Agenda Item B.5
CONSENT CALENDAR
Meeting Date: August 21, 2018

TO: Mayor and Councilmembers

FROM: Charles W. Ebeling, Public Works Director

CONTACT: James Winslow, Senior Project Engineer

SUBJECT: Approval of the Crosswalk at South Kellogg Avenue Project Plans and Specifications, Authorization to Advertise a Notice Inviting Bids, and Amendment No. 1 to Professional Design Services Agreement No. 2018-007 with Kimley-Horn and Associates, Inc.

RECOMMENDATION:

A. Approve Plans and Specifications for the Crosswalk at South Kellogg Avenue Project;

B. Authorize staff to advertise a notice inviting bids for the Crosswalk at South Kellogg Avenue Project; and

C. Authorize the City Manager to execute Amendment No. 1 to Professional Design Services Agreement No. 2018-007 with Kimley-Horn and Associates, Inc. to modify the scope of work to include additional services, change the Project Manager, and increase the contract amount by $20,000 for a new total not to exceed amount of $41,000.

BACKGROUND:

The Crosswalk at South Kellogg Avenue Project (Crosswalk Project) [Capital Improvement Program (CIP) Project No. 9098] is located adjacent to the Hollister/Kellogg Park Project (CIP No. 9035), which is a proposed four-acre neighborhood park located on the east side of Kellogg Avenue, north of Hollister Avenue. The park project is currently under construction. Neighborhood Services and Public Safety began the Hollister/Kellogg Park Project design in 2012 when Council authorized the Design Services Contract with Van Atta Associates, Inc. Neighborhood Services and Public Safety held several public workshops between 2012 to 2014. On September 1, 2015, Council provided direction on the design of the park project. Amid discussion of the park project, Council inquired about a “mid-block crosswalk” as a safe passage for the hundreds of children living in Villa La Esperanza who would be traveling to the new park. This crosswalk project will address Council’s concern and is planned to be delivered at or before completion of the park improvements.

On February 14, 2018, the City Manager executed a Professional Design Services Agreement (No. 2018-007) with Kimley-Horn and Associates, Inc. for the design of the Hollister/Kellogg Park Street Improvement Project (previous name for the Crosswalk Project) for a total not-to-exceed $21,000 and an expiration date of June 30, 2019. Neighborhood Services and Public Safety originally managed the Crosswalk Project, but in May 2018, the Public Works Department took over the responsibility for delivery of the project.

On May 1, 2018, Council awarded Construction and Construction Management, Inspection, and Testing contracts for the Hollister/Kellogg Park Project. The Public Works Department has committed to completing the Crosswalk Project in conjunction with the completion of the Hollister/Kellogg Park Project construction.

On June 5, 2018, Council held a Public Hearing for the Approval of the 2018-19 Community Development Block Grant (CDBG) Action Plan. The City received a higher allocation of CDBG funds from the U.S. Department of Housing and Urban Development (HUD) than originally estimated. Neighborhood Services and Public Safety recommended all the CDBG capital funds, totaling $140,633, be applied to the Crosswalk Project to provide for construction management, staff time, and additional design costs. The original budgeted amount of $103,000 provided for the construction estimate only. Council approved the CDBG Action Plan for Fiscal Year 2018-19.

DISCUSSION:

The Crosswalk Project completes a direct and enhanced path of travel primarily from residents of the apartment complex on the west side of South Kellogg Avenue – Villa La Esperanza located at 111 S. Kellogg Avenue – into the heart of the Hollister/Kellogg Park Project. The new park will increase pedestrian traffic across South Kellogg Avenue from Old Town Goleta and Villa La Esperanza. South Kellogg Avenue is also a heavily traveled road used by trucks and equipment to and from the commercial areas to the north between Armitos Avenue and US 101. The nearest existing crosswalks to the park on South Kellogg Avenue are located at the intersections of Hollister Avenue to the south approximately 342 feet and Armitos Avenue to the north approximately 562 feet. The overall distance between Hollister Avenue and Armitos Avenue is approximately 904 feet. The distance between crossings merits a crosswalk at the Hollister/Kellogg Park western entrance due to anticipated increases in pedestrian travel to the new park. The Public Works Department will construct the Crosswalk Project which includes curb extensions, a rectangular rapid flashing beacon (RRFB) crosswalk system, new striping and signage, and drainage improvements.

South Kellogg Avenue is a relatively flat road with few connections to drainage inlets. Accordingly, the crosswalk design maintains the current curb and gutter alignment for stormwater drainage while minimizing ponding at the new crosswalk ramp locations. Constrained by existing grades, the design employs a trench drain between the curb and the back of the curb ramp with a cover plate that complies with the Americans with Disabilities Act. In addition to accommodating drainage along this relatively flat section of roadway, Public Works will be installing curb extensions at the crosswalk location. The

curb extensions are approximately 6-feet-wide and extend into the parking lane. The locations of these curb extensions will provide multiple benefits. Curb extensions shorten the distance for children and adults crossing South Kellogg Avenue and improve the sight distance for both pedestrians and drivers. They provide a clearer line of sight and improved visual cues. Both features are crosswalk safety improvements.

Combined with additional signage and striping, the RRFB crosswalk system will provide a new crosswalk to meet the needs of the residents and park users. The Public Works Department will install solar panels to power the RRFB system. The RRFBs are light-emitting-diode (LED) systems that employ alternative flashing warning light patterns alerting drivers to pedestrians intending to cross the street (see Figure 1).

Figure 1 - RRFB System

Construction is anticipated to begin in the Fall 2018. The Crosswalk Project will be completed prior to the opening of the Hollister/Kellogg Park Project.

<u>Amendment No. 1</u>

The initial Professional Design Services Agreement with Kimley-Horn included studying and preparing a traffic engineering study, gathering existing traffic speed and collision data, preparing crosswalk recommendations, preparing the 95 percent and 100 percent crosswalk improvements, signage and striping, and electrical plan sheets, and preparing the specifications according to the latest American Public Works Association Greenbook and 2015 Caltrans standards. The Public Works Department is recommending additional scope of work that are components for the standard project delivery process. The additional scope includes the following: responding to requests for information during the bidding process, providing design support during construction, completing the project record drawings following construction, providing final changes to the project plans and specifications to include removing electrical conduits, potholing information, tying into the Hollister/Kellogg Park entrance, and Federal CDBG requirements, and providing cost estimates for ordering the signal equipment in advance.

The RRFB system and lighting equipment lead times are approximately 4 to 6 weeks. To avoid possible procurement delays, Public Works is recommending ordering the equipment in advance and providing it to the contractor as owner supplied materials. In

addition, Neighborhood Services and Public Safety has transitioned the Crosswalk Project to Public Works to manage. Amendment No. 1 provides for changing the City's Project Manager to the Public Works Department. The Public Works Department recommends that Council authorize Amendment No. 1 to the Kimley-Horn Professional Design Services Contract, increasing the contract amount by $20,000 for a new total not to exceed amount of $41,000. Following Council's authorization, Kimley-Horn will provide the final changes to the plans and specifications and assist the City with pre-ordering the signal equipment.

Plans, Specifications and Bidding

Public Works has overseen the design plans and specifications. The project design is complete and Public Works will work with the consultant to finalize the project specifications. Due to the desire to complete the Crosswalk Project as quickly as possible, the team is requesting Council take action on the 95% bid specifications and authorize Public Works to bid the project once the remaining 5% is complete. The changes will not affect the design, only the materials supplied and timing. Following City Council's approval, Public Works will make the final minor revisions and issue a notice inviting bids. Public Works anticipates returning to Council with a proposed construction contract for award in Fall 2018.

The proposed design of the crosswalk improvement is attached to this report (Attachment 1). The plans and specifications for the Crosswalk Project consist of the plan sheets, bid documents, the City's General Provisions, Project Special Provisions, and Federal CDBG contract requirements. The specifications also incorporate by reference the Caltrans Standard Specifications 2015 edition, the Caltrans 2015 Standard Plans, Occupational Safety and Health Administration (OSHA) regulations (the Construction Safety Orders), the Manual of Uniform Traffic Control Devices (MUTCD) and the Caltrans Construction Manual. The full set of plans and contract specifications are available for review in the City Clerk's office.

GOLETA STRATEGIC PLAN:

The Crosswalk at S. Kellogg Avenue Project is consistent with the City's Strategic Plan to address infrastructure improvements in Old Town.

City-Wide Strategy: Return Old Town to a Vital Center of the City
Strategic Goal: Address infrastructure improvements in Old Town

FISCAL IMPACTS:

There are sufficient CDBG funds in the FY18/19 Adopted Budget to cover the Amendment No. 1 to Professional Design Services Agreement No. 2018-007 with Kimley-Horn to increase the contract amount by $20,000 for a new total not to exceed amount of $41,000. The requested contract increase covers additional design components associated with potholing, lighting changes, and sidewalk connection; design support services during construction; and preparing the record drawings following construction. Funding adjustments are likely necessary for the construction phase including construction

management, inspection and testing, and staff time. Total project cost estimates are projected to be $185,164. The FY 18/19 Adopted Budget included $161,633 in total funding, resulting in an estimated $23,531 needed to support total project cost estimates. Public Works will confirm this need once the bids are received and request tan additional appropriation at the time the contract is recommended for award if needed.

Project Cost Estimates

Project Components	Estimated Costs
Labor*	$20,000
Consultants	$41,000
Construction*	$99,331
Construction Management*	$14,900
Contingency*	$9,933
Totals	**$185,164**

Funding Source	Funding Amounts
General Fund	$21,000
CDBG	$140,633
TBD	$23,531
Totals	**$185,164**

* Estimated costs.

Project Funding

Project Name: Crosswalk at S. Kellogg Avenue		
Account	**Fund Type**	**FY 2018-19 Adopted Budget**
101-5-9098-706	General Fund	$21,000
402-5-9098-706	CDBG Fund	$140,633
	Total	**$161,633**

ALTERNATIVES:

The Council may elect not to approve the plans and specifications, authorize the notice of invitation to bids for the Project, or amend the design contract. This would delay the construction of the Crosswalk Project and disrupt the coordination with the Hollister/Kellogg Park Project construction completion.

Reviewed By:	**Legal Review By:**	**Approved By:**
Carmen Nichols Deputy City Manager	Michael Jenkins City Attorney	Michelle Greene City Manager

ATTACHMENTS:

1. Plans for Crosswalk at S. Kellogg Avenue Project
2. Amendment No. 1 to Professional Design Services Agreement No. 2018-007 with Kimley-Horn and Associates, Inc.
3. Professional Design Services Agreement No. 2018-007 with Kimley-Horn and Associates, Inc. – Available online only

ATTACHMENT 1

Plans for Crosswalk at S. Kellogg Avenue Project

SHEET INDEX		
SHT. NO.	DWG ID.	DESCRIPTION
01	T-1	TITLE SHEET
02	G-1	GENERAL NOTES
03	X-1	TYPICAL SECTIONS PLAN
04	DM-1	DEMOLITION AND UTILITY PLAN
05	SI-1	STREET IMPROVEMENT PLAN
06 TO 08	CD-1 TO CD-3	CONSTRUCTION DETAILS
09	SS-1	SIGNING AND STRIPING PLAN
10	E-1	ELECTRICAL PLAN
11	EC-1	TEMPORARY EROSION CONTROL PLAN

CITY OF GOLETA

PUBLIC WORKS DEPARTMENT

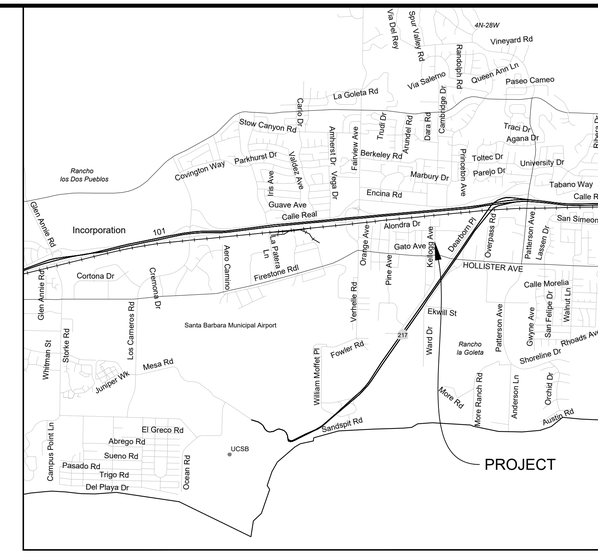
PROJECT PLANS FOR CONSTRUCTION

CROSSWALK AT S. KELLOGG AVENUE

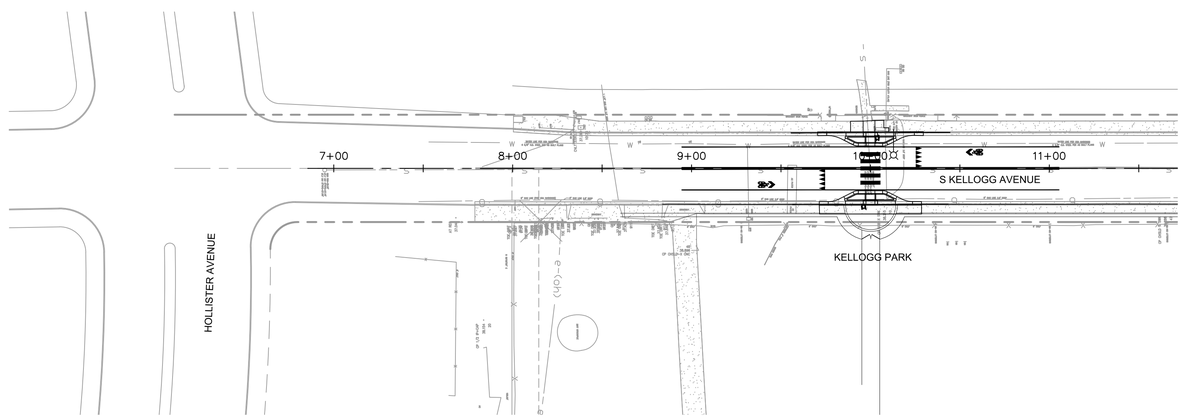
IN SANTA BARBARA COUNTY
ON KELLOGG AVENUE

