



**TO:** Planning Commission Chair and Members

**FROM:** Peter Imhof, Planning and Environmental Review Director

**CONTACT:** Lisa Prasse, Current Planning Manager  
Mary Chang, Supervising Senior Planner  
Chris Noddings, Assistant Planner

**SUBJECT:** Request for Development Plan and Condition Use Permit  
Revision to allow a Synagogue Building, Storage Building,  
and associated site improvements with a parking modification  
at 6045 Stow Canyon Road; Case No. 18-031-DP-CUPRV-  
DRB; APN 077-170-044

## RECOMMENDATION

It is recommended that the Planning Commission:

1. Open a public hearing to take verbal and written testimony; and
2. After considering the evidence presented during the public hearing, adopt Planning Commission Resolution No. 19- \_\_ entitled "A Resolution of the Planning Commission of the City of Goleta, California, Adopting the Final Mitigated Negative Declaration, Adopting the Mitigation Monitoring And Reporting Program, and Approving the Development Plan and Conditional Use Permit Revision with a parking modification to allow residential parking within a carport for Chabad of Santa Barbara located At 6045 Stow Canyon Road, Goleta, CA; Case No.18-032-DP-CUPRV-DRB; APN: 077-170-044."

## PROPERTY OWNER

Chabad of Santa Barbara  
Rabbi Mendel Loschak  
6047 Stow Canyon Road  
Goleta, CA 93117

## AGENT

SEPPS  
Steve Fort  
1625 State Street, Suite 1  
Santa Barbara, CA 93101

## **APPLICANT'S REQUEST**

Chabad of Santa Barbara has requested approval of a Development Plan and a Revision to an existing CUP that would add a new synagogue and storage buildings, remove an existing barn/storage garage, and make other site improvements. The request also includes a modification related to not requiring covered parking for the existing residence. While the required number of parking spaces would be provided, the applicant requests that these spaces be uncovered. One of the comments during DRB conceptual review was that a carport for the residence with a covered walkway connecting to the house would be appropriate. Staff finds that such an alternative would be a suitable way to provide covered parking for the residence and have prepared project attachments, such as the Conditions of Approval, to include a carport. Additionally, the request is to adopt a Mitigated Negative Declaration (MND) pursuant to California Environmental Quality Act (CEQA) Guideline §15074.

## **PROJECT TIMELINE**

<i>Project Application Filed:</i>	<i>March 21, 2018</i>
<i>Design Review Board (DRB)</i>	
<i>Conceptual Review:</i>	<i>December 11, 2018</i>
<i>Project Deemed Complete:</i>	<i>December 13, 2018</i>
<i>Native American Consultation Period Commenced:</i>	<i>January 3, 2019</i>
<i>Native American Consultation Period Ended:</i>	<i>February 2, 2019</i>
<i>Mitigated Negative Declaration Release:</i>	<i>November 1, 2019</i>
<i>Mitigated Negative Declaration Comment Period Close:</i>	<i>November 20, 2019</i>

## **JURISDICTION**

The Planning Commission has jurisdiction over the requested Conditional Use Permit Revision (CUPRV) and Development Plan (DP) pursuant to Sections 35-315.3, 35-315.5.2c, 35-315.11 (3), 35-222.3, and 35-317.4, Article III (Inland Zoning Ordinance) Chapter 35, Goleta Municipal Code. Therefore, the Planning Commission has discretionary authority over the project request, subject to appeal to the City Council.

## **BACKGROUND**

### ***Prior Approvals***

Chabad of Santa Barbara has been using the project site since the existing Conditional Use Permit (CUP) (Case No. 92-CP-018) was approved in 1992. Current uses on the site include a residence (used by the Rabbi) as well as a building that is used as a synagogue, a school, and a day care. The existing CUP states the maximum number of individuals that may be in attendance at the project site as 50 individuals at the synagogue, 25 at the school, and 15 at the daycare. In addition, three full-time and three part-time staff may also be at the site. However, the Religious Land Use and Institutionalized Persons Act (RLUIPA), 42 U.S.C. §§ 2000cc et seq., signed into law on September 22, 2000, effectively made limitations, such as the number of attendees allowed, unenforceable.

