 WM317	
Post Mounts	
PM10 PM20 PM30 PM40 PM50 PM319	
Cord Mounts Color & Style (See page 5 for color cord style)	
SJT Cord	
BLC (6' Black SJT Cord)	WHC (6' White SJT Cord)
Solid Fabric Colored Cords	
BLSF (6' Black)	ORSF (6' Orange)
GYSF (6' Gray)	LGSF (6' Lime Green)
CHSF (6' Chocolate Brown)	KGSF (6' Kelly Green)
WHSF (6' White)	CBSF (6' Cobalt Blue)
CASF (6' Cardinal)	SBSF (6' Sky Blue)
Patterned Fabric Colored Cords	
BWHPF (6' Black/White Houndstooth)	BIHPF (6' Brown/Ivory Houndstooth)
GYCPF (6' Gray/Citrus Yellow)	MOSPF (6' Magenta/Orange Stripe)
NMTPF (6' Navy Mini Tweed)	
Glossy Fabric Colored Cords	
GMGF (6' Gun Metal)	SSGF (6' Sterling Silver)
CPGF (6' Copper Penny)	BRGF (6' Bronze)
GOGF (6' Gold)	
Stem Mounts	
1/2" (13/16" OD) Rigid Stems	3/4" (1" OD) Rigid Stems
2ST6 2ST12 2ST18	3ST6 3ST12 3ST18
2ST24 2ST36 2ST48	3ST24 3ST36 3ST48
2ST60 2ST72 2ST96	3ST60 3ST72 3ST96
*Arm mounts, Wall mounts or Stem finish will match fixture finish.	

08012019

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ANP reserves the right to change material or design, without prior notice, in a continuing effort to upgrade its products.

1-800-548-3227
ANPlighting.com

Bollard - shielded, directed light

BEGA

Application
Bollard with shielded, direct light for illuminating entrances, paths, and many areas of public and private spaces. These luminaires provide glare-free illumination with a high degree of visual comfort for uniform lighting of ground surfaces.

Materials
Luminaire housing and base constructed of die-cast and extruded marine grade, copper free (≤0.3% copper content) A360.0 aluminum alloy
Impact resistant polycarbonate diffuser
High temperature silicone gasket
Mechanically captive stainless steel fasteners

NRTL listed to North American Standards, suitable for wet locations
Protection class IP65
Weight: 10.1 lbs

Electrical

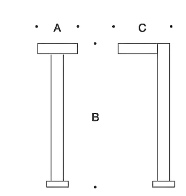
Operating voltage	120-277VAC
Minimum start temperature	-30°C
LED module wattage	7.8W
System wattage	10.0W
Controllability	0-10V, TRIAC, and ELV dimmable
Color rendering index	Ra > 90
Luminaire lumens	643 lumens (3000K)
LED service life (L70)	60,000 hours

LED color temperature
☐ 4000K - Product number + **K4**
☐ 3500K - Product number + **K35**
☐ 3000K - Product number + **K3**
☐ 2700K - Product number + **K27**

BEGA can supply you with suitable LED replacement modules for up to 20 years after the purchase of LED luminaires - see website for details

Finish
All BEGA standard finishes are matte, textured polyester powder coat with minimum 3 mil thickness.
Available colors ☐ Black (BLK) ☐ White (WHT) ☐ RAL:
☐ Bronze (BRZ) ☐ Silver (SLV) ☐ CUS:

Type:
BEGA Product:
Project:
Modified:



Bollard · shielded, directed light

	LED	A	B	C	Anchorage
77221	7.8W	7½	39½	11	79817

BEGA 1000 BEGA Way, Carpinteria, CA 93013 (805) 684-0533 info@bega-us.com

Due to the dynamic nature of lighting products and the associated technologies, luminaire data on this sheet is subject to change at the discretion of BEGA North America. For the most current technical data, please refer to bega-us.com
Updated 12/14/21
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LED FIXTURE SCHEDULE

TYPE	MANUFACTURER AND CATALOG NUMBER	LED MODULE TYPE	LED MODULE COLOR TEMP	LED MODULE WATTS	DRIVER	OPTIC/LENS	REMARKS
S1 / 36	SUN VALLEY LIGHTING DS6D-PLED-IV-FT-20LED-525MA-30K-UNV-1RAL8019-T-XPKM-HS-PLED		3000K	36	0-10V	FLAT,CLEAR W/SHIELD	FULL CUT OFF BELL PARKING LOT FIXTURE
	SUN VALLEY LIGHTING RTA1443-125-PT23-RAL8019-T						12 FT POLE
S2 / 36	SUN VALLEY DSD-PLED-II-FR-20LED-525MA-30K-UNV-RAL8019-T-XPKM-HS-PLED		3000K	36	0-10V	FLAT W/SHIELD	FULL CUT OFF BELL PARKING LOT FIXTURE
S3 / 10	BEGA 77221 K3 BKK		3000K	10	0-10V	FLAT CLEAR	FULL CUTOFF BOLLARD
S4 / 18	ANP LIGHTING W516MO16LDDW 30K SRTCW E3-76M		3000K	18	0-10V	DIFFUSE	WALL MOUNTED FULL CUTOFF

---

rrm design group
rrmdesign.com | (805) 543-1794
3765 S. Higuera, San Luis Obispo, CA 93401

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CONSULTANT

JMPE
ELECTRICAL ENGINEERING
LIGHTING DESIGN
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627 OLIVE STREET
SANTA BARBARA CA 93101
(805) 569-9216
FAX (805) 569-2405
email : maloney@jmpe.net
www.jmpe.net