TO BE SUPPLEMENTED BY STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION "GREENBOOK" DATED 2015
STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION "GREENBOOK" DATED 2012
CALTRANS 2015 STANDARD PLANS
AND SANTA BARBARA COUNTY STANDARD DETAILS DATED SEPTEMBER 2011

PROJECT NO. 9098



VICINITY MAP
NO SCALE



LOCATION MAP
SCALE: 1"=40'

NORTH

GRAPHIC SCALE IN FEET
0 20 40 80

UTILITY COORDINATION

SOUTHERN CALIFORNIA EDISON
103 DAVID LOVE PLACE
GOLETA, CA 93117
HUGO AYALA
(805) 683-5212

ATT TRANSMISSION
22311 BROOKHURST ST, SUITE 203
HUNTINGTON BEACH, CA 92646
JOSEPH FORKERT
(714) 963-7964

COX COMMUNICATIONS
PO BOX 787
GOLETA, CA 93117
CARL GIVENS
(805) 681-3842

GOLETA SANITARY DISTRICT
1 WILLIAM MOFFETT PLACE
GOLETA, CA 93117
LUIS ASTORGA
(805) 967-4519

NEXTGLAVEN CROWN CASTLE
2000 CORPORATE DR
CANONSBURG, PA 15317
BRYANT LOWE
(724) 416-2193

GOLETA WATER DISTRICT
4699 HOLLISTER AVE
GOLETA, CA 93110
DAN BROOKS
(805) 879-4625

LEVEL 3 COMMUNICATIONS
1025 ELDORADO BLVD
BROOMFIELD, CO 80021
(877) 366-8344

FRONTIER COMMUNICATIONS
201 FLYNN ROAD
CAMARILLO, CA 93012
JON TRELATSKY
(805) 388-2266

SOUTHERN CALIFORNIA GAS
9400 OAKDALE AVE
CHATSWORTH, CA 91311
SAM SIFUENTES
(818) 701-3245

SPRINT
2592 DUPONT DR
IRVINE, CA 92612
TIBOR LAKY
(800) 659-9698

PROPERTY LINES ARE APPROXIMATE

811 Know what's below. Call before you dig.

PROJECT ENGINEER
REGISTERED CIVIL ENGINEER

DATE

REGISTERED PROFESSIONAL ENGINEER
ADAM S. CHASE
No. 74043
Exp. 06-30-19
CIVIL
STATE OF CALIFORNIA

PLANS APPROVAL DATE
Kimley-Horn, Inc. or City of Goleta shall not be responsible for the accuracy or completeness of electronic copies of this plan.

100% SUBMITTAL NOT FOR CONSTRUCTION

CITY OF GOLETA

Kimley»Horn
660 S. FIGUEROA ST, SUITE 2050
LOS ANGELES, CA 90017
PHONE: 213-261-4040
WWW.KIMLEY-HORN.COM

PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION
APPROVED:
CITY ENGINEER DATE
ORIGINAL SIGNED DATE

DESIGN ASC
DRAWN PC
CHECKED CC

100 % SUBMITTAL

NO.	DATE	APPROVED	REVISIONS

CITY OF GOLETA
CROSSWALK AT S. KELLOGG AVENUE
TITLE SHEET

CIP 9098
PBW. NO.
BID NO. SHT. DES.
T-1
DWG. NO.
SHT. 01 OF 11

K:\LDT_RDWY\0996800X - Kellogg Midblock Crossing\Design\2 Sheets\1-Title Sheet.dwg; 7/3/2018 6:03 PM; Georgalis, Yiota

GENERAL NOTES

1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE COUNTY OF SANTA BARBARA DEPARTMENT OF PUBLIC WORKS ENGINEERING DESIGN STANDARDS.
2. WORK HOURS SHALL BE FROM 7:30 A.M. TO 4:30 P.M., MONDAY THROUGH FRIDAY, UNLESS OTHERWISE SPECIFIED BY THE PUBLIC WORKS DEPARTMENT.
3. THE HOURS SHALL BE STRICTLY ADHERED TO. ANY DEVIATION FROM THE SCHEDULE REQUIRES PRIOR APPROVAL FROM THE CITY TRAFFIC ENGINEER.
4. CONTRACTOR SHALL MAINTAIN A CLEAN AND ORDERLY WORK ZONE AND IS RESPONSIBLE FOR RESOLVING COMPLAINTS GENERATED BY THE ENCROACHMENT WORK.
5. NO DEBRIS OR SPOILS SHALL BE LEFT IN THE PUBLIC RIGHT-OF-WAY OVERNIGHT.
6. LIMITS OF CONCRETE REMOVAL SHALL BE APPROVED BY THE FIELD REPRESENTATIVE PRIOR TO START OF ENCROACHMENT WORK.
7. ALL CONCRETE THAT IS REMOVED SHALL BE SAW CUT AND REMOVED TO THE NEXT EXPANSION JOINT OR SCORE MARK.
8. ALL CUTS ARE TO BE PERPENDICULAR TO THE CURB AND GUTTER WITH NO LONGITUDINAL CUTS OR DIAGONAL CUTS.
9. ANY CURB, GUTTER, OR SIDEWALK SECTIONS WHICH ARE TO BE REMOVED AND/OR CONSTRUCTED SHALL BE REPLACED IN MINIMUM 5 FOOT (1.524 METERS) SECTIONS IN ACCORDANCE WITH THE COUNTY OF SANTA BARBARA DEPARTMENT OF PUBLIC WORKS ROAD STANDARDS AND AS DIRECTED BY THE PUBLIC WORKS DEPARTMENT.
10. CONSTRUCTION OF CURB, GUTTER, SIDEWALKS, AND RAMPS SHALL BE PER ROAD STANDARD DETAILS 3-100, 3-110, 4-030, 4-040, AND 4-060.
11. ALL CONCRETE CONSTRUCTION TO INCLUDE RAMPS SHALL BE IN ACCORDANCE WITH THE MOST CURRENT AMERICANS WITH DISABILITIES ACT (A.D.A.) REQUIREMENTS AND REGULATIONS.
12. FORM INSPECTIONS WILL BE REQUIRED PRIOR TO PLACEMENT OF CONCRETE AND AT THE COMPLETION OF WORK.
13. CONSTRUCTION OF SIDEWALK AND CURB-GUTTER SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION. FOR CURING, FINISHING AND CLEANING OF CURBS, SEE THE STANDARD SPECIFICATIONS, STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION.
14. ALL CONCRETE SHALL BE CLASS B.
15. EXPANSION JOINTS 1/4 INCH MAXIMUM IN THICKNESS, PRE-MOLDED TO THE SECTION OF THE BACK OF CURB AND SIDEWALK, SHALL BE OF MATERIAL CONFORMING TO THE SPECIFICATIONS OF ASTM DESIGNATION: 01715, AND SHALL BE PLACED AT THE BEGINNING AND END OF EACH CURVE, ON EACH SIDE OF STRUCTURES, AT DROP CURB MULTIDWELLING AND COMMERCIAL DRIVEWAYS (SEE DETAILS 3-070, 3-080 AND 3-090), AND AT OTHER LOCATIONS AS DIRECTED BY THE DIRECTOR OF PUBLIC WORKS. IN EXPANSIVE SOIL CONDITIONS, EXPANSION JOINT MATERIAL AS SPECIFIED ABOVE SHALL BE PLACED AT INTERVALS NOT TO EXCEED 20 FEET.
16. SIDEWALK CONTRACTION JOINTS 1-1/2" IN DEPTH SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 10'. CONTRACTION JOINTS MAY BE "QUICK-JOINT" OR AN APPROVED EQUAL.
17. CURB AND GUTTER CONTRACTION JOINTS 1-1/2" IN DEPTH SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 10'. SEE STANDARD DETAIL 4-040
18. 1/4" SCORE MARKS SHALL BE PROVIDED WITH A TOOL WHICH WILL LEAVE ROUNDED EDGES.
19. WHEN THE WIDTH EXCEEDS 6', LONGITUDINAL SCORE AT THE CENTER OF THE SIDEWALK SHALL BE PROVIDED. ADDITIONAL WIDTH TO MAINTAIN A MINIMUM OF 4' OF CLEAR SIDEWALK SHALL BE PROVIDED WHEN OBSTRUCTIONS SUCH AS UTILITIES EXIST.
20. REMOVAL OF THE SIDEWALK MAY BE REQUIRED IF THE EXISTING SIDEWALK IS BROKEN OR NOT CONSTRUCTED TO THE CORRECT ELEVATION.
21. ALL SIDEWALKS SHALL HAVE A MINIMUM THICKNESS OF 4 INCHES. ALL GUTTERS SHALL HAVE A MINIMUM THICKNESS OF 6" CLASS B CONCRETE. ALL CROSS GUTTERS SHALL HAVE A MINIMUM THICKNESS OF 8" CLASS B CONCRETE.
22. WHEN STRUCTURAL SECTION EXCEEDS 0.70', A MINIMUM OF 0.35' CLASS 2 AGGREGATE BASE, WILL BE REQUIRED UNDER CURBS AS SHOWN AND COMPACTED TO NOT LESS THAN 95% MAXIMUM DENSITY.
23. NATIVE MATERIAL SHALL BE COMPACTED TO 95% MAXIMUM DENSITY TO A DEPTH OF 0.75'. IN EXPANSIVE SOIL CONDITIONS, A MINIMUM OF 0.35' OF CLASS 2 AGGREGATE BASE WILL BE REQUIRED AND COMPACTED TO NOT LESS THAN 95% MAXIMUM DENSITY.
24. SMOOTH STRUCTURAL STEEL DOWELS ARE TO BE LUBRICATED PRIOR TO PLACING CLASS B CONCRETE. PLACE DOWELS AT EXPANSION JOINTS AS SHOWN ON STANDARD DETAIL 3-020.
25. WHEN THE STRUCTURAL SECTION EXCEEDS 0.70', A MINIMUM OF 0.35' CLASS 2 AGGREGATE BASE SHALL BE PLACED UNDER THE CROSS GUTTERS AS SHOWN AND COMPACTED TO NOT LESS THAN 95% MAXIMUM DENSITY.
26. NOTIFY THE CITY'S REPRESENTATIVE AND ALL OTHER INTERESTED PARTIES AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO THE START OF WORK.
27. AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO COMMENCING CONSTRUCTION, CONTACT THE REGIONAL NOTIFICATION CENTER (UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA – U.S.A. AT 1-800-227-2600) AND REQUEST THAT UTILITY OWNERS MARK OR OTHERWISE INDICATE THE LOCATION OF THEIR FACILITIES. BEFORE COMMENCING ANY EXCAVATION, POTHOLE AND EXPOSE THE EXISTING UTILITIES AT ALL CROSSINGS AND AT THE POINT OF TIE-IN; THEN CONTACT THE CITY'S REPRESENTATIVE SO THE ELEVATION OF THE EXISTING FACILITIES CAN BE VERIFIED. TAKE ALL NECESSARY MEASURES TO PROTECT ALL UTILITIES AND ALL STRUCTURES FOUND AT THE SITE.
28. ALL REQUIRED PERMITS AND NECESSARY CITY BUSINESS LICENSE(S) SHALL BE OBTAINED BY THE CONTRACTOR PRIOR TO BEGINNING CONSTRUCTION.
29. DURING ALL PHASES OF CONSTRUCTION, INCLUDING SUSPENSION OF WORK, UNTIL FINAL ACCEPTANCE OF THE PROJECT, OBSERVE, FOLLOW AND IMPLEMENT ALL THE REQUIREMENTS OF THE NPDES AND STORMWATER POLLUTION PREVENTION PROGRAM AND KEEP THE WORK SITE CLEAN AND FREE FROM RUBBISH AND DEBRIS. ALSO ABATE DUST NUISANCE BY CLEANING, SWEEPING AND SPRINKLING WITH WATER AND USING DUST FENCES OR THEIR METHODS AS DIRECTED BY THE CITY'S REPRESENTATIVE THROUGHOUT THE CONSTRUCTION OPERATION.
30. KEEP A STRICT RECORD OF ALL CHANGES AND SUBMIT THIS RECORD TO THE CITY PUBLIC WORKS DEPARTMENT. ALSO COORDINATE TRANSFERRING "AS-BUILT" INFORMATION ON THE CONTRACT DRAWINGS AND DELIVER THE CERTIFIED "AS-BUILT" PLANS TO THE CITY BEFORE THE RELEASE FOR OCCUPANCY OR FINAL ACCEPTANCE OF THE PROJECT SHALL BE FILED.
31. EXERCISE DUE CARE TO AVOID INJURY TO EXISTING IMPROVEMENTS OR FACILITIES, UTILITY FACILITIES, ADJACENT PROPERTY, AND TREES AND SHRUBBERY THAT ARE NOT TO BE REMOVED. ALL DAMAGE CAUSED TO PUBLIC STREETS, INCLUDING HAUL ROUTES, ALLEYS, SIDEWALKS, CURBS OR STREET FURNISHINGS, OR TO PRIVATE PROPERTY SHALL BE REPAIRED AT THE SOLE EXPENSE OF THE CONTRACTOR TO THE SATISFACTION OF THE CITY'S REPRESENTATIVE.
32. DESIGNATE AND KEEP ON THE PROJECT AT ALL TIMES WHILE WORK IS BEING PERFORMED A COMPETENT SUPERINTENDENT WHO SHALL NOT BE REPLACED WITHOUT A WRITTEN NOTICE TO THE CITY'S REPRESENTATIVE. THE SUPERINTENDENT WILL BE THE CONTRACTOR'S REPRESENTATIVE AT THE SITE AND SHALL HAVE AUTHORITY TO ACT ON BEHALF OF THE CONTRACTOR. ALL COMMUNICATIONS GIVEN TO THE SUPERINTENDENT SHALL BE AS BINDING AS IF GIVEN TO THE CONTRACTOR. DURING PERIODS WHEN THE WORK IS SUSPENDED, MAKE APPROPRIATE ARRANGEMENTS FOR ANY EMERGENCY WORK WHICH MAY BE REQUIRED.
33. SECTION 4-1.4 "TEST OF MATERIALS" OF THE "GREENBOOK" SHALL BE MODIFIED TO READ "THAT THE COST OF ALL INITIAL TESTING AND RETESTING TO BE PERFORMED UNDER THE DIRECTION OF THE CITY'S REPRESENTATIVE SHALL BE BORNE BY THE CONTRACTOR."
34. WHEN THE WORK ON ANY PORTION IS SUFFICIENTLY COMPLETE TO BE USED OR PLACED INTO SERVICE, THE CITY SHALL HAVE THE RIGHT UPON WRITTEN NOTIFICATION TO THE CONTRACTOR TO UTILIZE SUCH PORTIONS OF THE WORK AND TO PLACE THE OPERABLE PORTIONS INTO SERVICE AND TO OPERATE SAME. UPON SAID NOTICE AND COMMENCEMENT OF UTILIZATION OR OPERATION BY THE CITY, THE CONTRACTOR SHALL BE RELIEVED OF THE DUTY OF MAINTAINING THE PORTIONS SO UTILIZED OR PLACED INTO OPERATION; PROVIDED, HOWEVER, THAT NOTHING IN THIS NOTE SHALL BE CONSTRUED AS RELIEVING THE CONTRACTOR OF THE FULL RESPONSIBILITY FOR COMPLETING THE WORK IN ITS ENTIRETY, FOR MAKING GOOD DEFECTIVE WORK AND MATERIALS, FOR PROTECTING THE WORK FROM DAMAGE, AND FOR BEING RESPONSIBLE FOR DAMAGE.
35. CONDUCT OPERATIONS AS TO OFFER THE LEAST POSSIBLE OBSTRUCTION AND INCONVENIENCE TO THE PUBLIC, AND HAVE UNDER CONSTRUCTION NO GREATER LENGTH OR AMOUNT OF WORK THAN HE CAN PROSECUTE PROPERLY WITH DUE REGARD TO THE RIGHTS OF THE PUBLIC. CONVENIENT ACCESS TO DRIVEWAYS, HOUSES, AND BUILDINGS ALONG THE LINE OF WORK SHALL BE MAINTAINED AND TEMPORARY CROSSINGS SHALL BE PROVIDED AND MAINTAINED IN GOOD CONDITION. NO MORE THAN ONE CROSSING OR INTERSECTION STREET OR ROAD SHALL BE CLOSED AT ANY ONE TIME.
36. UNTIL THE ACCEPTANCE OF THE WORK, THE RESPONSIBLE CHARGE AND CARE OF THE WORK AND OF THE MATERIALS TO BE USED THEREIN (INCLUDING MATERIALS FOR WHICH HE HAS RECEIVED PARTIAL PAYMENT OR MATERIALS WHICH HAVE BEEN FURNISHED BY THE CITY) AND SHALL BEAR THE RISK OF INJURY, LOSS, OR DAMAGE TO ANY PART THEREOF BY THE ACTION OF ELEMENTS OR FROM ANY OTHER CAUSE, WHETHER ARISING FROM THE EXECUTION OR FROM THE NON-EXECUTION OF THE WORK.
37. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK; AND FULLY COMPLY WITH ALL STATE, FEDERAL AND OTHER LAWS, RULES, REGULATIONS, AND ORDER RELATING TO SAFETY OF WORKERS AND ALL OTHERS. THIS MAY INCLUDE THE ISSUANCE OF PERSONAL PROTECTIVE EQUIPMENT.
38. ALL UNDERGROUND UTILITIES OR STRUCTURES REPORTED BY THE OWNER OR THOSE SHOWN ON RECORDS EXAMINED ARE INDICATED WITH THEIR APPROXIMATE LOCATION AND EXTENT. THE CONTRACTOR, BY ACCEPTING THESE PLANS OR PROCEEDING WITH IMPROVEMENTS PURSUANT THERETO, UNDERSTANDS THAT HE AGREES TO ASSUME THE LIABILITY, AND AGREES TO HOLD THE CITY HARMLESS FOR ANY LIABILITY FOR DAMAGE RESULTING FROM THE EXISTENCE OF UNDERGROUND UTILITIES OR STRUCTURES NOT REPORTED TO THE CITY, NOT INDICATED ON THE PUBLIC RECORDS EXAMINED, OR LOCATED AT VARIANCE WITH THAT REPORTED OR SHOWN ON RECORDS EXAMINED. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITIES OR STRUCTURES FOUND AT THE SITE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE OWNERS OF THE UTILITIES CONCERNED BEFORE STARTING WORK.
39. TYPICAL DETAILS APPLY WHETHER OR NOT THEY ARE SPECIFICALLY REFERENCED ON INDIVIDUAL PLANS, DETAILS OR SECTIONS.
40. VERIFY DIMENSIONS AND CONDITIONS AT THE SITE BEFORE STARTING WORK. ANY CONFLICT BETWEEN DETAILS OR DIMENSIONS ON THE DRAWINGS SHALL BE REPORTED PROMPTLY TO THE CITY'S REPRESENTATIVE WHO WILL DETERMINE THE INTENT OF THE DRAWINGS.