The existing CUP (Case No. 92-CP-018) allowed for the construction and operation of an 873 SF synagogue, 2,235 SF office/meeting room, and a 2,235 SF daycare and school facility on the property. These improvements were in addition to the existing 2,002-SF residence (a single-family dwelling) and a 1,440-SF barn/storage garage on site. As part of the proposed project, the barn was to be removed to allow for the construction of a new driveway. Subsequently, two Substantial Conformity Determinations (SCDs) for the project were approved. The first SCD, approved on May 26, 1994, increased the size of the proposed office/meeting room building and the proposed daycare and school facility buildings by 210 SF each to a new total of 2,445 SF each. The second SCD, approved on July 1, 1999, moved the proposed driveway 18 to 20 feet to the east, and extended the parking turnaround a maximum of 26 feet to the south, to retain existing, mature fruit trees and to retain the existing barn for storage.

Ultimately, only the approved 2,445 SF daycare and school facility and revised parking lot were constructed. Since the 873 SF synagogue and the 2,445 SF office/meeting room were never constructed, the existing 2,445 SF daycare and school facility has been modified to serve all of these functions (daycare, school, office, meeting room, and synagogue worship space). Occasionally, a temporary tent is placed on the concrete slab that was constructed for the synagogue.

Concurrent with the Santa Barbara County Planning Commission's approval of the aforementioned Lot Line Adjustment and Conditional Use Permit, the Santa Barbara County Planning Department approved a permit (93-LUS-028) for a residential remodel (including the conversion of the attached garage) and addition. Specifically, the project would increase the size of the 2,002-SF residence by 2,142 SF (to 4,144 SF total) and provide replacement parking in a new, detached, 630-SF garage. As part of this project, the driveway serving the former, attached garage was proposed to be removed and a new driveway on the eastern side of the property was proposed to be constructed to serve the new, detached garage. While the former, attached garage has since been converted, the former driveway has not been removed and neither the new driveway nor detached garage have been constructed.

### ***DRB Review***

On December 11, 2018, the DRB conducted conceptual review of the project (DRB minutes are provided as Attachment 2). In general, the DRB found the design of the new buildings to be compatible with the site and well-integrated into the topography. The DRB comments focused on the design and design details, landscaping, and site plan features. Regarding the requested modification, the DRB also commented, "It would be nice and feel appropriate to have some covered parking that is associated to the residence, including a covered pathway from the parking spaces to the residence. A garage would be too much." See the DRB minutes for the full summary, which are provided as Attachment 2.

If the Project is approved by the Planning Commission, then the design of the Project will be required to return to the DRB for Design Review Approval before the City issues a Land Use Permit prior to the commencement of construction.

### ***Native American Consultation***

On January 3, 2019, the City sent letters inviting consultation to the tribal representatives identified on the list provided by the NAHC as having a traditional and cultural association with the geographic area of the proposed project pursuant to Public Resources Code Section 21080.3.1. On February 8, 2019, Santa Ynez Band of Chumash Indians indicated that it did not want to formally consult but would like the City to impose the requirement of a discovery condition. Such a condition (see Mitigation Measure CUL-3 in the Mitigation Monitoring and Report Program) requires that, in the event resources are found, construction work either halts or moves to another portion of the site until the find can be evaluated.

In addition to the Tribal groups provided to the City by the Native American Heritage Commission for formal consultation, staff has provided the Barbareño Band of Chumash Indians (BBCI) an opportunity to provide input on this project. The BBCI representative has not provided any additional comments.

### **PROJECT LOCATION**

The project site is located at 6045 Stow Canyon Road, west of North Fairview Avenue in the City of Goleta (City). The property encompasses approximately 3.3 acres (see Figure 1). The Assessor Parcel Number (APN) is 077-170-044.