REGISTERED PROFESSIONAL ENGINEER
JOHN MALONEY
No. E013083
EXP. 06/30/2025
ELECTRICAL
STATE OF CALIFORNIA

AGENCY

CALLE REAL MIXED USE
6491 CALLE REAL, GOLETA, CA 93117

DATA SHEETS AND FIXTURE SCHEDULES

NO.	REVISION	DATE
△		
△		
△		
△		
△		
△		

PROJECT MANAGER

DRAWN BY | CHECKED BY

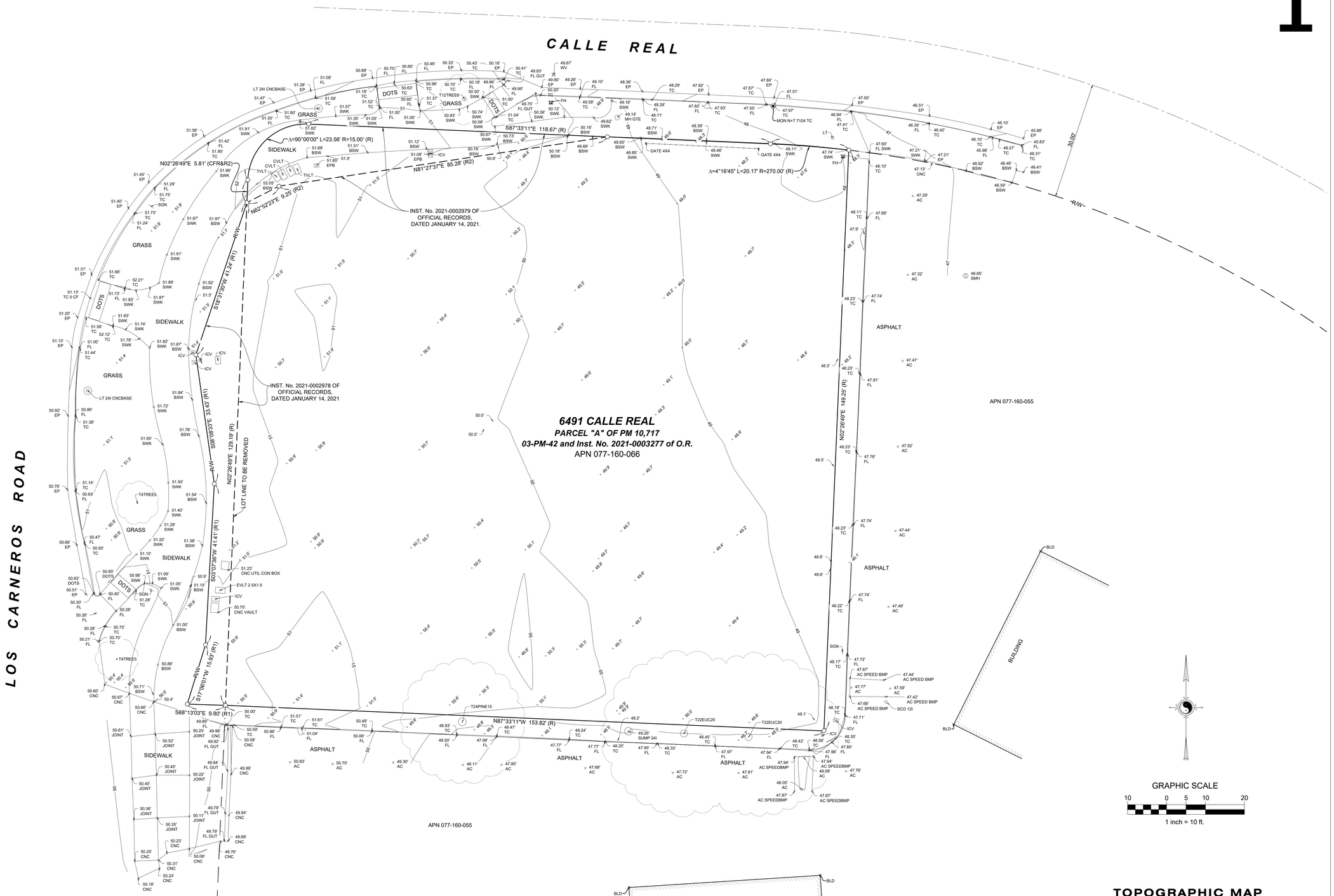
DATE
06-27-2023

PROJECT NUMBER

SHEET
SL-2

LEGEND

AC	ASPHALT CONCRETE
BLD	BUILDING
BMP	BUMPER
BSW	BACK OF SIDEWALK
CNC	CONCRETE
CVLT	COMM VAULT
EP	EDGE OF PAVEMENT
EPB	ELECTRIC PULL BOX
FH	FIRE HYDRANT
FL	FLOW LINE
FS	FINISH SURFACE
GB	GRADE BREAK
GND	GROUND
ICV	IRREGATION CONTROL VALVE
LT	LIGHT STANDARD
MON	MONUMENT
R	RECORD
R/W	RIGHT OF WAY
SCO	SEWER CLEAN OUT
SGN	SIGN
SMH	SEWER MANHOLE
SWK	SIDEWALK
T*	(TREE TRUNKSIZE*SPECIES*DRIP RADIUS*)
TC	TOP OF CURB
TVLT	TELEVISION VAULT
WV	WATER VALVE



NOTES:

(1) HORIZONTAL BASIS OF COORDINATES IS NAD83 (1991.35 EPOCH) PER STATIONS ASCN 8 AND ASCN 9 PER RECORD OF SURVEY BOOK 170, PAGES 63-65.

(2) VERTICAL DATUM IS NAVD88 PER STATION ASCN 8 PER RECORD OF SURVEY BOOK 170 PAGES 63-65. ELEVATION = 12.12' FEET.

(3) ○ = NO MONUMENT FOUND OR SET UNLESS OTHERWISE NOTED.

(4) ◉ = FOUND MONUMENT AS NOTED.

(5) (R) = PARCEL MAP No. 10,717 FILED IN BOOK 3, PAGE 42, ROTATED AT NAD83 (1991.35 EPOCH).

(6) (R1) =INSTRUMENT No. 2021-0002977 OF OFFICIAL RECORDS, DATED JANUARY 14, 2021, ROTATED TO NAD83 (1991.35 EPOCH).

(7) (R2) =INSTRUMENT No. 2021-0002979 OF OFFICIAL RECORDS, DATED JANUARY 14, 2021, ROTATED AT NAD83, (1991.35 EPOCH).

TOPOGRAPHIC MAP
OF 6491 CALLE REAL
APN 077-160-066
~ COUNTY OF SANTA BARBARA ~
~ STATE OF CALIFORNIA ~

~OCTOBER 2021~

WATERS CARDENAS LAND SURVEYING, LLP
JOSE V. CARDENAS & BARRY J. WATERS
LICENSED LAND SURVEYORS
5553 HOLLISTER AVE.-STS. 7&8 - GOLETA, CALIFORNIA 93117
PHONE: (805) 967-4416

SCALE: 1" = 10' WC W.O. #21-1016

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Attachment 3

Traffic and VMT Analysis

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ASSOCIATED TRANSPORTATION ENGINEERS

100 N. Hope Avenue, Suite 4, Santa Barbara, CA 93110 • (805)687-4418 • main@atesb.com

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June 20, 2022 22007L02

Jason Jaeger/Ben Williams
6491 Calle Real Partners, LLC
Jaeger Partners Commercial Real Estate
21 East Carrillo Street, Suite 270
Santa Barbara, California 93101

TRAFFIC AND VMT ANALYSES FOR THE
6491 CALLE REAL MIXED-USE DEVELOPMENT - GOLETA, CALIFORNIA

The following letter presents the traffic and Vehicle Miles Travelled (VMT) analyses completed by Associated Transportation Engineers (ATE) for the 6491 Calle Real Mixed-Use development (the "Project"). It is our understanding that the traffic and VMT analyses will be submitted to the City of Goleta as part of the development application for the Project.