TRAFFIC CONTROL

1. TRAFFIC CONTROL SHALL INCLUDE ALL MATERIAL, LABOR, AND EQUIPMENT TO PROVIDE SAFE AND EFFECTIVE WORK AREAS AND TO WARN, CONTROL, PROTECT, AND EXPEDITE VEHICULAR, BICYCLE, AND PEDESTRIAN TRAFFIC. ALL WORK AND MATERIAL PROVIDED UNDER THIS SECTION SHALL BE PERFORMED OR FURNISHED IN ACCORDANCE WITH THE FOLLOWING PUBLICATIONS AS APPLICABLE:
   - STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, LATEST EDITION
   - STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS, LATEST EDITION
   - STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION "MANUAL OF TRAFFIC
   - MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD), LATEST EDITION.
2. NO WORK SHALL BE PERFORMED IN PUBLIC RIGHTS-OF-WAY WITHOUT PERMISSION AND PERMITS FROM THE AUTHORIZING AGENCY.
3. CONSTRUCTION AREA, DETOUR, AND SPECIAL SIGNS SHALL BE FURNISHED, INSTALLED, MAINTAINED, AND REMOVED WHEN NO LONGER REQUIRED IN ACCORDANCE WITH THE PROVISIONS IN PART 6, "TEMPORARY TRAFFIC CONTROL" OF THE GREENBOOK STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
4. SIGNS AND EQUIPMENT SHALL CONFORM TO THE REQUIREMENTS OF THE "UNIFORM SIGN CHART", MUTCD AND THE "MANUAL' OF TRAFFIC CONTROLS – FOR CONSTRUCTION AND MAINTENANCE – WORK ZONES."
5. WORK SHALL BE PLANNED AND CONDUCTED AS TO CREATE THE LEAST POSSIBLE INCONVENIENCE TO THE TRAVELING PUBLIC.

BENCHMARK DATA

BASIS OF BEARINGS PER RECORDED MAPS AND DEEDS
HORIZONTAL: NGS HPGN STATION CA 05AS NAD83, 1991.35
NORTHING: 1988200.329 EASTING: 6022976.580
VERTICAL: NGS BENCHMARK BM F28 NAVD88
ELEVATION: 48.36' US SURVEY FEET

CONTROL POINT #9, PK NAIL & WASHER, N:1986080.8400, E:6012789.0300, ELEV:37.0400, AT REC
#20, N:1986095.3849, E:6012846.4597, ELEV:38.1537, CP 1/2 IP+CAP
#21, N:1986056.4580, E:6013108.3890, ELEV:39.0390, CP SET 1/2 IP+PP
#49, N:1986260.1640, E:6013053.6520, ELEV:43.8250, CP CHSLD-X CNC
#308, N:1986297.7409, E:6012795.0274, ELEV:38.6045, MONF 1/2 IP 3220
#321, N:1986195.6201, E:6012793.5409, ELEV:37.0321, MONF 1/2IP+TAG

ABBREVIATIONS

PROJECT ABBREVIATIONS REFER TO CALTRANS STANDARD PLANS A3A TO A3C AND THE FOLLOWING BELOW:

AC	ASPHALT CONCRETE	K	RATE OF CURVATURE
AB	AGGREGATE BASE	LP	LOW POINT
ALIGN	ALIGNMENT	LT	LEFT
BC	BEGIN CURVE	MAX	MAXIMUM
BF	BOTTOM OF FOOTING	MH	MANHOLE
BFP	BACK FLOW PREVENTOR	MIN	MINIMUM
BVCS	BEGINNING VERTICAL CURVE STATION LABEL	NB	NORTH BOUND
℄ OR CL	CENTER LINE	NE	NORTHEAST
CF	CURB FACE	NW	NORTHWEST
CLF	CHAIN LINK FENCE	OG	ORIGINAL GROUND
CNC	CONCRETE	O/H	OVERHEAD
CONST	CONSTRUCT	PCC	PORTLAND CEMENT CONCRETE
CP	CATCH POINT	PROP	PROPOSED
DWY OR D/W	DRIVEWAY	PVI	POINT OF VERTICAL INTERSECTION
DI	DROP INLET	(R)	RECORD INFORMATION
DIA	DIAMETER	RT	RIGHT
EC	END OF CURVE	ROW OR R/W	RIGHT OF WAY
EG	EXISTING GROUND/GRADE	SB	SOUTH BOUND
EL OR ELEV	ELEVATION	SD	STORM DRAIN
EP	EDGE OF PAVEMENT	SE	SOUTHEAST
EPB OR ELPB	ELECTRIC PULLBOX	SDMH	STORM DRAIN MANHOLE
EX OR EXIST	EXISTING	SSMH	SANITARY SEWER MANHOLE
EVCS	END VERTICAL CURVE STATION LABEL	STA	STATION
FDTN	FOUNDATION	STD	STANDARD
FG	FINISHED GRADE	STLT	STREET LIGHT
FH	FIRE HYDRANT	SW	SOUTHWEST
FL	FLOW LINE	TC	TOP OF CURB
FS	FINISHED SURFACE	TCB	TRAFFIC CONTROL BOX
FSW	FRONT OF SIDEWALK	TCE	TEMPORARY CONSTRUCTION EASEMENT
FT	FEET	TS	TOP OF SLAB
GALV	GALVANIZED	TSPB	TRAFFIC SIGNAL PULL BOX
GB	GRADE BREAK	TW	TOP OF WALL
GL	GUTTER LIP	TYP	TYPICAL
GND	GROUND	UHMW	ULTRA HIGH MOLECULAR WEIGHT POLYETHYLENE
GV	GAS VALVE		
HORIZ	HORIZONTAL	UNO	UNLESS NOTED OTHERWISE
HP	HINGE POINT	UVLT	UTILITY VAULT
ICV	IRRIGATION CONTROL VALVE	VC	VERTICAL CURVE
ICP	IRRIGATION CONTROL PAD	VERT	VERTICAL
INV	INVERT	WM	WATER METER
IRR	IRRIGATION CONTROL BOX	WV	WATER VALVE

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CITY OF GOLETA
CROSSWALK AT S. KELLOGG AVENUE
GENERAL NOTES

CIP 9098
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BID NO. SHT. DES.
G-1
DWG. NO.
SHT. 02 OF 11

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TYPICAL PAVEMENT SECTIONS

1 — 6" AC (CLASS B PG 64–10)
9" CAB

2 — 6" WOOD MULCH
4" CAB

3 — PCC INTEGRAL TRENCH DRAIN
6" CAB

4 — MIN 2" GRIND
MIN 2" AC OVERLAY (CLASS B PG 64–10)

5 — 4" PCC SW
4" CAB

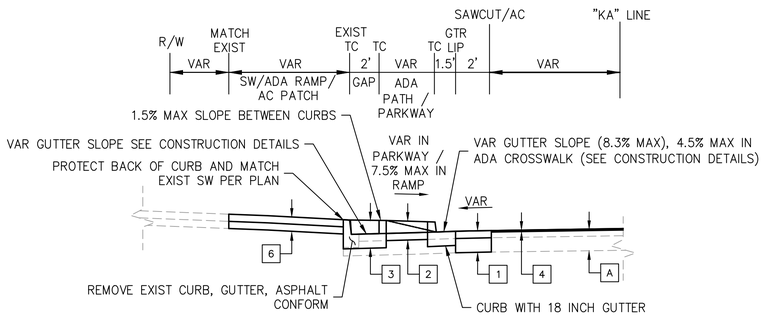
6 — 4" AC PATCH
2" SAND
4" CAB

EXISTING STRUCTURAL SECTIONS

A — 6" AC (ASSUMED)
9" CAB (ASSUMED)

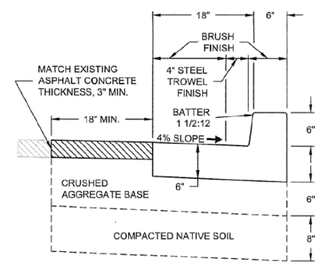
GENERAL NOTES

1. ALL DIMENSIONS FOR THE MEASUREMENT OF LENGTH ARE IN FEET, UNLESS OTHERWISE NOTED.
2. SIDEWALK LAYOUT OF EXPANSION AND CONTRACTION JOINTS SHALL BE PER COUNTY OF SANTA BARBARA STANDARD PLAN 4–040 "SIDEWALK DETAILS" AND AS SHOWN PER THE PLANS.
3. SEE ELECTRICAL PLANS FOR CONDUIT INFORMATION NOT SHOWN.
4. SEE CONSTRUCTION DETAILS FOR ADDITIONAL STREET IMPROVEMENT LAYOUT.
5. EXACT LOCATIONS AND TYPES OF CURBS, CONCRETE, SIDEWALK, ARE SHOWN ON THE LAYOUT SHEETS.
6. KELLOGG AVENUE "KA" LINE STATIONING IS ASSUMED.
7. PLACE CRUSHED AGGREGATE BASE PER SPPWC MIN 6" THICK UNDER CURB AND GUTTER.