**Figure 1: Project Location and Vicinity**





## PROJECT DESCRIPTION

Chabad of Santa Barbara has requested approval of a Development Plan and a Revision to an existing CUP (18-031-DP-CUPRV) that would add a new synagogue and storage building, remove an existing barn/storage garage, and make other site improvements. A request for one modification related to how parking is provided for the existing residence is also included. The project consists of the following components as shown in the project plans and detailed in the project application:

1. A Development Plan (DP) to allow for the following:
  - Construction of a new, approximately 7,293 square foot (SF) one-story synagogue with an 815 SF roof terrace (8,108 SF total). The building will include a sanctuary, event hall, kitchen, offices, conference rooms, bathrooms, storage, and mechanical and electrical space. The tallest proposed roof line is 24 feet, 6.5 inches, although the building also includes an architectural projection that would have a maximum height of 34 feet, 7.25 inches. Solar panels will also be included on the roof.
  - Construction of a new, 841 SF, one-story storage building. The maximum height would be approximately 16 feet.
  - Removal of the existing 1,440 SF, approximately 80-year-old barn/storage garage.
  - Approval of the existing, 176-SF, open-sided, lean-to storage container.
  - Removal/demolition of an existing, 120-SF trailer and a 53-SF shed.
  - Consolidation of access to the site into a single driveway. This entails the removal of the existing residential driveway and the removal and relocation of the existing synagogue driveway to approximately 35 feet west of its current location.
  - Replacement, reorientation, and expansion of the existing 24-space parking area (including 1 van-accessible space) with a 31-space parking area. The 31-space parking area would include 3 van-accessible spaces as well as 3 uncovered spaces designated for the residence.
  - Addition of landscape improvements to integrate the proposed synagogue and storage building with existing buildings and provide landscape buffers for neighboring properties and the Las Vegas Creek. Plants are intended to be generally native and drought tolerant; 24 trees to be planted include coast live oak (6), California sycamore (5), Catalina ironwood (1), and New Zealand Christmas tree (12). The removal of 8 fruit trees (various types), 2 myoporum trees, and one pepper tree (11 trees total) are proposed.
  - Addition of two new bioretention basins totaling approximately 3,462 SF (2.40% of the site).
  - Additional improvements on the site to accommodate the above proposed changes, including hardscape, new site lighting, and a trash enclosure.
  - No development is proposed within the General Plan-required, 100-foot Stream Protection Area setback from Las Vegas Creek.

The total net additional building area after demolition and new construction as described above will be approximately 6,641 SF. The Goleta Water District and the Goleta Sanitary

District would continue to provide water and sanitary sewer service to the proposed project. Currently, a portion of the on-site water usage (irrigation) is permitted to come from an on-site well (allowed as part of Case No. 92-CP-018); no change is proposed.

2. A Modification request to eliminate a requirement of Section 35-256 (1) of the Inland Zoning Ordinance to provide a garage for the three required parking spaces for the residence. While the three parking spaces for the residence are proposed to be provided, they are proposed to be uncovered spaces located within the parking lot. The three residential parking spaces are proposed approximately 25 feet from the front property line (outside of the required 20-foot front yard setback), are approximately 2 feet below the elevation of Stow Canyon Road, and are screened from view from Stow Canyon Road by proposed landscaping.
3. A Revision to the existing CUP to permit the expanded synagogue and pre-school/daycare activities as described below:
  - Synagogue:
    - Services and programs associated with the synagogue include activities typically associated with a religious institution, including worship, fellowship and educational activities. Example services and programs include various weekly religious services, holiday services, weddings, funerals, coming-of-age events, after-school programs, adult education programs, teen events, and a seasonal summer camp.
    - Activities would occur throughout the year on varying days of the week and times of the day.
    - The number of people would fluctuate depending on the specific activity. The maximum number of attendees anticipated is 113 persons, including members, guests, and staff; such events are anticipated to occur on average once per week.
    - The new synagogue has fixed seating for 81 people. Many members walk to services in observance of religious practice.
    - Services and programs described above would occur within the new 7,293-SF synagogue and support and ancillary activities may take place in a 1,980-SF portion of the existing 2,445-SF synagogue/pre-school building. Outdoor facilities (refer to the Development Plan description above) may also be utilized.
    - Although synagogue services are open to the public, special events such as interfaith-gatherings, open houses, carnivals, and similar events that would attract a larger group of people (that would exceed more than 113 persons, including members, guests, and staff) would be regulated separately as provided for in City regulations.
  - Pre-School/Daycare:
    - The pre-school/daycare would operate year-round, Monday through Friday, from 9AM through 3PM.
    - Attendees would be limited to no more than 20 students and 4 teachers.