PROJECT DESCRIPTION

The Project site is located at 6491 Calle Real in the City of Goleta. The Project site is currently zoned Office and Institutional (OI) in the General Plan. The current Project application is for the construction of a mixed-use development consisting of 14 apartment units and 585 square-feet of retail commercial space. The Project would therefore require a zone change and General Plan Amendment. Figure 1 (attached) illustrates the Project site plan. Access to the site will be provided via an existing driveway connection to Calle Real.

PROJECT TRIP GENERATION

The Project is proposing to construct 14 apartment units and 585 square-feet of retail commercial space. Trip generation estimates were calculated for the Project based on the rates published in the Institute of Transportation Engineers (ITE), Trip Generation, 11th Edition for Multi-Family Housing Low-Rise (Land Use Code #220) and Strip Retail Plaza (Land Use Code #822). Table 1 presents the trip generation estimates for the Project.

Table 1
Project Trip Generation

Land Use	Size	ADT		AM Peak Hour		PM Peak Hour	
		Rate	Trips	Rate	Trips (In/Out)	Rate	Trips (In/Out)
Apartments (Multi-Family)	14 Units	6.74	94	0.40	6 (1/5)	0.51	7 (4/3)
Retail Commercial	585 SF	54.45	32	2.36	2 (2/1)	3.39	4 (2/2)
		Total Trips:	126		8 (2/6)		11 (6/5)

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The data presented in Table 1 indicate that the Project is forecast to generate 126 ADT, 8 AM peak hour trips and 11 PM peak hour trips. It is noted that the Project would result in a reduction in peak hour trips compared to 6,000 square-feet of office space (10 AM trips and 13 PM trips) which would be allowed under the existing OI zoning.

PROJECT TRIP DISTRIBUTION

Project-generated traffic was distributed onto the study-area roadway system based on existing traffic patterns in the study-area and our knowledge of the employment and commercial centers in and surrounding the City of Goleta. The Project trip distribution is presented in Table 2.

Table 2
Project Trip Distribution

Route	Origin/Destination	Percentage
U.S. Highway 101	East	30%
	West	20%
Calle Real	East	25%
Los Carneros Road	North	5%
	South	20%
	Total:	100%

POTENTIAL TRAFFIC IMPACTS

Project trips were distributed and assigned to the local street system based on the percentages illustrated on Figure 2 (attached). The Project trip additions to the U.S. Highway 101 interchange, Calle Real, and Los Carneros Road would not be significant.

Table 3
Project Specific Impacts - AM Peak Hour

Intersection	Existing		Project-Added Trips	Impact?
	ICU/Delay	LOS		
U.S. Highway 101 NB Ramps/Los Carneros Road	0.654	B	5	No
U.S. Highway 101 SB Ramps/Los Carneros Road	0.648	B	3	No
Calle Real/Los Carneros Road	6.6 sec.	A	3	No

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Table 4
Project Specific Impacts - PM Peak Hour

Intersection	Existing		Project-Added Trips	Impact?
	ICU/Delay	LOS		
U.S. Highway 101 NB Ramps/Los Carneros Road	0.633	B	8	No
U.S. Highway 101 SB Ramps/Los Carneros Road	0.736	C	5	No
Calle Real/Los Carneros Road	10.4 sec.	B	8	No

As shown in Table 3 and 4 the Project would not generate project-specific impacts at the study-area intersections. The Project traffic additions are well below the impact thresholds adopted by the City of Goleta.

TRANSIT, PEDESTRIAN AND BICYCLE FACILITIES

Transit Service

The Metropolitan Transit District (MTD) is the local transit provider for the City of Goleta. The nearest transit stop to the Project site is the #10 Route (Cathedral Oak) located at the corner of the Cathedral Oaks Road/Los Carneros Road intersection to the north. The #10 Route runs from the State Street/La Cumbre Road intersection to the Camino Real Market Place. The #10 Route operates on weekdays and weekends providing fixed route bus service on Cathedral Oaks Road in the vicinity of the Project site. The other transit stops in the vicinity of the Project site are the #6 Route (Goleta), #11 Route (State/Hollister/UCSB) and #12X Route (Goleta Express) located at the corner of the Hollister Avenue/Los Carneros Road intersection to the south. The #6 Route runs from the Downtown Transit Center to the Camino Real Market Place. The #6 Route operates on weekdays and weekends providing fixed route bus service on Hollister Avenue in the vicinity of the Project site. The #11 Route runs from the Downtown Transit Center to UCSB and the Camino Real Market Place. The #11 Route operates on weekdays and weekends providing fixed route bus service on Hollister Avenue in the vicinity of the Project site. The #12X Route runs express service from the Downtown Transit Center to the Camino Real Market Place. The #12X Route operates on weekdays and weekends providing fixed route bus service on Hollister Avenue in the vicinity of the Project site.

Pedestrian Facilities

Sidewalks are provided along Calle Real and Los Carneros Road. The sidewalk along Calle Real connects the Project to the retail commercial center to the east. The sidewalk along Los Carneros

Road connects the Project to Cathedral Oaks Road and Hollister Avenue where transit service is provided in the study-area. The Project would provide curb, gutter and sidewalk along its frontage.

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Bicycle Facilities

Calle Real and Los Carneros Road are identified as part of the City of Goleta Bikeway System. Class II bike lanes exist along Calle Real and Los Carneros Road through the City of Goleta. The bike lanes on Calle Real and Los Carneros Road connect the Project to the residential, employment and commercial areas north, south and east of the Project.

SITE ACCESS AND CIRCULATION

As illustrated on the Project site plan (Figure 1), access to the Project site would be provided via an existing driveway connection to Calle Real located opposite the Islamic Society of Santa Barbara outbound only driveway on the north side of the street. Calle Real is a 2-lane east-west roadway with a 11-foot-wide center turn lane that provides left-turn access to the Project and the adjacent commercial businesses. The posted speed limit on Calle Real and Los Carneros Road is 45 MPH.

There is a driveway serving the retail commercial center located on the south side of Calle Real approximately 20 feet east of the Project driveway. Figure 3 illustrates the trip assignment of Project trips at the driveway. Given the estimated trip generation, the Project driveway is expected to operate at an acceptable level of service. The Project driveway connection will allow full access to the Project site with adequate sight distance. Figure 4 (attached) illustrates the driver's view along Calle Real looking to the west and east from the Project driveway. As shown on Figure 6, there is a tree located on the south side of Calle Real west of the Project driveway. It is recommended that trees and landscaping be maintained so that growth does not block the sight distance looking to the west and east from the Project driveway.