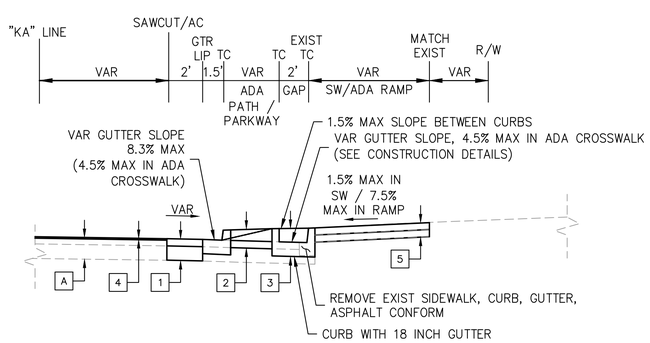
EASTERN CURB EXTENSION

Typical Section



CURB WITH 18 INCH GUTTER

1. ALL CURBS AND GUTTERS SHALL BE PLACED MONOLITHICALLY.
2. PREMOLDED 0.25 INCH THICK EXPANSION JOINTS SHALL BE PLACED AT THE ENDS OF CURB RETURNS AND BACK OF CURB ON EAST SIDE. PROVIDE 1.5 INCH DEEP CONTRACTION JOINTS IN ALL CURB AND GUTTER AT APPROXIMATELY 10 FOOT INTERVALS TO MATCH SCORE MARKS IN EXISTING SIDEWALK.
3. THE TOP EDGE OF CURB, THE GUTTER FLOW LINE AND THE GUTTER EDGE SHALL HAVE 0.5 INCH RADIUS, UNLESS OTHERWISE NOTED.
4. MINIMUM 6 INCH CRUSHED AGGREGATE BASE UNDER CURB AND GUTTER.
5. COMPACT NATIVE SOIL TO A DEPTH OF 8 INCHES BENEATH AGGREGATE BASE BELOW CURB AND GUTTER, TO 90% RELATIVE COMPACTION.
6. CUTS IN EXISTING CURBS AND GUTTERS SHALL BE MADE AT RIGHT ANGLES TO THE FACE OF CURB.
7. WHERE EXISTING CURB HEIGHT VARIES, MATCH EXISTING OR ADJACENT CURB FOR SHORT REACHES.
8. EXTRUDED OR SLIP–FORMED CURB AND GUTTER IS NOT PERMITTED.



WESTERN CURB EXTENSION

Typical Section

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TYPICAL SECTIONS PLAN

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GENERAL NOTES

1. VERIFY THE EXISTENCE AND HORIZONTAL/VERTICAL LOCATION OF ALL UTILITIES WITHIN THE PROJECT AREA THAT MAY CONFLICT WITH THE PROPOSED CONSTRUCTION. VERIFY THAT ANY UTILITIES SHOWN TO BE RELOCATED BY OTHERS HAVE BEEN RELOCATED AND ARE NO LONGER IN CONFLICT. IMMEDIATELY NOTIFY THE ENGINEER OF ANY REMAINING UTILITY CONFLICTS PRIOR TO THE START OF ANY DEMOLITION WORK.
2. CONTRACTOR SHALL MAINTAIN EXISTING ELECTRICAL CIRCUITS FOR EQUIPMENT NOT REMOVED DURING DEMOLITION.
3. FOR SIGN REMOVALS OR RELOCATIONS REFER TO SIGNING AND STRIPING PLAN, SHEET SS–1.
4. SEE ELECTRICAL PLANS FOR ELECTRICAL AND STREET LIGHTING EQUIPMENT REMOVALS, ADJUSTMENTS, RELOCATIONS, AND NEW INSTALLATIONS, SHEET E–1.
7. EXISTING STREET LIGHTS SHALL REMAIN IN OPERATION UNTIL NEW SYSTEMS ARE OPERATIONAL. SEE ELECTRICAL PLANS.
8. UNLESS OTHERWISE SHOWN ON PLANS, CONTRACTOR SHALL CLEAR AND GRUB ALL AREAS WITHIN THE PROJECT GRADING LIMITS AND REMOVE ALL IMPEDANCES FOR THE PROPOSED FACILITIES.

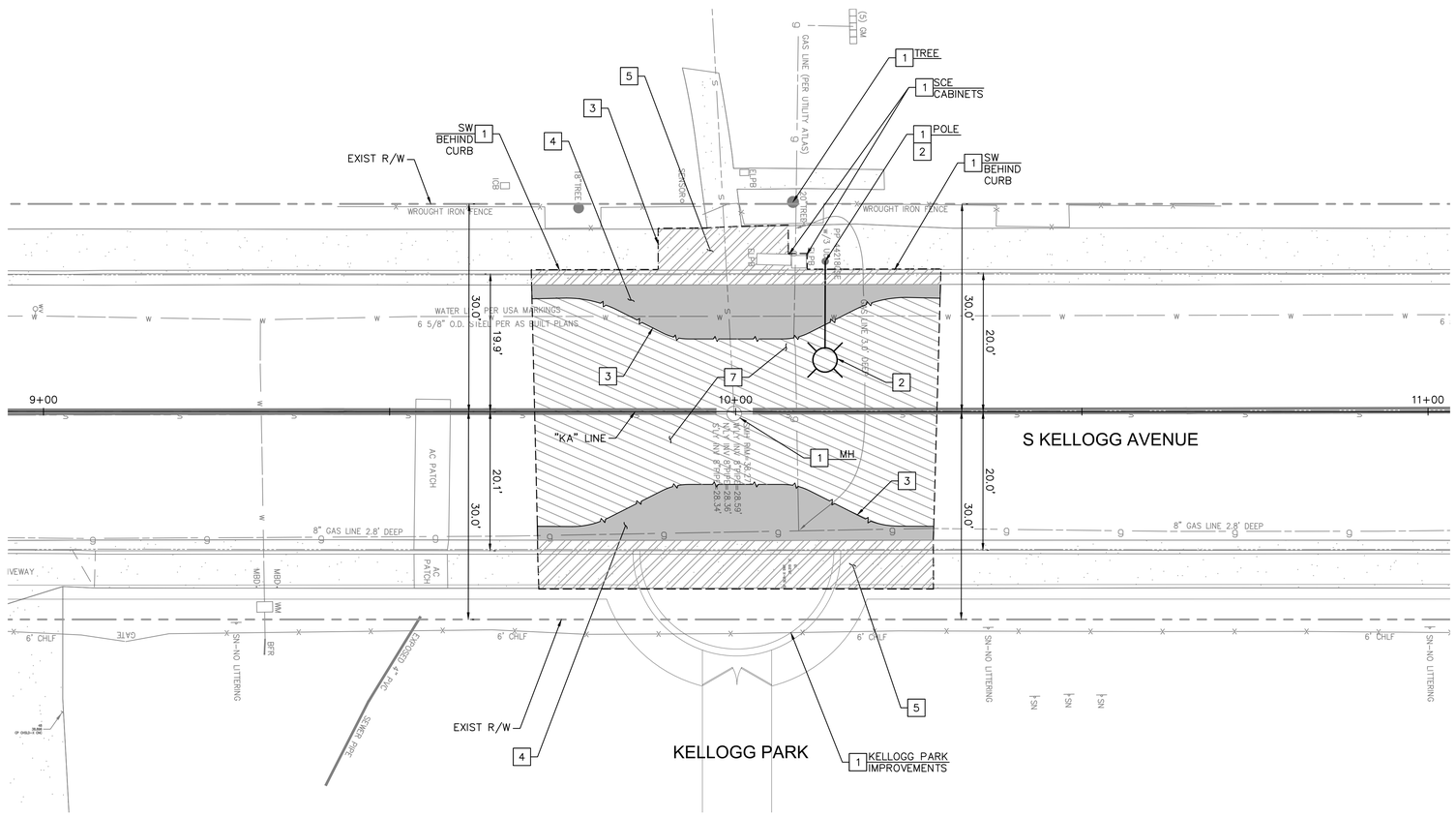
DEMOLITION NOTES

1. PROTECT IN PLACE.
2. REPLACE EXISTING STREET LIGHT WITH NEW LED STREET LIGHT (SCE).
3. SAWCUT EXISTING ASPHALT PAVEMENT, CONCRETE CURB, GUTTER, SIDEWALK.
4. REMOVE AND DISPOSE OF EXISTING ASPHALT PAVEMENT, CRUSHED AGGREGATE BASE, AND SUBGRADE.
5. REMOVE AND DISPOSE OF EXISTING PCC CONCRETE (CURB, GUTTER, SIDEWALK, ADA RAMP, AND MISCELLANEOUS AREAS) AND BASE.
6. SAWCUT, REMOVE AND DISPOSE OF EXISTING PCC CONCRETE SIDEWALK NOTCH FOR NEW POLE FOUNDATION INSTALLATION.
7. GRIND 2" MINIMUM DEPTH OF EXISTING ASPHALT PAVEMENT.

LEGEND

- FULL DEPTH ASPHALT CONCRETE AND BASE REMOVAL
- CONCRETE AND BASE REMOVAL (INCLUDES CURB, SIDEWALK, AND OTHER CONCRETE SURFACING IN CONFLICT WITH THE PROPOSED IMPROVEMENTS)
- GRIND ASPHALT CONCRETE (2" MIN)
- SAWCUT WITH FULL DEPTH REMOVALS
- LIMIT OF IMPROVEMENTS

WESTERN CURB EXTENSION



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CITY OF GOLETA
CROSSWALK AT S. KELLOGG AVENUE
DEMOLITION AND UTILITY PLAN

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2. SEE CONSTRUCTION DETAIL PLANS FOR ELEVATIONS AND CURVE/LINE TABLES FOR PROPOSED GEOMETRY.
3. ALL PROPOSED GUTTER SLOPES AT PEDESTRIAN RAMPS SHALL NOT EXCEED 4.5%, SEE CONSTRUCTION DETAIL PLANS FOR MORE INFORMATION.
4. ALL PROPOSED SIDEWALK SHALL MATCH EXISTING SIDEWALK PATTERN, COLOR, AND FINISH.
5. FOR SIGN REMOVALS OR RELOCATIONS REFER TO SIGNING AND STRIPING PLAN, SHEET SS-1.
6. SEE ELECTRICAL PLANS FOR REMOVALS, RELOCATIONS, AND/OR ADJUSTMENTS SHEETS E-1.
7. CONTRACTOR SHALL MAINTAIN EXISTING ELECTRICAL CIRCUITS FOR EQUIPMENT NOT REMOVED DURING DEMOLITION.

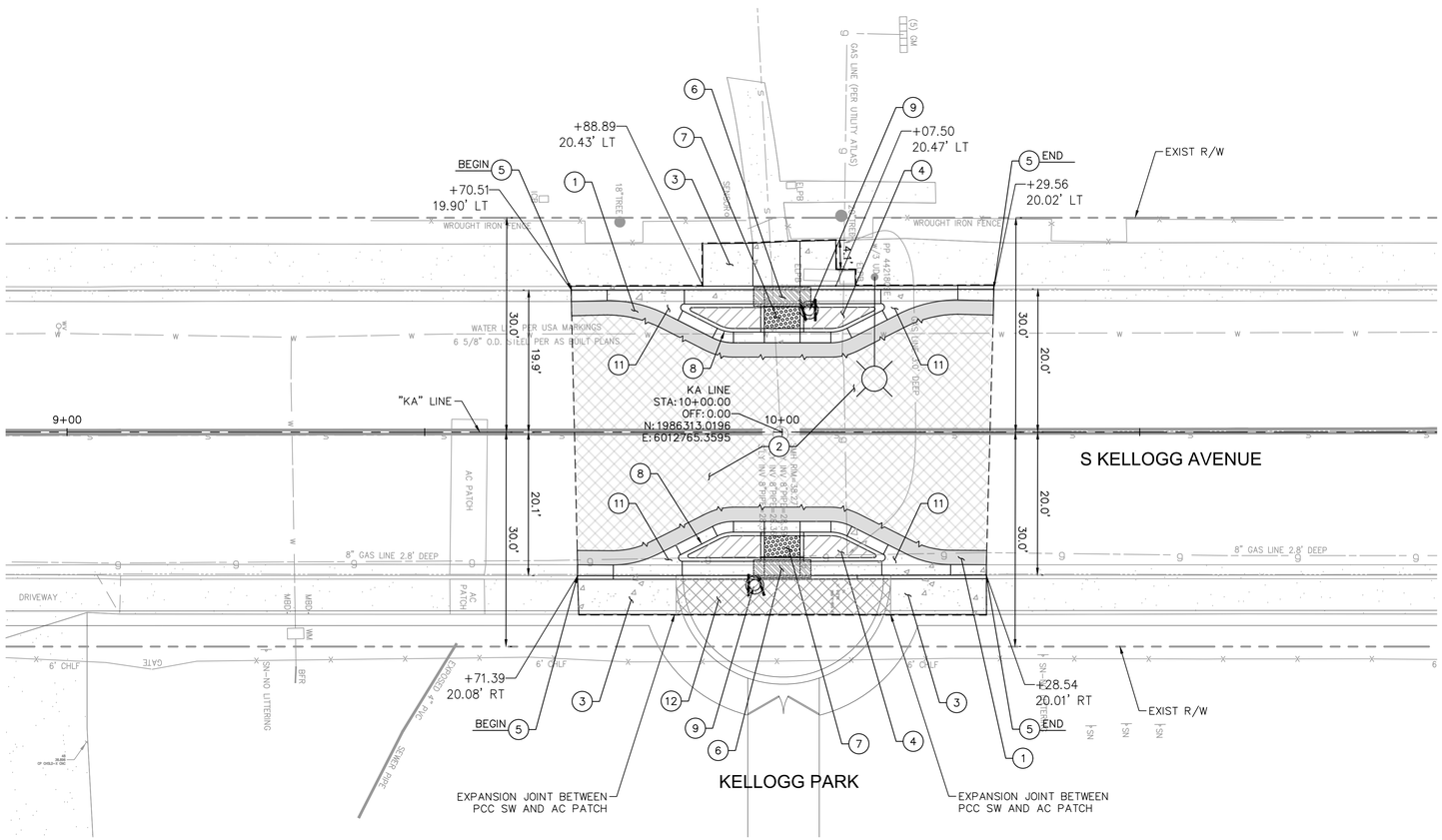
LEGEND

- CONSTRUCT FULL DEPTH ASPHALT CONCRETE AND BASE
- ASPHALT CONCRETE OVERLAY (2" MIN)
- WOOD MULCH
- CONCRETE SIDEWALK AND BASE
- CONSTRUCT AC PATCH
- PCC INTEGRAL TRENCH DRAIN
- DETECTABLE WARNING SURFACE
- SAWCUT WITH FULL DEPTH REMOVALS
- LIMIT OF IMPROVEMENTS