- Events may utilize a 465-SF portion of the existing 2,445-SF synagogue/pre-school building as well as outdoor facilities (refer to the Development Plan description above).

## PARKING

As shown in Table 1, a total of 29 parking spaces are required under the City of Goleta's Inland Zoning Ordinance to simultaneously support all existing and proposed uses on the site: a residence (a single-family dwelling), a synagogue, and a pre-school/daycare. The applicant is proposing to provide 31 parking spaces and 5 bicycle parking spaces. Three of the parking spaces would be provided for the residence. As noted above, a modification is requested to have the residential parking spaces be uncovered and not be provided in a garage as required.

Table 1 Existing, Required, and Proposed Parking	
Existing Parking Spaces	<p>24 spaces, including 1 van-accessible space, provided in the existing lot as required by the existing CUP (92-CP-18).</p> <p>2 spaces provided in the residential driveway; the permit for the garage conversion included the construction of a detached garage.</p>
Parking Required for the Proposed Project <sup>1</sup>	<p><u>Existing Residence over 3,000 SF:</u>      3 spaces (within a garage)</p> <p><u>Synagogue*:</u> One per four fixed seats:      81 fixed seats/4 = 21 spaces</p> <p>*The synagogue includes office and storage space, an event hall, after-school and adult religious/educational programs, and a seasonal summer camp.</p> <p><u>Pre-School/Daycare:</u> One space per two employees:      4 employees/2 = 2 spaces One space per ten students:      30 students/10 = 3 spaces</p> <p><b><u>Total Required Parking:</u>      3+21+2+3= 29 spaces</b></p>

<b>Table 1 Existing, Required, and Proposed Parking</b>		
Proposed Parking	<u>Residence:</u>	3 uncovered standard-sized spaces <sup>2</sup>
	<u>Synagogue and Pre-School/Daycare:</u>	25 standard and 3 van-accessible spaces
	<b>Total Provided:</b>	<b>31<sup>2</sup></b>
	<b>Total Required:</b>	<b>29, including 3 within a garage</b>
<sup>1</sup> Planning Commission to determine if there is a need to provide for bicycle parking; the applicant is proposing 5 spaces. <sup>2</sup> The proposed residential parking is uncovered, not located within a garage, and subject to approval of a Modification.		

## DISCUSSION

### ***General Plan Consistency and Zoning Consistency***

The project site has a Single Family (SF) General Plan land use designation. Religious institutions are allowed uses in all of the residential General Plan categories. A detailed analysis of the project's consistency with the GP/CLUP is provided in Exhibit 3 of Attachment 1.

The project site is zoned Design Residential (D-R) 4.6. The residential use is allowed by right with the religious institution and day care/school use allowed with the approval of a CUP. As mentioned above, the County of Santa Barbara approved a CUP to allow the synagogue and the day care/school in 1992. The CUP uses are already occurring on site in one building and the proposed expansion will allow the synagogue and daycare/school to operate in separate buildings (which was the original plan that did not occur). With the exception of providing a garage for the existing residential use, the proposed project has been designed to satisfy all of the applicable development standards of the DR-4.6 zone, and those applicable to religious institutions and day care/school facilities as outlined in Exhibit 4 to Attachment 1. A modification has been requested to not provide parking for the residence in a garage and is discussed further below.