VEHICLE MILES TRAVELED ANALYSIS

The City of Goleta has adopted a new set of transportation assessment guidelines, in compliance with Senate Bill 743, which are based on a Vehicle Miles Traveled (VMT) metric rather than the traditional Level of Service (LOS) metric. Per the State's Natural Resource Agency Updated Guidelines for the Implementation of the CEQA adopted in 2018, VMT has been designated as the most appropriate measure of transportation impacts. "Vehicle Miles Traveled" refers to the amount and distance of automobile travel attributable to a project. Other relevant considerations may include the effects of the project on transit and non-motorized travel. For land use projects, vehicle miles traveled exceeding an applicable threshold of significance may indicate a significant impact.

City of Goleta's VMT Calculator

The City of Goleta has developed the VMT Calculator ("sketch tool") to assess a Project's potential VMT based on the project's description, location, and attributes. For most residential and commercial development projects, the sketch tool is the approved method to calculate Project VMT. The City provides the sketch tool for use by transportation consultants, developers, and others in assessing VMT for development projects and evaluating TDM plans. The City sketch planning tool indicates that no VMT analysis is required for the Project. The Project trips related to the "Work Commute Component" are below the minimum threshold. The Project trips related to the "Residential Component" are below the minimum threshold. City of Goleta sketch planning tool calculations are attached.

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Associated Transportation Engineers

By: Scott A. Schell
Principal Transportation Planner

attachments: Figure 1 - Project Site Plan
Figure 2 - Project Trip Distribution and Assignment
Figure 3 - Project Driveway Trip Assignment
Figure 4 - Project Driveway Line of Sight
VMT Calculator Results

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Attachment 4

Sight Distance Analysis

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Scott A. Schell

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September 6, 2022 22007L04

Jason Jaeger/Ben Williams
6491 Calle Real Partners, LLC
Jaeger Partners Commercial Real Estate
21 East Carrillo Street, Suite 270
Santa Barbara, California 93101

SIGHT DISTANCE ANALYSIS FOR THE
6491 CALLE REAL MIXED-USE PROJECT, CITY OF GOLETA, CALIFORNIA

Associated Transportation Engineers (ATE) has prepared the following sight distance evaluation for the 6491 Calle Real Mixed-Use Project (the "Project"), proposed in the City of Goleta. City staff requested that an evaluation be conducted demonstrating that adequate stopping sight distances are provided at the Project driveway on Calle Real.

SIGHT DISTANCE EVALUATION

The existing driveway that would provide access to the Project site is located east of the Los Carneros Road/Calle Real roundabout and west of the driveway for the 6487 Calle Real building, as illustrated on Figure 1 (attached). Sight distances were analyzed at the Project driveway to determine if the sight lines along Calle Real are sufficient in length to permit drivers to anticipate and avoid potential collisions when using the driveway. The Caltrans Highway Design Manual stopping sight distance standards were used to determine the requirements at the private driveway.[1]

The Project driveway is located at the beginning of a horizontal curve on Calle Real that transitions to the east; and approximately 120 feet west of the roundabout with flared entry and exit approaches. Radar speed measurements were taken at the driveway to determine the eastbound and westbound 85th percentile speeds (speed survey data attached). These 85th percentile speeds are used to determine the required stopping sight distances.

[1] Highway Design Manual, California Department of Transportation, 7th Edition, July 2020.

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The 85th percentile speed for westbound vehicles approaching the driveway and the roundabout was measured at 32 mph. The required stopping sight distance for 32 mph is 220 feet. The 85th percentile speed for eastbound vehicles exiting the roundabout and approaching the driveway was measured at 21 mph. The required stopping sight distance for 21 mph is 130 feet.

Sight distances were measured looking east and west from the Project driveway along Calle Real. The sight distance looking to the west was measured along the curve at 152 feet to the Los Carneros Road/Calle Real roundabout, as shown on Figure 2 (attached). This sight distance meets the Caltrans 130-foot minimum requirement. The sight distance looking to the east was measured at 295 feet along the curve on Calle Real, as shown on Figure 3 (attached). This sight distances meets the Caltrans 220-foot minimum stopping sight distance requirement.

It is noted that the landscaping along the Calle Real frontage of the Project site should be maintained at a height of 3.5 feet in order to provide intervisibility between the eastbound through traffic on Calle Real and the vehicles exiting the Project driveway (see Figure 4).

This concludes our sight distance evaluation for the 6491 Calle Real Mixed-Use Project. We appreciate the opportunity to assist you with the Project.