CONSTRUCTION NOTES

1. CONSTRUCT FULL DEPTH PAVEMENT 6" AC (CLASS B PG 64-10) OVER 9" CAB OVER COMPACTED SUBGRADE.
2. CONSTRUCT 2" AC OVERLAY (CLASS B PG 64-10).
3. CONSTRUCT 4" PCC SIDEWALK OVER 4" CAB.
4. CONSTRUCT 6" THICK WOOD MULCH 1" BELOW ADJACENT CURB.
5. CONSTRUCT 6" CURB AND 18" GUTTER OVER 6" CRUSHED AGGREGATE BASE PER TYPICAL SECTIONS, X-1.
6. CONSTRUCT PCC INTEGRAL TRENCH DRAIN COMPLETE, 8' IN LENGTH AS SHOWN SEE ADDITIONAL INFORMATION PER THE CONSTRUCTION DETAIL PLANS.
7. CONSTRUCT CURB RAMP PER CALTRANS STANDARD PLAN RSP A88A WITH 3' DEEP DETECTABLE WARNING SURFACE (CAST-IN-PLACE, COLOR "BRICK RED"). SEE CONSTRUCTION DETAILS FOR ADDITIONAL INFORMATION WITH TRENCH DRAIN DETAIL AND ELEVATIONS.
8. CONSTRUCT REDUCED HEIGHT MODIFIED CURB AND GUTTER OVER 6" CRUSHED AGGREGATE BASE PER TRENCH DRAIN DETAIL ON THE CONSTRUCTION DETAIL SHEETS.
9. CONSTRUCT RRFB SIGNS, POSTS, AND FOUNDATION COMPLETE, SEE ELECTRICAL PLANS.
10. CONSTRUCT STREET LIGHT POLE, FOUNDATION, AND APPURTENANCES, COMPLETE. SEE ELECTRICAL PLANS.
11. CONSTRUCT VARIABLE WIDTH GUTTER PER TYPICAL SECTIONS, X-1. SLOPE VARIES, SEE CONSTRUCTION DETAIL SHEET, CD-1.
12. CONSTRUCT 4" THICK ASPHALT CONCRETE PATCH, OVER 2" SAND, OVER 4" CAB.

WESTERN CURB EXTENSION



EASTERN CURB EXTENSION

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STREET IMPROVEMENT PLAN

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GENERAL NOTES

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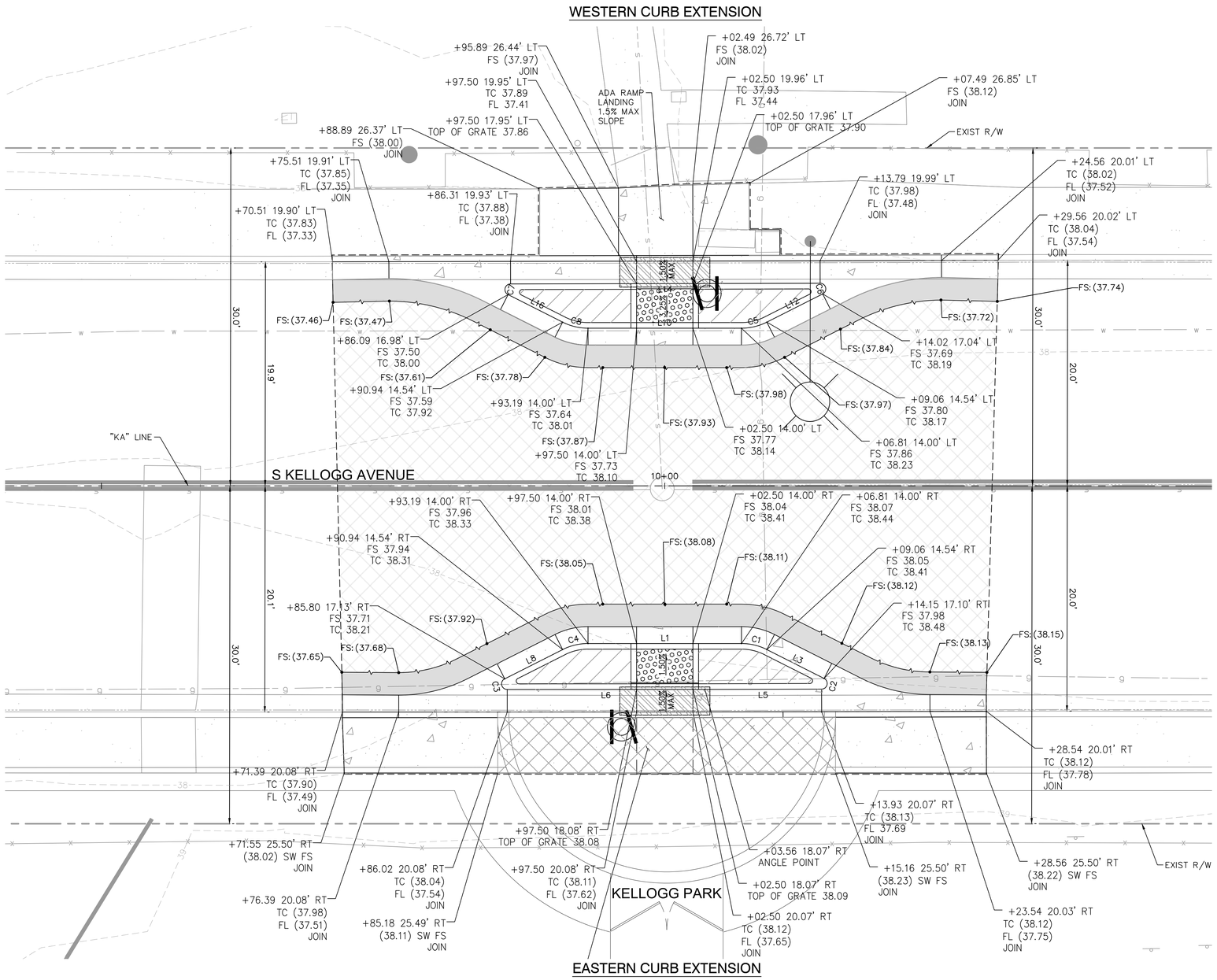
2. ALL PROPOSED GUTTER SLOPES AT PEDESTRIAN RAMPS SHALL NOT EXCEED 4.5%, SEE CONSTRUCTION DETAIL PLANS FOR MORE INFORMATION.

LEGEND

- CONSTRUCT ASPHALT CONCRETE FULL DEPTH AND BASE
- ASPHALT CONCRETE OVERLAY (2" MIN)
- WOOD MULCH
- CONCRETE SIDEWALK AND BASE
- CONSTRUCT AC PATCH
- PCC INTEGRAL TRENCH DRAIN
- DETECTABLE WARNING SURFACE
- SAWCUT WITH FULL DEPTH REMOVALS
- LIMIT OF IMPROVEMENTS
- XX.XX DENOTES NEW ELEVATION
- (XX.XX) DENOTES EXISTING ELEVATION TO MATCH

LINE AND CURVE TABLE				
NO.	DELTA OR BRG	RADIUS	LENGTH	REMARKS
C5	Δ=26° 47' 19"	5.00'	2.34'	
C6	Δ=153° 19' 50"	0.50'	1.34'	
C7	Δ=153° 05' 02"	0.50'	1.34'	
C8	Δ=26° 47' 49"	5.00'	2.34'	
L10	N 0°58'27" E		13.62'	
L12	N 25°48'52" W		5.55'	
L14	S 0°51'18" W		27.48'	
L16	N 27°46'17" E		5.43'	

LINE AND CURVE TABLE				
NO.	DELTA OR BRG	RADIUS	LENGTH	REMARKS
C1	Δ=26° 47' 19"	5.00'	2.34'	
C2	Δ=153° 04' 43"	0.50'	1.34'	
C3	Δ=153° 13' 43"	0.50'	1.34'	
C4	Δ=26° 47' 49"	5.00'	2.34'	
L1	N 0°58'27" E		13.62'	
L3	N 27°45'46" E		5.69'	
L5	S 0°50'29" W		10.36'	
L6	S 0°56'55" W		17.54'	
L8	N 25°49'22" W		5.76'	



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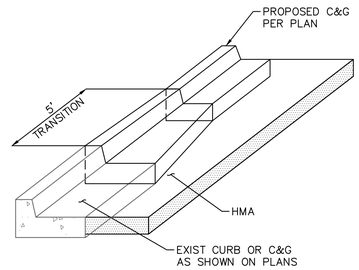
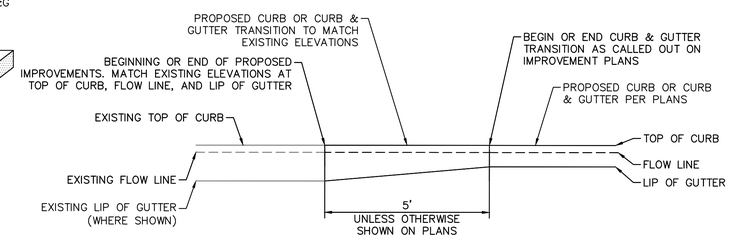


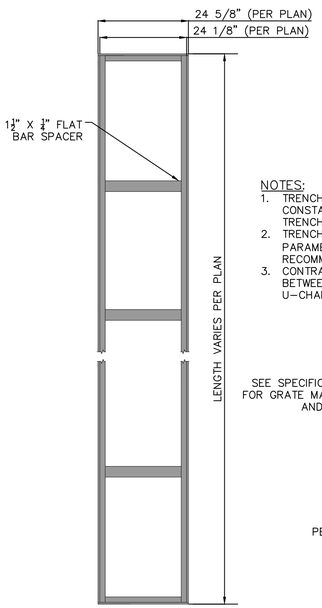
ISOMETRIC VIEW



PLAN VIEW

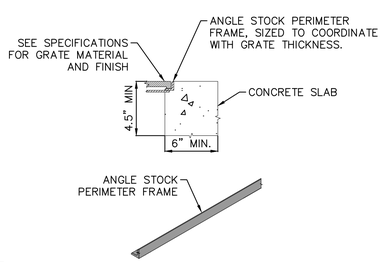
CURB OR CURB AND GUTTER TRANSITION DETAIL

NOT TO SCALE

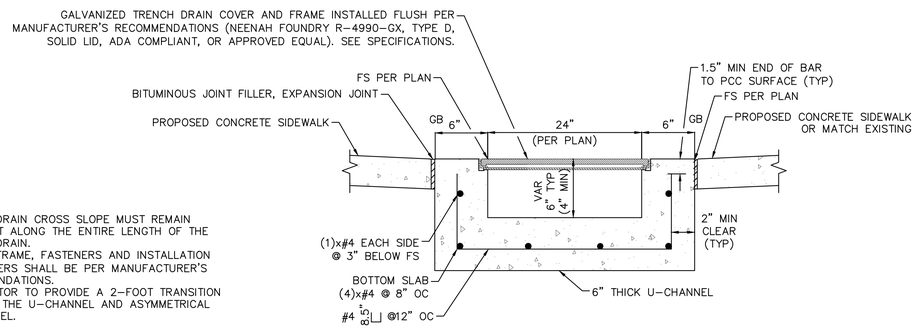


NOTES:

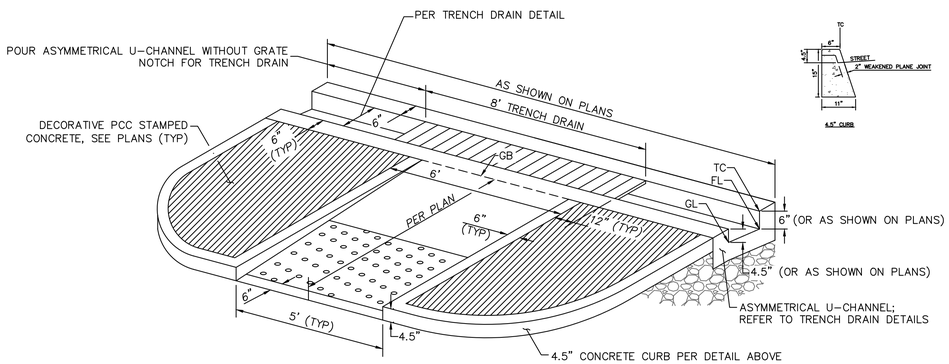
1. TRENCH DRAIN CROSS SLOPE MUST REMAIN CONSTANT ALONG THE ENTIRE LENGTH OF THE TRENCH DRAIN.
2. TRENCH FRAME, FASTENERS AND INSTALLATION PARAMETERS SHALL BE PER MANUFACTURER'S RECOMMENDATIONS.
3. CONTRACTOR TO PROVIDE A 2-FOOT TRANSITION BETWEEN THE U-CHANNEL AND ASYMMETRICAL U-CHANNEL.