### ***Development Plan and Conditional Use Permit Revision***

#### *Uses*

All of the proposed primary uses (religious worship and day care/school) are already occurring on site in one building. The proposal will incrementally increase the intensity of the activities occurring on the site and will create additional space for them to occur. The proposal allows for the construction of a sanctuary/event hall building and various site

improvements (e.g., parking lot reorientation and expansion, as well as storm water infrastructure) needed to support the development. The day care/school use request decreases the overall number of students and teachers, while the synagogue is not expecting a substantial increase in congregants. The timing of social functions and religious education activities are intended not to overlap with the timing of either religious services or day care/school use, although this is not a condition of approval. All of the proposed activities are typically associated with religious institutions or other places of assembly as well as day cares/schools. The existing residence on the property will continue to serve as the home for the Rabbi and his family. The uses occurring on the property are in keeping with the nearby uses occurring in the neighborhood, namely residential, religious, and educational.

#### *Parking and Modification Request*

As indicated in the project description, the applicant is proposing to provide 31 parking spaces to accommodate the uses as described. Based on the applicable parking provisions, 29 spaces are required; as such, the proposed 31 parking spaces are an adequate number of parking spaces to accommodate the synagogue and daycare/school use as described. Moreover, applying the standard parking rates is considered to create a conservatively high parking space requirement as many attendees at services and holiday events often do not drive, but will walk to the site in keeping with religious practices and observances. In addition, the applicant is proposing five (5) bicycle parking spaces. Currently the City does not have a bicycle parking standard and the appropriate number of bicycle parking spaces per use is to be determined by the Planning Commission. Five bicycle parking spaces seems reasonable given the use of the site and that they would primarily be used during the weekday as bicycle riding on the Sabbath is not in keeping with religious observance. In considering the merits and design of the project, the Planning Commission should determine if five bicycle parking spaces are adequate.

In addition, adequate area exists on the project site to provide parking for construction workers and to accommodate material and equipment staging outside of the 100-foot Stream Protection Area.

As part of the project, the applicant is requesting a modification to not provide required residential parking spaces within a garage but provide the spaces as part to the open parking lot. The applicant has not requested a reduction in the number of required spaces, just in the manner they are provided. The applicant's full justification for the modification request is provided in Attachment 3. In summary, the applicant believes that not providing a garage and using the parking spaces within the parking lot would reduce traffic conflicts (cars backing onto Stow Canyon), reduce the number of curb cuts (from two to one), reduce impermeable surface areas, and reduce visual impacts to Stow Canyon Road and within the parking lot of the view of the synagogue building. The parking area will be screened from view with landscaping and there are many open parking lots along this portion of Stow Canyon Road, such as at Goleta Valley Junior High and Goleta Valley Church.



The DR zone district does allow for “parking lots, carports, and garages designed and used for individual units” (Section 35-222.4 of the Inland Zoning Code). However, single-family and two-family dwellings are required to provide parking within a garage (Section 35-256.1 of the Inland Zoning Ordinance).

As mentioned above, when the DRB conducted Conceptual Review of the project, one of the comments was that a carport for the residence with a covered walkway connecting to the house would be appropriate. Staff finds that such an alternative would be a suitable way to provide covered parking for the residence. It would be unusual for a residential structure not to have covered parking available. While not providing a garage for the residence, the carport would be consistent with the DR zone district standards.

If the Planning Commission does not approve the parking modification, either as proposed by the applicant (i.e., uncovered spaces) or as a carport alternative, then the applicant would need to redesign the project to include a three-car garage to serve the residence.

### *Traffic*

Many of the uses planned within the new, 7,293-SF synagogue building are already occurring within the existing 2,445-SF synagogue/pre-school building. Further, many attendees at services and holiday events often do not drive but will walk to the site in keeping with religious practices and observances (Associated Transportation Engineers 2018). The project’s location does not conflict with a transit plan or transit activities.