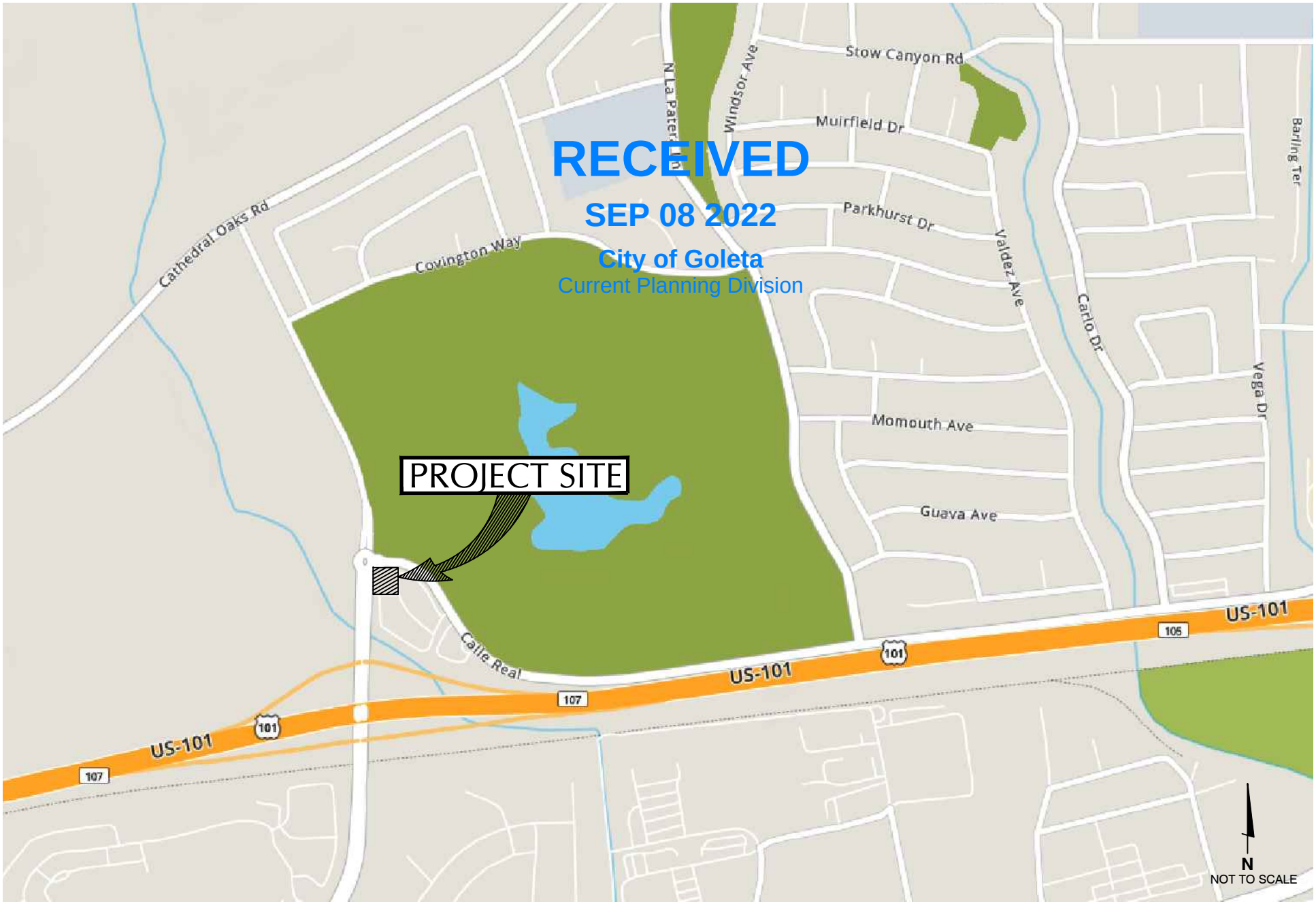
Associated Transportation Engineers

Scott A. Schell
Principal Transportation Planner

SAS/GOM

Attachments

Cc: Rachel Raynor, RRM Design Group



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PROJECT SITE LOCATION

FIGURE 1

GM, ATE#22007

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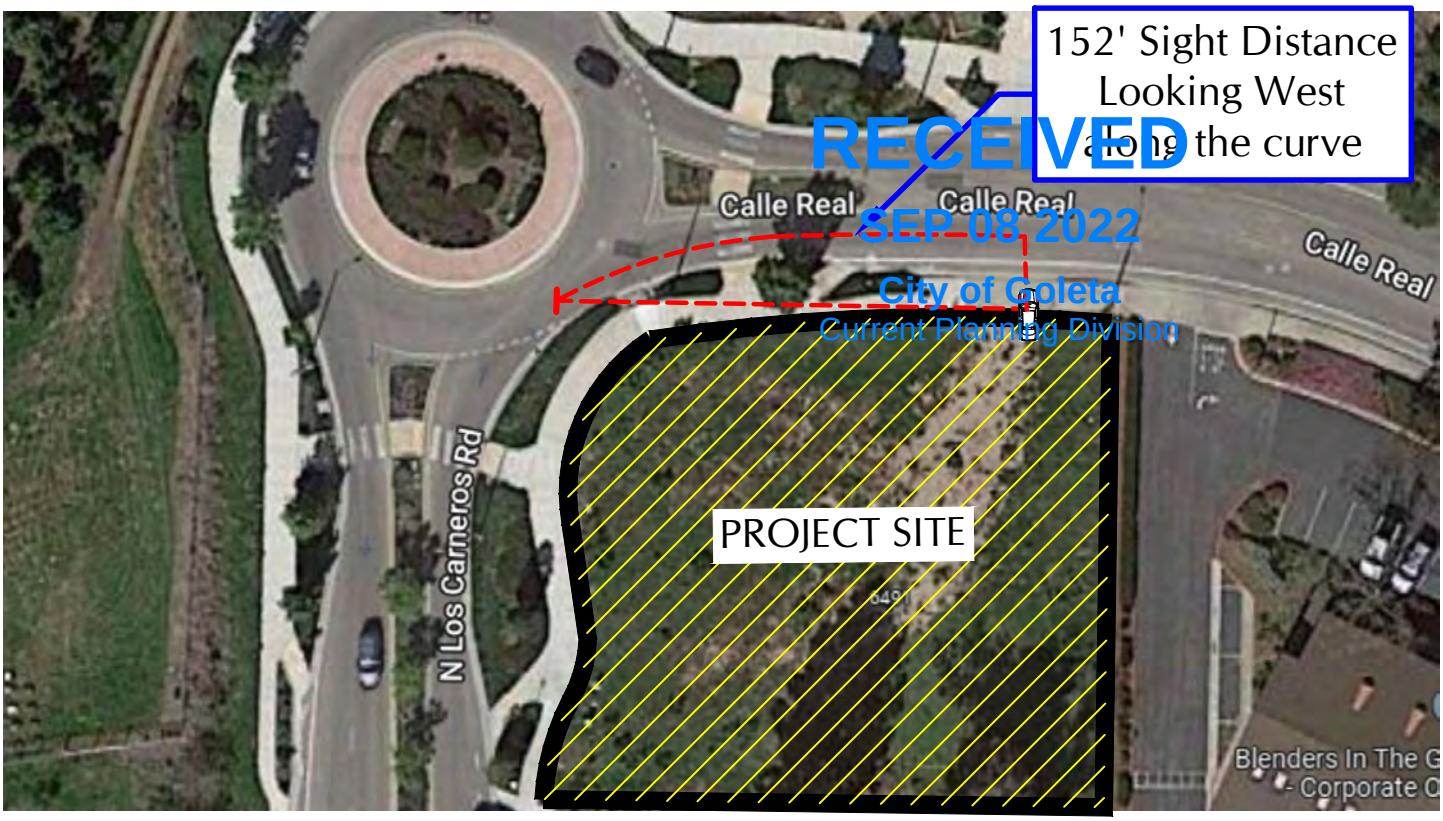