PLAN: TRENCH GRATE FRAME



SECTION: SINGLE TRENCH DRAIN WITHIN GUTTER



PEDESTRIAN ACCESS ON CURB EXTENSION

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TRENCH DRAIN DETAILS

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SHT. 07 OF 11

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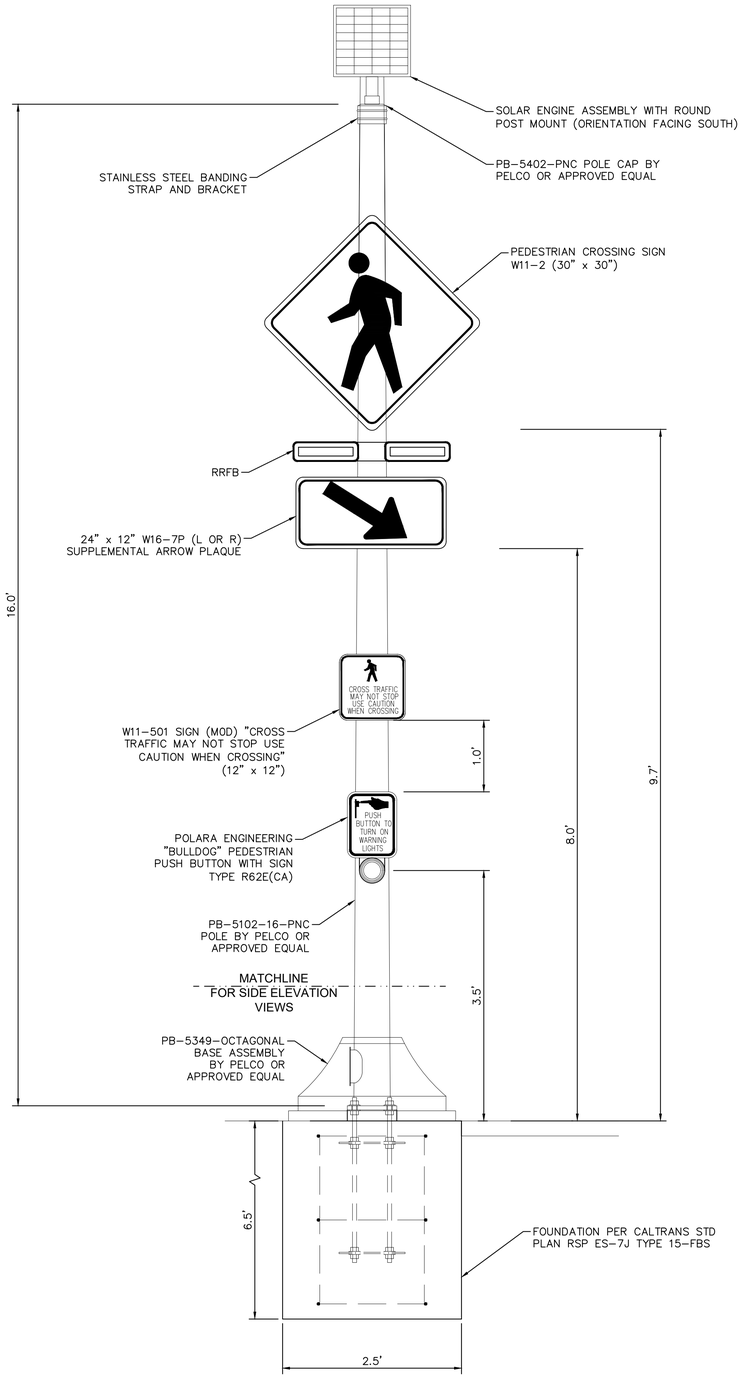
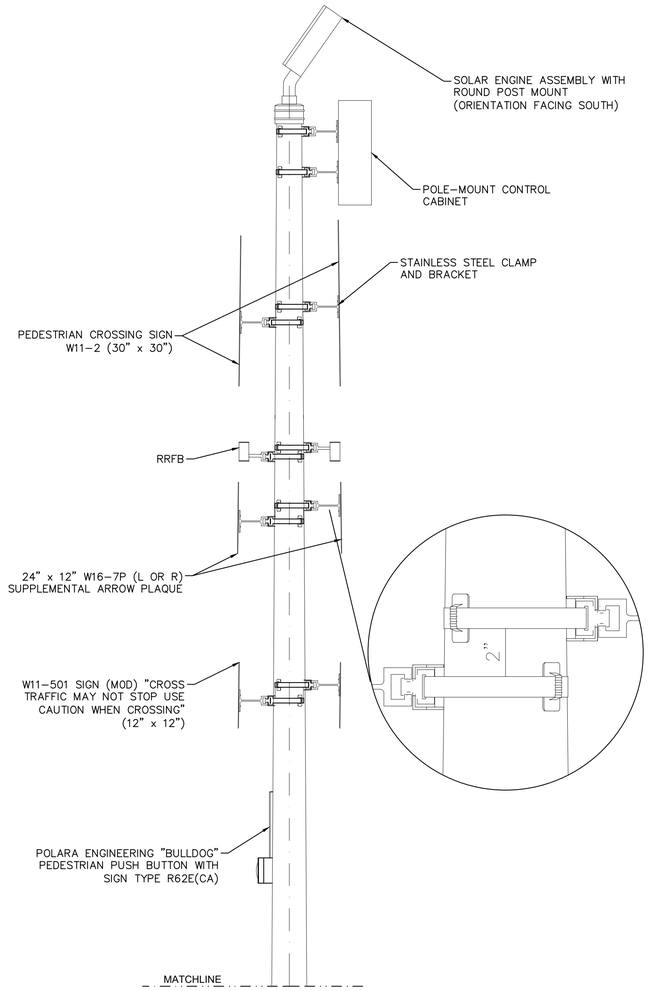


FRONT ELEVATION VIEW



SIDE ELEVATION VIEW

BI-DIRECTIONAL CONFIGURATION DETAILS

PROPERTY LINES ARE APPROXIMATE

Know what's **below.** **Call** before you dig.

RECTANGULAR RAPID FLASHING BEACON (RRFB) (WITH SOLAR ENGINE)

NOT TO SCALE

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CITY OF GOLETA

Kimley»Horn

660 S. FIGUEROA ST, SUITE 2050
LOS ANGELES, CA 90017
PHONE: 213-261-4040
WWW.KIMLEY-HORN.COM

PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION

APPROVED: CITY ENGINEER DATE

ORIGINAL SIGNED DATE

DESIGN ASC
DRAWN PC
CHECKED CC

100 % SUBMITTAL

NO.	DATE	APPROVED	REVISIONS

CITY OF GOLETA

CROSSWALK AT S. KELLOGG AVENUE

CONSTRUCTION DETAILS

CIP 9098
PBW. NO.

BID NO. SHT. DES.

CD-3
DWG. NO.

SHT. 08 OF 11

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GENERAL NOTES

1. ALL ROADSIDE SIGNING, PAVEMENT DELINEATION, MARKINGS, AND MARKERS SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF CALTRANS STANDARDS PLANS (CSP) AND SPECIFICATIONS, AND THE LATEST EDITION OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CA–MUTCD).
2. FOR ACCURATE RIGHT–OF–WAY DATA, CONTACT THE RIGHT–OF–WAY ENGINEERING AT THE CITY.
3. ALL EXISTING SIGNS NOT SHOWN FOR REMOVAL, RELOCATION, OR RESETTING SHALL BE PROTECTED IN PLACE.
4. FEDERAL SIGN CODES ARE SHOWN UNLESS DESIGNATED BY (CA), WHICH INDICATES A CALIFORNIA SIGN CODE. ADD TWO (2) RED OR ORANGE FLAGS (CLOTH OR RETROREFLECTIVE SHEETING) 16 INCH SQUARE OR LARGER ABOVE NEW W23–2 SIGNS, WITH THE FLAGS ORIENTED AT 45 DEGREES TO THE VERTICAL. FLAGS SHALL BE MAINTAINED FOR 90 DAYS AFTER WHICH THEY CAN BE REMOVED.
5. EXACT LOCATIONS OF ALL NEW SIGNS SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
6. SIGNS INSTALLED ON ELECTROLIER SHALL BE PER CALTRANS STD PLAN RS4.
7. CONTRACTOR SHALL REMOVE ALL CONFLICTING TRAFFIC STRIPES AND PAVEMENT MARKINGS.
8. PAVEMENT MARKINGS AND CROSSWALK SHALL BE THERMOPLASTIC, UNLESS OTHERWISE NOTED. TRAFFIC STRIPES SHALL BE 2–COAT PAINT.
9. TRAFFIC STRIPES AND MARKINGS SHALL BE MARKED/CAT–TRACKED IN THE FIELD BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.

PAVEMENT DELINEATION NOTES

(1) FURNISH AND INSTALL THERMOPLASTIC WHITE "CONTINENTAL" CROSSWALK MARKINGS PER CALTRANS STD PLANS A24F (12'L x 2'W x 3' SPACING).

(2) FURNISH AND INSTALL 2–COAT PAINT YELLOW LINE, DETAIL 21 PER CALTRANS STD. PLAN RSP A20A.

(3) FURNISH AND INSTALL 2–COAT PAINT WHITE RIGHT EDGE LINE, DETAIL 27B PER CALTRANS STD. PLAN RSP A20B.

(4) FURNISH AND INSTALL THERMOPLASTIC YIELD LINE MARKINGS PER CALTRANS STD. PLAN A24E.

(5) FURNISH AND INSTALL RED CURB (LENGTH 29' PER LOCATION) (2–COAT PAINT).

(6) FURNISH AND INSTALL WHITE REFLECTIVE PAVEMENT MARKERS ON TOP OF CURB ON ALL CURB EXTENSIONS APPROACHES (EIGHT SPACED ON EACH CURB EXTENSION).

(7) FURNISH AND INSTALL WHITE THERMOPLASTIC SHARED ROADWAY BICYCLE PAVEMENT MARKING PER CALTRANS STD. PLAN A24C.

(RM) REMOVE PAVEMENT MARKINGS, PAVEMENT STRIPES, AND MARKERS.

SIGNAGE CONSTRUCTION NOTES

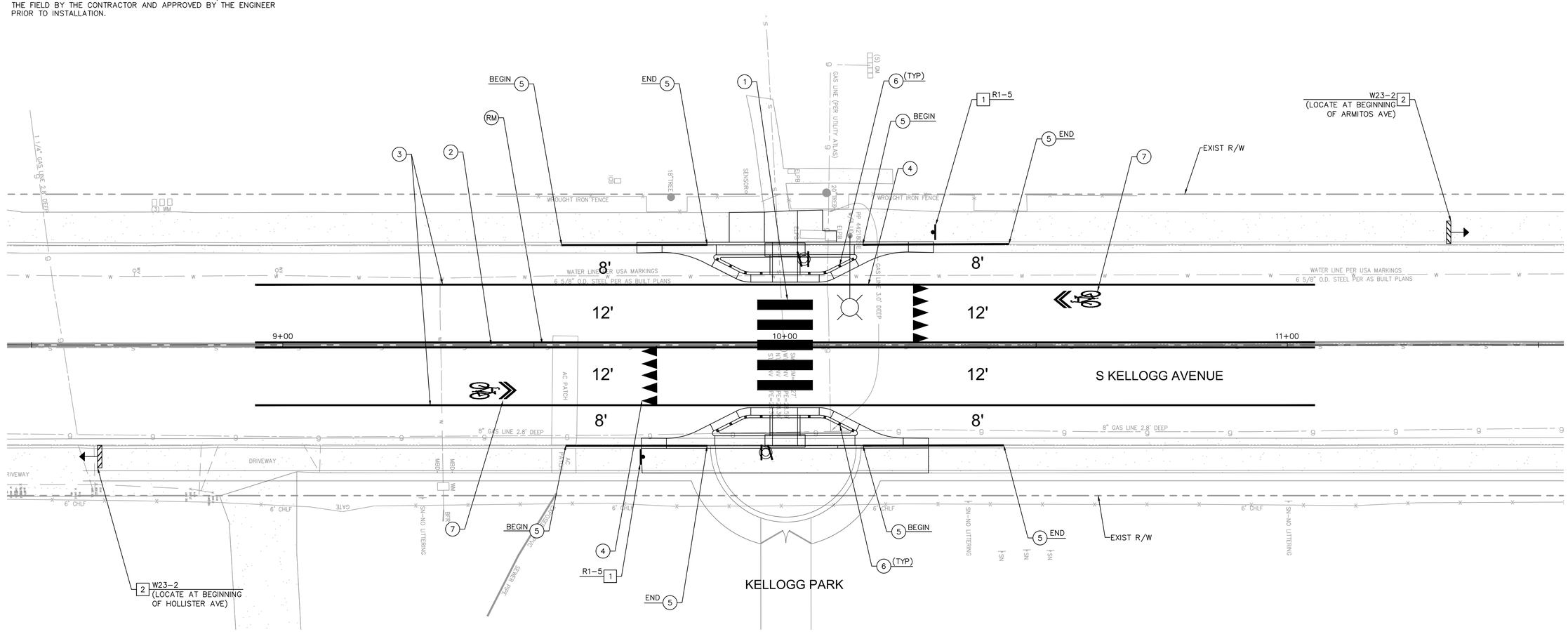
[1] FURNISH AND INSTALL SIGN AND POST.

[2] FURNISH AND INSTALL SIGN ON BARRICADE FOR 90 DAYS DURATION.

SIGN LEGEND

R1–5

W23–2
(WITH FLAGS FOR A 90 DAYS DURATION)



PROPERTY LINES ARE APPROXIMATE

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CITY OF GOLETA

Kimley»Horn
660 S. FIGUEROA ST, SUITE 2050
LOS ANGELES, CA 90017
PHONE: 213–261–4040
WWW.KIMLEY–HORN.COM

PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION

APPROVED: CITY ENGINEER DATE
ORIGINAL SIGNED DATE

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NO.	DATE	APPROVED	REVISIONS

CITY OF GOLETA
CROSSWALK AT S. KELLOGG AVENUE
SIGNING AND STRIPING PLAN

CIP 9098
PBW. NO.

BID NO. SHT. DES.

SS-1
DWG. NO.

SHT. 09 OF 11

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GENERAL NOTES

1. ALL WORK SHOWN HEREON SHALL BE CONSTRUCTED IN ACCORDANCE WITH 2015 CALTRANS STANDARD PLANS AND SPECIFICATIONS, LATEST EDITION OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL SERVICES (CA-MUTCD), AND THE MODIFICATIONS TO THOSE STANDARD SPECIFICATIONS CONTAINED IN THE CONTRACT SPECIFICATIONS.
2. UTILITIES AND FACILITIES ABOVE AND BELOW THE GROUND ARE LOCATED FROM THE BEST AVAILABLE RECORD INFORMATION. THE CONTRACTOR SHALL TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITIES OR FACILITES SHOWN ON THE PLANS FROM HARM. ATTENTION IS DIRECTED TO POSSIBLE EXISTENCE OF UNDERGROUND UTILITIES OR FACILITIES NOT KNOWN TO THE CITY OR IN A LOCATION DIFFERENT FROM THAT WHICH IS SHOWN ON UNDERGROUND UTILITIES ND FACILITIES PRIOR TO DOING WORK IN ORDER TO AVOID DAMAGE OF SUCH UTILITY OR FACILITY OR INTERFERE WITH THEIR SERVICE.
3. EQUIPMENT IN THE FIELD SHALL OPERATE ACCORDING TO THE DESIGN SPECIFIED IN THE PLAN AND SPECIFICATIONS AND TO THE SATISFACTION OF THE ENGINEER-IN-THE-FIELD.
4. OBTAIN APPROVAL FOR EXACT EQUIPMENT LOCATION PRIOR TO FINAL PLACEMENT.
5. ALL MATERIALS AND EQUIPMENT SHALL BE NEW, FURNISHED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE NOTED ON THE PLANS.
6. ALL TRAFFIC SIGNAL POLES AND FERROUS MATERIALS SHALL BE GALVANIZED.
7. SEE SIGNING AND STRIPING PLANS FOR FOR TRAFFIC STRIPING, PAVEMENT MARKERS, PAVEMENT MARKINGS, AND TRAFFIC SIGNS.
8. SEE DEMOLITION AND UTILITY PLANS FOR REMOVALS AND UTILITY IMPROVEMENTS.