Project-related construction traffic would result primarily from construction personnel commuting to and from the project site. Given the small scale of the construction activities, it is expected that project construction would not require a substantial number of construction workers and would not generate a significant amount of construction-related traffic. To minimize conflicts with peak traffic periods of the nearby schools, staff has included a condition of approval that limits the delivery of construction materials to non-peak traffic periods when schools are not in session. Short-term traffic generation from construction activities should not conflict as the arrival and departure of construction workers and materials deliveries would have little to no overlap with traffic associated with school start and end times.

In regard to long-term use of the site, the applicant’s traffic engineer, Associated Transportation Engineers, prepared an Updated Traffic Study (see Appendix H of the Final MND) that concludes that the project would result in a net increase of 72 average daily trips (ADT), 7 AM peak hour trips, and 13 PM peak hour trips (Table 2).

<b>Table 2 Project Trip Generation Estimates (Net Increase)</b>							
<b>Land Use</b>	<b>Size</b>	<b>ADT</b>		<b>AM Peak Hour</b>		<b>PM Peak Hour</b>	
		<b>Rate</b>	<b>Trips</b>	<b>Rate</b>	<b>Trips (In/Out)</b>	<b>Rate</b>	<b>Trips (In/Out)</b>
Synagogue <sup>1</sup>	7,293 SF	9.82	72	0.98	7 (3/4)	1.78	13 (6/7)

**Notes:**

1. Trip generation rates per 1,000 SF of building area using average of ITE and SANDAG rates.

Existing levels of service for nearby key intersections, and the number of trips that the proposed project would add to each intersection, are provided in Table 3 (AM peak) and Table 4 (PM peak). These tables also identify whether the impact would be significant based on the City's Thresholds Manual. The analysis considered the improvements on Fairview Avenue that have been made since 2005. As shown, the key intersections studied currently operate in the LOS A-C range under existing conditions and the project would add between 3 and 13 peak hour trips at these locations. This analysis is considered to be conservatively high, as many attendees at services and holiday events often do not drive but will walk to the site in keeping with religious practices and observances (Associated Transportation Engineers 2018) and because proposed changes to ongoing activities are minor. It should also be noted that the Fairview Avenue/Stow Canyon Road intersection operates at LOS C with the improvements that have been completed since 2005.

<b>Table 3 Existing LOS and Project-added Traffic (AM Peak Hour)</b>				
<b>Intersection</b>	<b>Existing</b>		<b>Project-Added</b>	
	<b>V/C</b>	<b>LOS</b>	<b>Trips</b>	<b>Significant Impact?</b>
Fairview Avenue/Stow Canyon Road	19.1 Sec <sup>1</sup>	C	7	No
Fairview Ave/Calle Real	0.61	B	5	No
Fairview Ave/US101 Northbound	0.65	B	5	No
Fairview Ave/US 101 Southbound	0.53	A	3	No
<b>Notes:</b> <ol style="list-style-type: none"> <li>1. This intersection is not signalized; LOS is based on average weighted delay per vehicle, in seconds.</li> </ol>				

<b>Table 4 Existing LOS and Project-added Traffic (PM Peak Hour)</b>				
<b>Intersection</b>	<b>Existing</b>		<b>Project-Added</b>	
	<b>V/C</b>	<b>LOS</b>	<b>Trips</b>	<b>Significant Impact?</b>
Fairview Avenue/Stow Canyon Road	17.1 Sec <sup>1</sup>	C	13	No
Fairview Ave/Calle Real	0.71	C	8	No
Fairview Ave/US101 Northbound	0.60	A	8	No
Fairview Ave/US 101 Southbound	0.59	A	5	No
Notes:				
1. This intersection is not signalized; LOS is based on average weighted delay per vehicle, in seconds.				

Given the above analysis, the additional trips resulting from the project would not significantly impact traffic conditions at these intersections. Moreover, the LOS at the intersection of Fairview Avenue and Stow Canyon Road will further improve when the future planned improvements at the intersection (i.e., signalization) and condition of approval (i.e., providing an unimpeded right turn for eastbound Stow Canyon Road) are implemented.