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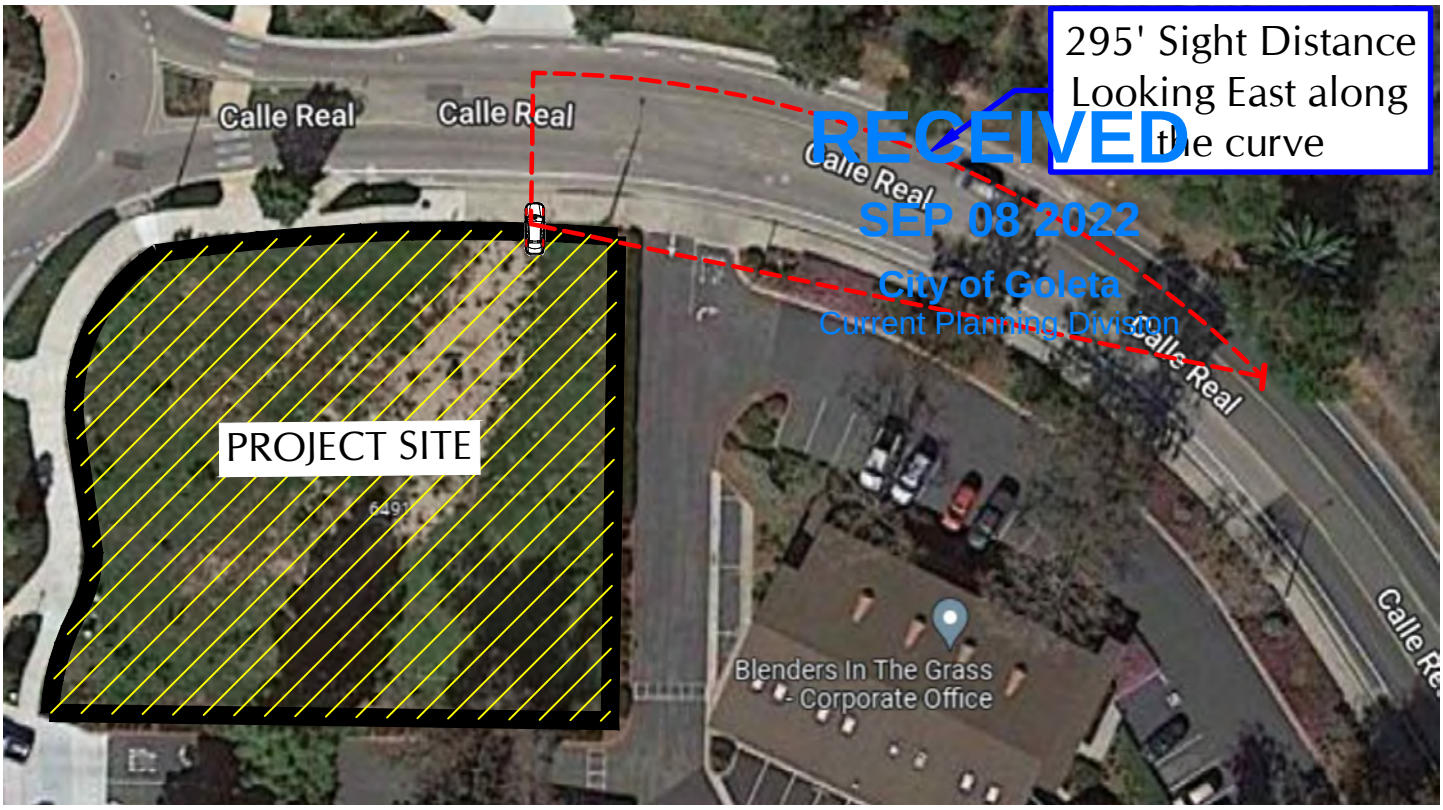
Looking West

Associated Transportation Engineers

PROJECT DRIVEWAY LINE OF SIGHT - LOOKING WEST

FIGURE 2

GM/JOB#22007



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Looking East

Associated
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PROJECT DRIVEWAY LINE OF SIGHT - LOOKING EAST

FIGURE 3

GM22-22007

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CALLE REAL SIGHT VISIBILITY

FIGURE 4

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Spot Speed Study

Prepared by: Associated Transportation Engineers

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DATE: 9/2/2022 **Location:** Calle Real @ Los Carneros - Eastbound

DAY: Friday **Posted Speed:** 45 MPH **Project #:** 22007

Spot Speeds

Speed mph	ALL Vehicles
<=10	
11	
12	
13	
14	
15	6
16	10
17	10
18	8
19	3
20	4
21	7
22	
23	2
24	
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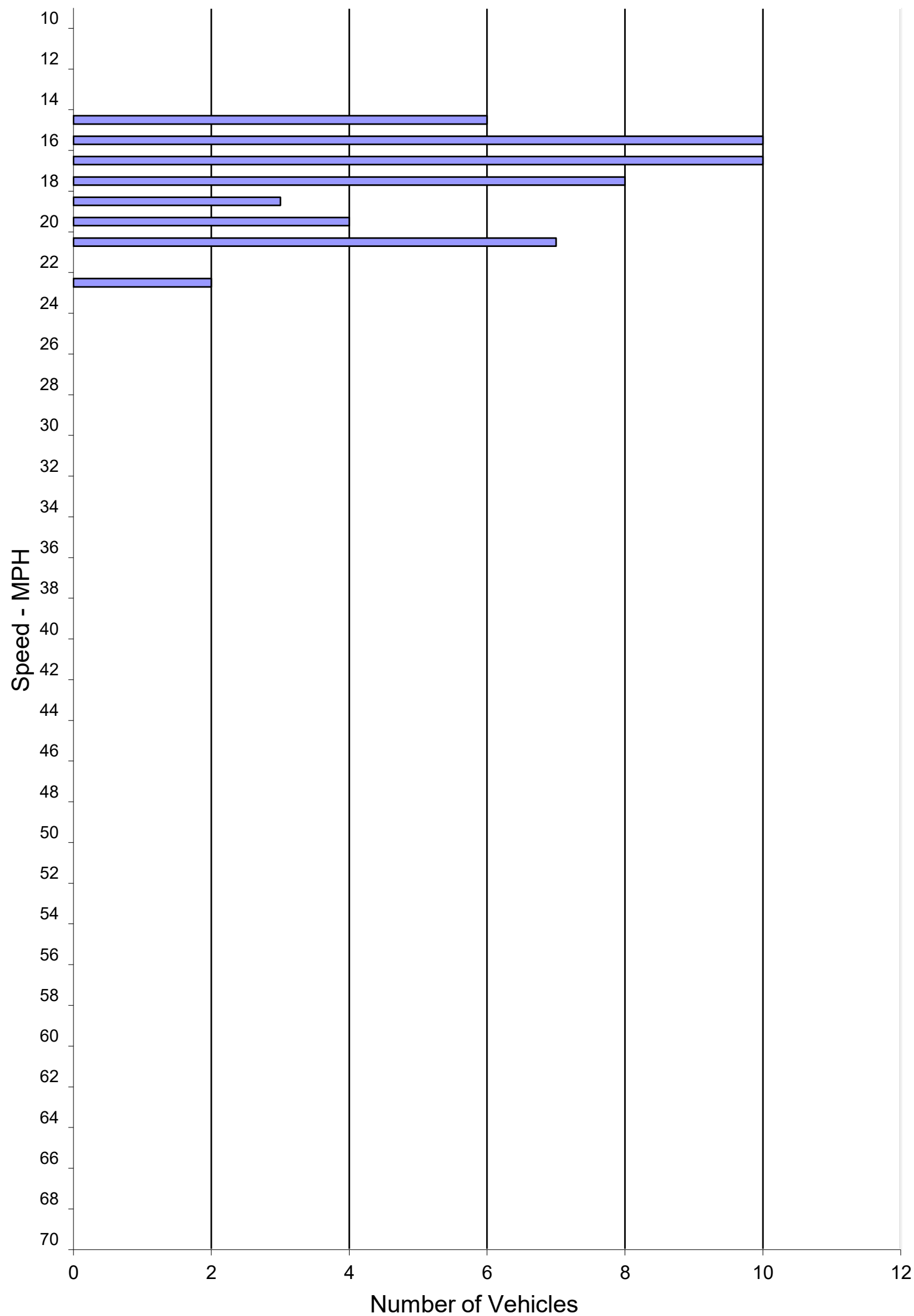