CONSTRUCTION NOTES

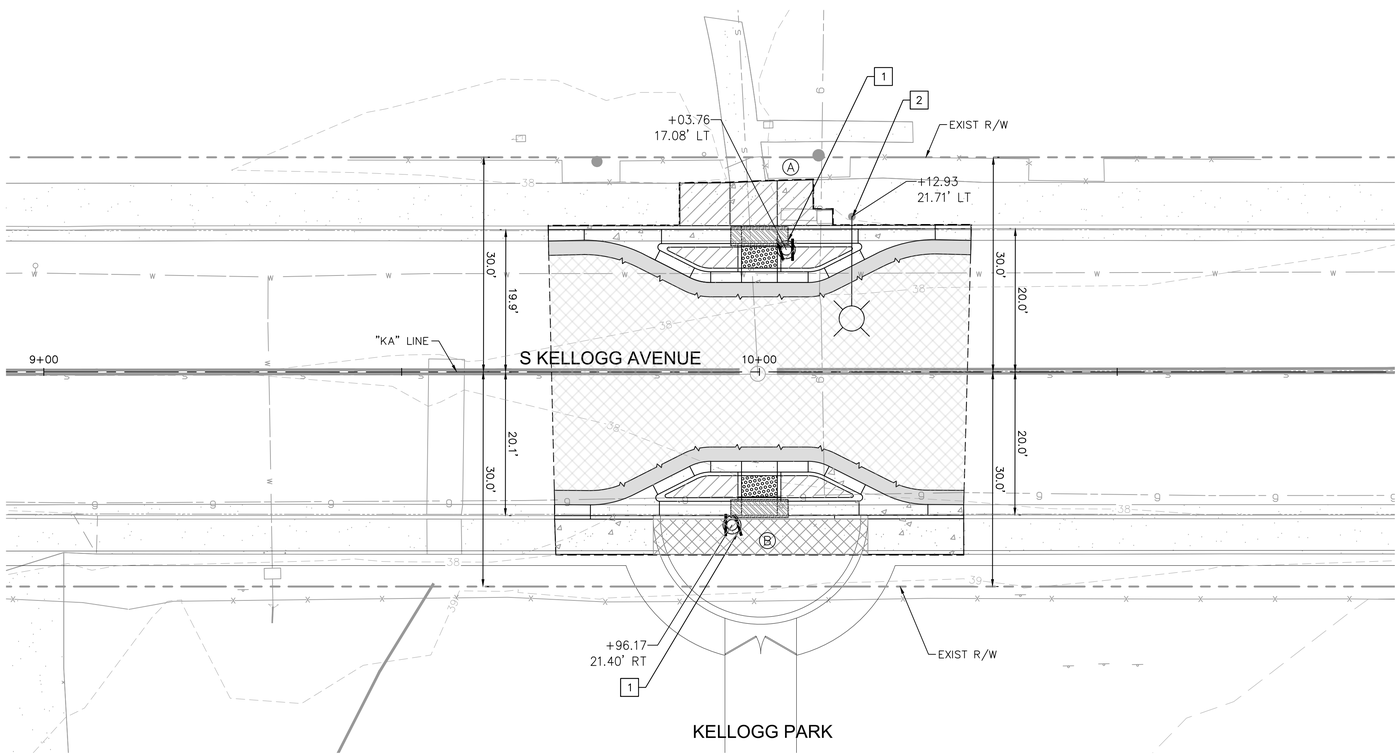
1. FURNISH AND INSTALL TYPE PB-5102-16-PNC POLE BY PELCO OR APPROVED EQUAL ON NEW FOUNDATION WITH ALL APPURTENANCES PER CONSTRUCTION DETAILS. INSTALL NEW POLE AND EQUIPMENT PER POLE SCHEDULE. FURNISH AND INSTALL RRFB ASSEMBLY (WITH SOLAR ENGINE) BACK-TO-BACK LED BLINKER SIGNS W11-2 AND W16-7P PER TAPCO OR APPROVED EQUAL. SEE CONSTRUCTION DETAILS FOR SIGN PLACEMENT. SIGN ORIENTATION PER PLAN.
2. REMOVE EXISTING HPS SAFETY LIGHTING FIXTURE ON EXISTING POWER POLE AND REPLACE WITH 165W LED FIXTURE WITH HOUSE SHIELD AND 12' LMA. EXISTING ELECTRICAL SERVICE TO REMAIN. CONTRACTOR SHALL COORDINATE WITH SCE PRIOR TO CONSTRUCTION. SCE TO PERFORM WORK.

POLE SCHEDULE ■●

No.	STANDARD				LUMINAIRE	VEH. SIG. MTG.		PED. SIGNAL	PPB	REMARKS
	TYPE	HEIGHT	SMA	LMA	LED	MAST ARM	POLE	MTG.	Ø	
Ⓐ	PB-5102-16-PNC	16'	–	–	–	–	*	–	–	1
Ⓑ	PB-5102-16-PNC	16'	–	–	–	–	*	–	–	1

■ ALL EQUIPMENT IS NEW.
* MOUNT LED BLINKER ASSEMBLY PER MANUFACTURER'S SPECIFICATIONS.
● EXACT POLE LOCATION SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

WESTERN CURB EXTENSION



EASTERN CURB EXTENSION

PROPERTY LINES ARE APPROXIMATE

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CITY OF GOLETA

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PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION

APPROVED: CITY ENGINEER ORIGINAL SIGNED DATE DATE

DESIGN ASC
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NO.	DATE	APPROVED	REVISIONS

CITY OF GOLETA
CROSSWALK AT S. KELLOGG AVENUE
ELECTRICAL PLAN

CIP 9098
PBW. NO.

BID NO. SHT. DES.

E-1
DWG. NO.

SHT. 10 OF 11

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GENERAL EROSION CONTROL NOTES

1. ALL CONSTRUCTION ACTIVITY SHALL BE PERFORMED IN COMPLIANCE WITH REQUIREMENTS OF CITY OF GOLETA AND SANTA BARBARA COUNTY STORMWATER QUALITY MANAGEMENT PROGRAM, NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES), AND PROJECT SPECIAL PROVISIONS.
2. ERODED SEDIMENTS AND OTHER POLLUTANTS MUST BE RETAINED ON SITE AND MAY NOT BE TRANSPORTED FROM THE SITE VIA SHEET FLOW, SWALES, AREA DRAINS, NATURAL DRAINAGE COURSES, OR WIND. FIELD CONDITIONS MAY NECESSITATE MODIFICATIONS TO THESE DRAWINGS.
3. NON-STORMWATER DISCHARGES ARE PROHIBITED FROM ENTERING ANY STORM DRAIN SYSTEM AND/OR STREET.
4. POLLUTANTS SHALL BE REMOVED FROM STORMWATER DISCHARGES TO THE MAXIMUM EXTENT PRACTICABLE (MEP) THROUGH DESIGN & IMPLEMENTATION.
5. PORTABLE SANITARY FACILITIES SHALL BE LOCATED ON RELATIVELY LEVEL GROUND AWAY FROM TRAFFIC AREAS, DRAINAGE COURSES, AND STORM DRAIN INLETS.
6. EMPLOYEES, SUBCONTRACTORS AND SUPPLIERS SHALL BE EDUCATED ON ALL BMP'S INCLUDING CONCRETE WASTE STORAGE AND DISPOSAL PROCEDURES.
7. SEDIMENT CONTROL PRACTICES SHALL EFFECTIVELY PREVENT A NET INCREASE OF SEDIMENT LOAD IN STORMWATER DISCHARGE.
8. NO AREAS, WHERE NATIVE SOIL IS DISTURBED BY CONSTRUCTION ACTIVITY, SHALL HAVE A SURFACE WHICH REMAINS BARE. EITHER DURING OR AT COMPLETION OF CONSTRUCTION, SUCH AREAS SHALL BE COVERED WITH APPROVED SOIL STABILIZATION TECHNIQUES.
9. PROTECTION OF STORM DRAIN INLETS WITHIN THE AREA OF CONSTRUCTION SHALL BE REQUIRED USING BMP'S.
10. ALL BARE SLOPES AND DISTURBED AREAS SHALL BE PROTECTED AS EACH STAGE OF GRADING IS COMPLETED. SUITABLE MEASURES TO PREVENT SLOPE EROSION, INCLUDING, BUT NOT LIMITED TO MULCH COVER, SHOULD BE INSTALLED ON ALL AREAS WHILE THE PERMANENT VEGETATION COVER MATURES ENOUGH TO PROVIDE STABILITY.
11. ANY EXPOSED SOIL, INCLUDING SOIL STOCKPILES, THAT WILL NOT BE DISTURBED FOR TEN (10) DAYS OR MORE SHALL BE FULLY PROTECTED FROM EROSION UNTIL ADEQUATE LONG-TERM PROTECTIONS ARE INSTALLED. ALL EROSION CONTROL MEASURES SHALL REMAIN INSTALLED AND MAINTAINED DURING ANY INACTIVE PERIOD.
12. SLOPES WITH DISTURBED SOILS OR DENUDED VEGETATION MUST BE STABILIZED SO AS TO INHIBIT EROSION BY WIND AND WATER.
13. PROVIDE GRAVEL BAG, SILT BASIN, OR TRAP OR OTHER APPROVED DEVICE BY EVERY STORM DRAIN INLET TO PREVENT SEDIMENT FROM ENTERING THE STORM DRAIN SYSTEM. CHECK DAMS SHALL BE INSTALLED IN THE STREET GUTTER UPSTREAM OF CURB INLETS IN ACCORDANCE TO THE SPACING PROVIDED IN NOTE 14.
14. IN ADDITION TO THOSE AREAS SHOWN ON THIS PLAN, FIBER ROLLS OR OTHER SEDIMENT CONTAINMENT BARRIERS SHALL BE PLACED BY THE CONTRACTOR IN AREAS WHERE THE CITY INSPECTOR OR THE ENGINEER DEEM NECESSARY UPON OBSERVATION OF CONSTRUCTION.
15. SHOULD EROSION OCCUR IN THE PROJECT AREA EITHER DURING OR WITHIN 30 DAYS UPON THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RETURN SUCH AREAS TO ORIGINAL OR FINISHED CONDITION AND PROVIDE FIBER ROLLS AND SOIL STABILIZATION MEASURES TO ELIMINATE THE RECURRENCE OF SUCH EROSION.
16. EXCESS OR WASTE CONCRETE MAY NOT BE WASHED INTO THE PUBLIC RIGHT-OF-WAY OR ANY OTHER DRAINAGE SYSTEM. PROVISIONS SHALL BE MADE TO RETAIN CONCRETE WASTES ON SITE UNTIL THEY CAN BE DISPOSED OF AS SOLID WASTE.
17. FUELS, OILS, SOLVENTS, AND OTHER TOXIC MATERIALS MUST BE STORED IN ACCORDANCE WITH THEIR LISTING AND ARE NOT TO CONTAMINATE THE SOIL AND SURFACE WATERS. ALL APPROVED STORAGE CONTAINERS ARE TO BE PROTECTED FROM THE WEATHER. SPILLS MUST BE CLEANED UP IMMEDIATELY AND DISPOSED OF IN A PROPER MANNER. SPILLS MAY NOT BE WASHED INTO THE DRAINAGE SYSTEM.
18. TRASH AND CONSTRUCTION-RELATED SOLID WASTES MUST BE DEPOSITED INTO A COVERED RECEPTACLE TO PREVENT CONTAMINATION OF RAINWATER AND DISPERSAL BY WIND.
19. SEDIMENTS AND OTHER MATERIALS MAY NOT BE TRACKED FROM THE SITE BY VEHICLE TRAFFIC. THE CONSTRUCTION ENTRANCE ROADWAYS MUST BE STABILIZED SO AS TO INHIBIT SEDIMENTS FROM BEING DEPOSITED INTO THE PUBLIC RIGHT-OF-WAY. ACCIDENTAL DEPOSITIONS MUST BE SWEPT UP IMMEDIATELY AND MAY NOT BE WASHED DOWN BY RAIN OR OTHER MEANS.
20. THE FOLLOWING BMP'S AS OUTLINED IN, BUT NOT LIMITED TO, THE "BEST MANAGEMENT PRACTICE HANDBOOK, CALIFORNIA STORMWATER QUALITY TASK FORCE, SACRAMENTO, CALIFORNIA, 2009," OR THE LATEST REVISED EDITION, MAY APPLY DURING THE CONSTRUCTION OF THIS PROJECT (ADDITIONAL MEASURES MAY BE REQUIRED IF DEEMED APPROPRIATE BY CITY INSPECTORS):

EROSION AND SEDIMENT CONTROL NOTES

1. AREA OF DISTURBANCE = 0.06 ACRES

PROJECT PRIORITIZATION

1. GRAVEL BAGS AS NEEDED DURING RAIN EVENTS (50' O.C.).
2. CONTRACTOR SHALL PERFORM CONTINUOUS AND PREVENTATIVE SEDIMENT TRACKING SITE CLEANUP OF INGRESS AND EGRESS CONSTRUCTION ENTRANCE FROM TRUCK TRACKING WITH EACH TRIP. IF CONTRACTOR'S ELECTED METHODS OF CLEANING UP OF SEDIMENT TRACKING IS NOT MAINTAINED, INSTALLATION OF STABILIZED CONSTRUCTION ENTRANCE SHALL BE UTILIZED.