SPEED PARAMETERS										
Class	Count	Average Speed	Range	50th Percentile	85th Percentile	10 MPH Pace	# in Pace	Percent in Pace	# / % Below Pace	# / % Above Pace
ALL	50	17.9	15 - 23	17 mph	21 mph	14 - 23	50	100%	0% / 0	0% / 0

Spot Speed Study

Prepared by: Associated Transportation Engineers

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City of Goleta
Current Planning Division

DATE: 9/2/2022 **Location:** Calle Real @ Los Carneros - Westbound

DAY: Friday **Posted Speed:** 45 MPH **Project #:** 22007

Spot Speeds

Speed mph	ALL Vehicles
<=10	
11	
12	
13	
14	
15	
16	
17	
18	
19	
20	
21	1
22	1
23	
24	
25	8
26	4
27	8
28	6
29	5
30	3
31	5
32	4
33	2
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35	1
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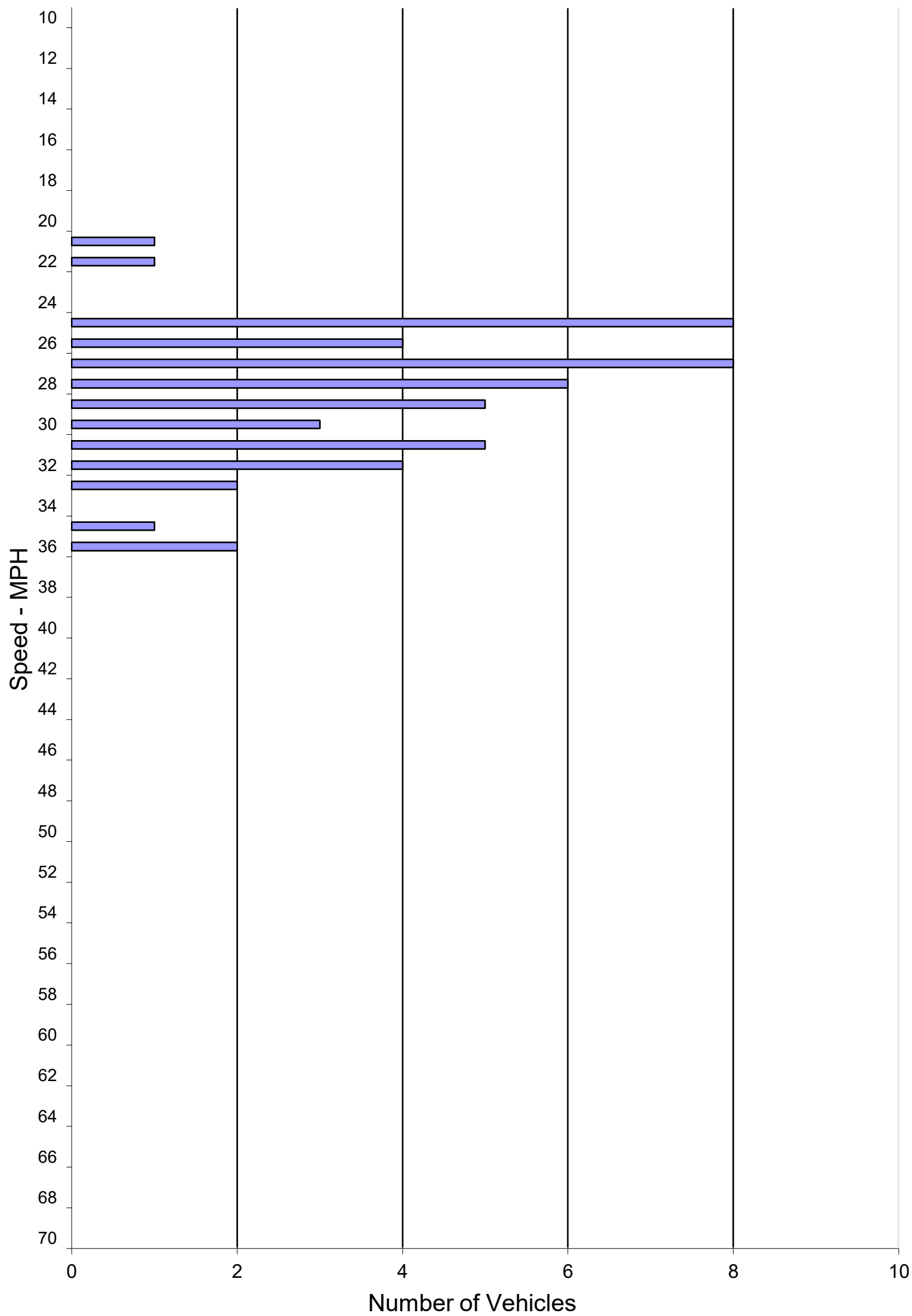

SPEED PARAMETERS										
Class	Count	Average Speed	Range	50th Percentile	85th Percentile	10 MPH Pace	# in Pace	Percent in Pace	# / % Below Pace	# / % Above Pace
ALL	50	28.4	21 - 36	28 mph	32 mph	24 - 33	45	90%	4% / 2	6% / 3

Attachment 5

Staff Presentation

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Planning Commission Hearing

September 11, 2023

6491 Calle Real Mixed-Use Project

CITY OF GOLETA

Project Site

6491 Calle Real

0.53-acre vacant parcel south of Calle Real and east of Los Carneros Road

Approval Requests

- **Development Plan**
- **Conditional Use Permit**
- **CEQA Categorical Exemption**

Project Description

- **Development Plan (DP):**
One office space of 585 SF; 14 one-bedroom units of 11,520 SF; 2,152 SF of common areas; and 20 parking spaces.