TEMPORARY BEST MANAGEMENT PRACTICES

TEMPORARY SOIL STABILIZATION
- EC-1 SCHEDULING
- EC-2 PRESERVATION OF EXISTING VEGETATION
- EC-4 HYDRAULIC MULCH

TEMPORARY SEDIMENT CONTROL
- SE-6 GRAVEL BAG BERM
- SE-7 STREET SWEEPING AND VACUUMING
- SE-10 STORM DRAIN INLET PROTECTION

WIND EROSION CONTROL
- WE-1 WIND EROSION CONTROL

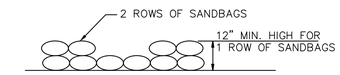
EQUIPMENT TRACKING CONTROL
- NON-STORMWATER MANAGEMENT
- NS-1 WATER CONSERVATION PRACTICES
- NS-2 DEWATERING OPERATIONS
- NS-6 ILLICIT CONNECTION / DISCHARGE
- NS-7 POTABLE WATER / IRRIGATION
- NS-9 VEHICLE AND EQUIPMENT FUELING
- NS-10 VEHICLE AND EQUIPMENT MAINTENANCE
- NS-12 CONCRETE CURING
- NS-13 CONCRETE FINISHING

WASTE MANAGEMENT & MATERIAL POLLUTION CONTROL
- WM-1 MATERIAL DELIVERY AND STORAGE
- WM-2 MATERIAL USE
- WM-3 STOCKPILE MANAGEMENT
- WM-4 SPILL PREVENTION AND CONTROL
- WM-5 SOLID WASTE MANAGEMENT
- WM-6 HAZARDOUS WASTE MANAGEMENT
- WM-8 CONCRETE WASTE MANAGEMENT
- WM-9 SANITARY / SEPTIC WASTE MANAGEMENT
- WM-10 LIQUID WASTE MANAGEMENT

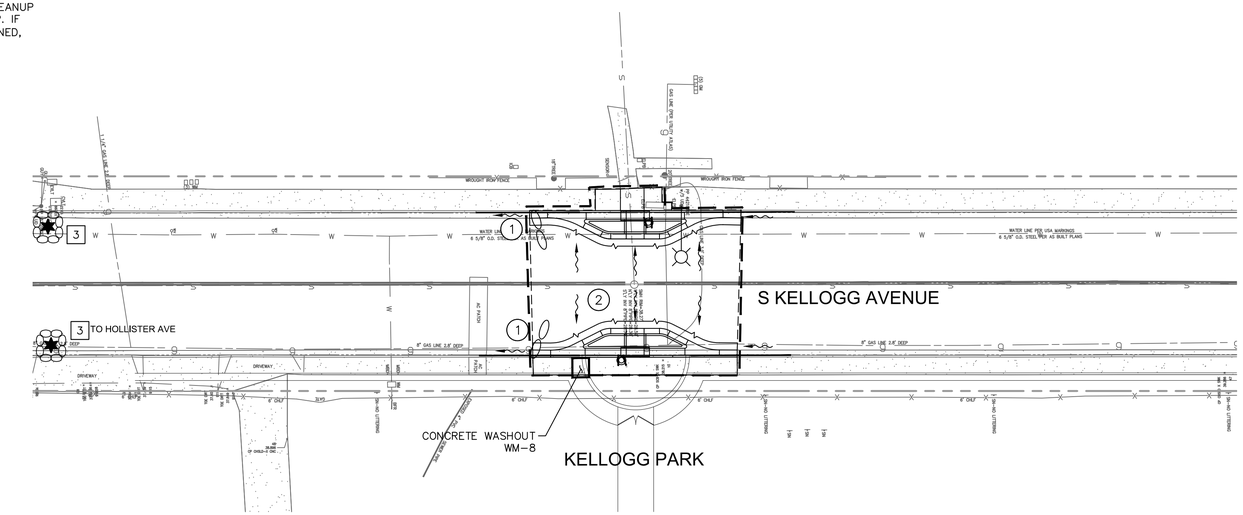
TEMPORARY TRACKING CONTROL BMPs
- TC-1 STABILIZED CONSTRUCTION ENTRANCE/EXIT OR ADDITIONAL EFFORTS PER SE-7 AFTER EACH DELIVERY

LEGEND

- LIMIT OF DISTURBANCE
- SANDBAGS/GRAVEL BAGS
- DIRECTION OF FLOW
- STORMWATER DISCHARGE LOCATION
- 1 SE-10 STORM DRAIN INLET PROTECTION/ GRAVEL BAG PER (TYPE 3A) PER CALTRANS STD PLAN T62 OR
- 2 SE-10 STORM DRAIN INLET PROTECTION/ GRAVEL BAG PER (TYPE 3B) PER CALTRANS STD PLAN T62 OR
- 3 SE-10 STORM DRAIN INLET PROTECTION/ GRAVEL BAG PER (TYPE 6B) PER CALTRANS STD PLAN T64



GRAVEL BAG / SANDBAG DETAIL



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PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION
APPROVED: CITY ENGINEER DATE
ORIGINAL SIGNED DATE

DESIGN ASC
DRAWN PG
CHECKED CC

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NO.	DATE	APPROVED	REVISIONS

CITY OF GOLETA
CROSSWALK AT S. KELLOGG AVENUE
TEMPORARY EROSION CONTROL

CIP 9098
PBW. NO.

BID NO. SHT. DES.

EC-1
DWG. NO.

SHT. 11 OF 11

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ATTACHMENT 2

Amendment No. 1 to Professional Design Services Agreement No. 2018-007 with Kimley-Horn and Associates, Inc.

AMENDMENT NO. 1
TO A PROFESSIONAL SERVICES AGREEMENT
BETWEEN THE CITY OF GOLETA
AND
KIMLEY-HORN AND ASSOCIATES, INC.

This **Amendment No. 1** to a PROFESSIONAL DESIGN SERVICES AGREEMENT by and between the **City of Goleta**, a municipal corporation ("City") and **KIMLEY-HORN AND ASSOCIATES, INC.**, a California Corporation, ("Agreement," Agreement No. 2018-007) is made this 21st day August 2018.

RECITALS

WHEREAS, the CITY has a need for professional engineering design services specifically traffic and transportation for the Crosswalk at South Kellogg Avenue Project; and

WHEREAS, the Agreement between City and Consultant currently provides in Section 3 Subsection (a) for the total compensation amount not to exceed twenty-one thousand dollars ($21,000); and

WHEREAS, the parties desire to amend the Agreement so as to provide for additional compensation in the amount of twenty thousand dollars ($20,000) for additional tasks in conjunction with the Hollister Kellogg Park Street Improvements Project; and

WHEREAS, the Agreement currently designates Vyto Adomaitis, Director of Neighborhood Services and Public Safety as Project Manager,

WHEREAS, the City desires to change the Project Manager to James Winslow, Senior Project Engineer, and

WHEREAS, the Agreement between City and Consultant currently provides in Exhibit A entitled "Scope of Work" the complete and particular description of services as set forth for Consultant; and

WHEREAS, the parties desire to amend Exhibit A of the Agreement by adding additional engineering services to finalize the Engineering Documents for construction at the midblock location along Kellogg Avenue north of Hollister Avenue, as more completely and particularly set forth in the Scope of Work, attached as Exhibit "A-1"; and

WHEREAS, the City Council approved this Amendment No. 1, on this 21st day of August, 2018.

AMENDED TERMS

Now therefore City and Consultant agree as follows that the Agreement be, and hereby is, amended as follows:

1. **Section 3 Subsection (a).** **COMPENSATION AND PAYMENT** of the Agreement is amended to add an additional authorized amount of $20,000 and to read in its entirety:

   (a) Maximum and Rate. The total compensation payable to CONSULTANT by CITY for the services under this AGREEMENT **SHALL NOT EXCEED** the sum of $41,000 (herein "not to exceed amount"), and shall be earned as the work progresses.

2. **Section 5.** **CITY PROJECT MANAGER AND SERVICES BY CITY** is amended to designate James Winslow, Senior Project Engineer, as Project Manager and to read in its entirety:

   CITY PROJECT MANAGER AND SERVICES BY CITY

   The services to be performed by CONSULTANT shall be accomplished under the general direction of, and coordinate with, CITY's "Project Manager", as that staff person is designated by CITY from time to time, and who presently is James Winslow, Senior Project Engineer. Project Manager shall have the authority to act on behalf of the CITY in administering this AGREEMENT but shall not be authorized to extend the term of the AGREEMENT or increase the not to exceed amount.

3. This Agreement is amended to deleted and replace in its entirety:

Exhibit A "Scope of Work" with **Exhibit A-1 "Scope of Work"** attached hereto and incorporated herein.

4. Except as otherwise specifically provided herein, all other provisions of the Agreement shall remain in full force and effect.

In concurrence and witness whereof, this Amendment No. 1 has been executed by the parties effective on the date and year first above written.

CITY OF GOLETA

Michelle Greene, City Manager

CONSULTANT

Jean Fares, Vice President

ATTEST:

Deborah Lopez, City Clerk

Adam Chase, Secretary

APPROVED AS TO FORM

Winnie Cai, Assistant City Attorney

Exhibit A-1
Scope of Work

CONSULTANT shall provide professional traffic/transportation design services for mid-block crosswalk improvements to Kellogg Avenue adjacent to the Hollister/Kellogg Park project as further detailed below:

Task 1: Traffic Engineering Study
CONSULTANT shall prepare a draft and final traffic engineering study to the City for review and consideration for final traffic recommendations at a midblock crosswalk location on Kellogg Avenue between Hollister Avenue and Armitos Avenue, as more particularly shown on Fuscoe's 65% Plan Submittal for the Hollister/Kellogg Park. CONSULTANT shall gather existing traffic data for 85th percentile speeds and ADT information (and collision data) along Kellogg Avenue during a 2-day weekday period. CONSULTANT shall perform a field visit, make observations, provide pedestrian warrants, and finalize the study to incorporate into the final design.

Deliverables:

- Draft and Final Traffic Engineering Study

Task 1.1 Additional Design Services
CONSULTANT shall provide additional design services associated with modifications due to coordination efforts with the adjacent Kellogg Park (wood mulch, temporary ac patches, etc), such as revisions to the final plan sheets, estimate, and special provisions, and addressing comments. In addition, CONSULTANT shall obtain and provide manufacturer detectable warning surface samples for the City to select the appropriate colors. We will provide lead times for the recommended Pelco poles and RRFB assemblies.

- CONSULTANT shall prepare the City upfront specifications in a word document. CONSULTANT shall prepare potholing exhibits, order the digalert ticket, and include a field meeting to support the City maintenance department's effort to perform potholing work within the month of July.

Deliverables:

- Revisions to the Plans, Specifications, OPCC, and City Specifications
- Potholing exhibits, digalert ticket, samples, and lead times.

Task 2: 95%/100% Final Design
Having received the final selection from the Traffic Engineering Study recommendations, CONSULTANT shall work together with City Staff to refine and accomplish the following:

- Provide Final Construction Documents (Per City of Goleta Standards) – both sides of the street for midblock crosswalk, including:
    - Civil Plans - Kellogg Avenue Street Improvements (1 sheet at 1"=20') to Existing
        - Right of Way to Match Kellogg Park Plan Set
        - Sidewalk, Curb & Gutter, AC Conforms
        - Trench Drains (for positive surface flow)
        - ADA Ramp Construction Details
    - Signing and Striping Plans (1 sheet at 1"=40')
        - Includes signing, striping, and additional traffic safety enhancements for midblock crosswalks
    - Electrical Plans (1 sheet at 1"=20')
        - Includes street lighting and traffic safety devices that require power, pole schedule, foundations, assemblies, and equipment
        - Utility coordination will be provided for point of connection

CONSULTANT shall meet with City staff to review the 95% design and discuss additional critical areas of concern. CONSULTANT shall incorporate the City requests and recommendations as applicable to the project. CONSULTANT shall prepare the signing and striping plans per the latest edition of the California Manual of Traffic Control Devices (CA-MUTCD). These plans shall implement various traffic control devices e.g. pavement stripes, markings, retroreflective markers, object markers, traffic signs, etc. The plans shall incorporate features to support all users, such as bike and pedestrian accommodations.

CONSULTANT shall compile, and prepare the Opinion of Probable Construction Costs (OPCC). Contingencies shall be included at the appropriate percentage.

CONSULTANT shall prepare the project special provisions based on the latest APWA Greenbook Specifications and 2015 Caltrans Standard Specifications. Final plans shall be submitted on Mylar sheets 24"x 36" in size.

Deliverables:

- 95%/100% Plans, Specifications, OPCC

CONSULTANT shall begin the collection of traffic data and field work within 1 week from the notice to proceed.

TASK 3 Construction Design Support
CONSULTANT shall perform the following during construction:

- Attend a preconstruction meeting with the City.
- Prepare and issue revisions and/or new design plans and/or specifications and maintain a log of changes to the "Issue for Construction" plan set. For this project,

CONSULTANT shall anticipate up to one (1) change order requiring design plan and specification changes, (submittal in PDF/CAD and MS Word).

- Review and respond to Request for Information (RFI) (up to 4 requests).
- Review and respond to Change Order Request (CR) from the City (up to 1 request).
- Review submittals and shop drawings for conformity with the plans and specifications as requested.
- Attend project site observations at the City's request (up to 1). Conduct construction observations after the construction progress meeting.
- Final as-built drawings from City/Contractor provided master redlined set, product to be developed in AutoCAD with PDF Files.

Assumptions / Exclusions
Any other services, not including but not limited to the following, are not included in this Agreement:

- Contractor Final Punch List

Site visits and observations by the CONSULTANT are not intended to be exhaustive or to extend to every aspect of Contractor's work in progress. Observations are limited to spot checking, selective measurement, and similar methods of general observation of the Work based on the CONSULTANT's exercise of professional judgment. Based on information obtained during such visits and such observations, the CONSULTANT will evaluate whether Contractor's work is generally proceeding in accordance with the Contract Documents, and the CONSULTANT shall keep the City informed of the general progress of the work.