- **Conditional Use Permit (CUP):**
Required for Mixed-Use in OI Zoning District and establishes parameters of the use.

Preliminary Housing Development Application (SB 330) and Housing Accountability Act (HAA)

- **SB330:** 5 total hearings allowed, development rights vested May 2022.

- **HAA:** City must disregard density-bonus units and concessions/waivers when determining project consistencies with objective standards.
  - City must make findings based upon substantial evidence to disapprove or condition the approval.

State Density Bonus Law

- Eligible for 50% density bonus, project is requesting 30% density bonus.
- Project includes 2 low-income affordable units.
- 2 concessions requested:
  - Reduction of heat island effect requirement.
  - Increase of height from 35’ to 37’2”.

Inclusionary Housing

- Five or more housing units required to provide 20% of total units to be deed restricted for occupancy by certain income levels.

- 2 units are deed restricted for low-income households.

- Housing In-Lieu Fee for 0.2 of a unit required.

Vehicle Parking

- City requires 32 spaces for residential units, 2 commercial spaces, total of 34 spaces.

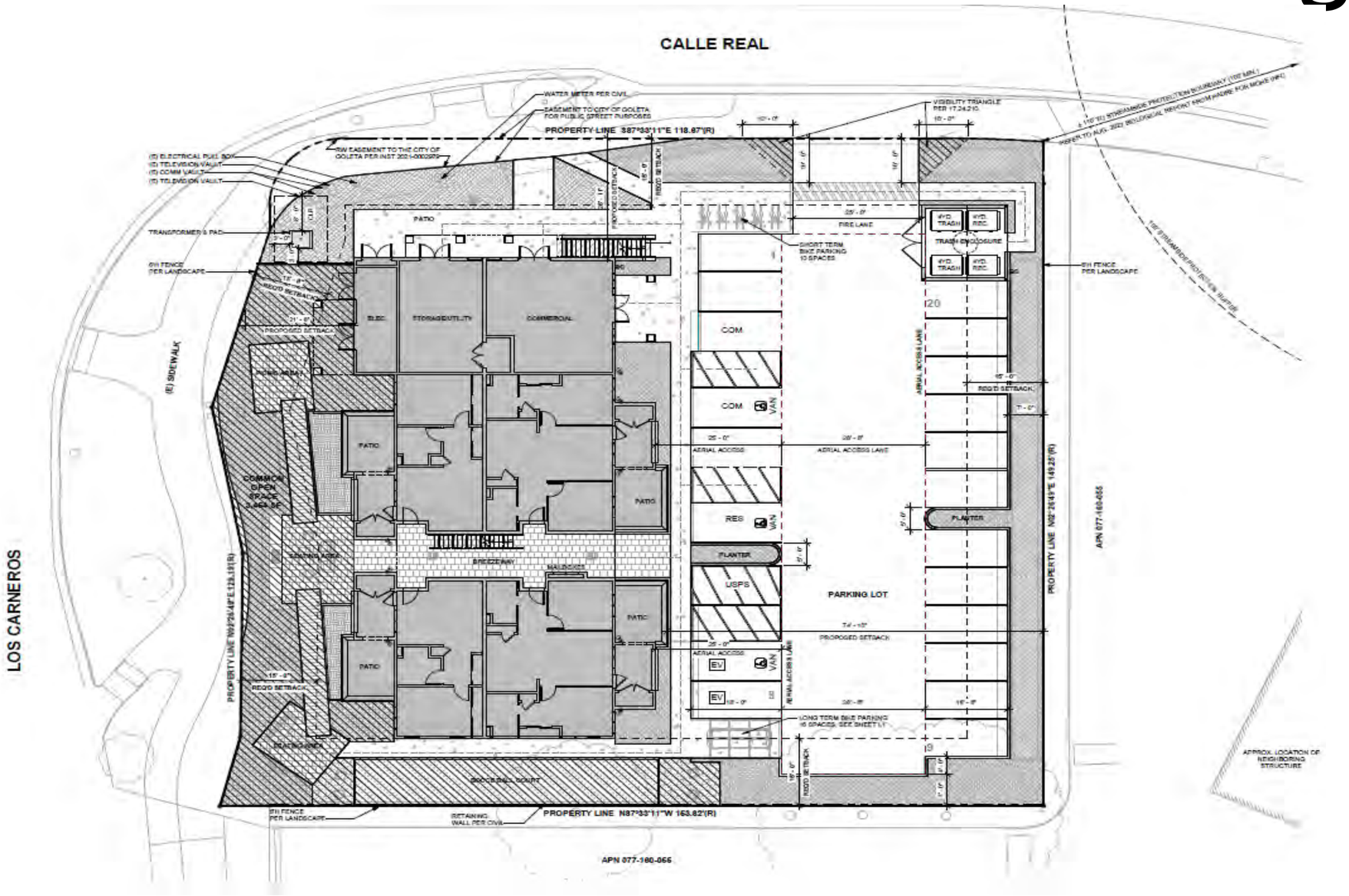
- Using State Density Bonus, 14 spaces for and 2 commercial spaces, total of 16 total spaces.

- 20 uncovered parking spaces proposed.

Background

- May 2022 – Preliminary Housing Development Application (SB 330) Submitted and Deemed Complete
- June 2022 - Project application Submitted
- January 2023 – Application deemed complete
- July 2023 - CEQA Categorical Exemption made available
- July 2023 – Preliminary Design Approval recommended by Design Review Board

Site Plan for the Mixed-Use building



Architectural Style

The project received overall favorable DRB review on July 25, 2023.

Building Height

The project is using a concession to increase height 2’2” above 35’

Reasons for Development Plan

- Any structural development is subject to provisions where a Development Plan is required.

- Purpose of the Development Plan is to provide Discretionary Review and comprehensive analysis at a public hearing.

California Environmental Quality Act (CEQA)

Project meets all criteria for Class 32 Categorical Exemption (Section 15332 CEQA Guidelines).

Draft Notice of Exemption available to public since July 2023.

Project Consistency

General Plan – Project is consistent with General Plan policies recognizing Mixed-Use Development as allowable in the OI zone.

Zoning – Project is consistent with Zoning regulations and applicable Development standards.

Recommendation

That Planning Commission approve the project design and all associated components:

- Development Plan
- Conditional Use Permit
- CEQA Exemption