#### *Streamside Protect Area*

Las Vegas Creek is adjacent to a portion of the western boundary of the Chabad of Santa Barbara property. The project complies with GP/CLUP Policy CE 2.2 by providing a 100-foot SPA buffer between the proposed new development (including the bioretention basin, storage building, and patio) and the top of the bank or the outer limit of wetlands and/or riparian vegetation (whichever is greater). The synagogue would be located approximately 180 feet from the buffer. Therefore, project would not result in any direct impacts to Las Vegas Creek or its SPA buffer.

#### *Beneficial Project Fee Waiver*

On July 16, 2019, the City Council adopted Resolution 19-43 establishing a fee reduction program for Beneficial Projects (Attachment 4). All projects by qualifying Non-Profit organizations and child care facilities are entitled to a 100% development impact fee reduction up to the first 15,000 square feet of the project. This project qualifies for the fee waiver as the Chabad of Santa Barbara is a non-profit as defined by the Resolution, made the fee reduction request in a timely manner, and the new building square footage is less than 15,000 square feet in area. Condition No. 55.e reflects the fee waiver and the requirements associated with the fee waiver.

#### *Findings*

The CUPRV and DP are consistent with the findings of Sections 35-315.8 and 35-317.17.7 of the Inland Zoning Ordinance as the project site has adequate size, shape,

location, and access to accommodate the density and intensity of the proposed project. In addition, adequate public services exist to serve the site as Goleta Water District, Goleta Sanitary District, and Santa Barbara County Fire Department have indicated that the project can be served. Potentially significant environmental effects of the project would be reduced to a less than significant level with the implementation of the Mitigation Measures identified in the project MND. These findings are discussed in more detail in the attached Resolution (Attachment 1).

## **ENVIRONMENTAL REVIEW**

A MND was prepared for the project by City staff pursuant to the requirements of CEQA (Pub. Resources Code §§ 21000 et seq.) and released for a 20-day review on November 1, 2019. The Final MND finds that the proposed project would not have a significant adverse effect on the environment with the implementation of the Environmental Conditions of Approval and/or Mitigation Measures for the environmental issue areas described below. The Final MND is included as Exhibit 1 of Attachment 1.

### ***Cultural Resources and Tribal Cultural Resources***

The Final MND identifies that there are potentially significant impacts in the areas of Cultural Resources and Tribal Cultural Resources that can be reduced to a level of less than significant with the adoption of three mitigation measures. In regard to Cultural Resources, the impact relates to the removal of the utilitarian-designed barn on the site that is at least 80 years old. The barn is not a particularly noteworthy structure from either a historic or architectural standpoint and is best characterized as a remnant of the past agrarian period of the area. Mitigation measures CUL-1 and CUL-2 require the building to be offered for relocation and moved to another site, if possible, and to be professionally documented before either relocation or demolition.

As described in the Final MND, the possibility of finding significant cultural resources are unlikely given the fill soils placed on the site and the lack of known Native American use of the area. However, the Santa Ynez Band of Chumash Indians indicated that it would like a mitigation measure that requires work to halt, if a resource is encountered during grading, so that the resource can be evaluated and a plan developed. Mitigation Measure CUL-3 reflects this requirement. This potentially significant impact on cultural resources/tribal cultural resources would be reduced to a less than significant level with the implementation of Mitigation Measure MM-CUL-3.

### ***Other Topic Areas***

The analysis within the Final MND did not lead to a conclusion of impacts in any other topic areas analyzed, primarily because the project would be required to comply with existing standard requirements/conditions of approval that would offset potential issues. For these reasons, the Final MND identifies conditions of approval in the areas of Aesthetics (vegetative screening from Stow Canyon Road and lighting), Biological Resources (nesting bird survey, streamside protection observation and prohibition on use of invasive species), Geology/Soils (geotechnical and soils engineering report),

Hazards/Hazardous Materials (asbestos abatement and proper disposal of trailer to be removed), Hydrology (stormwater plans, stormwater operations and maintenance agreements, and location of washing/fueling of equipment away from inlets), Noise (noise attenuation measures and timing, noticing, and other requirements during construction), and Traffic (Stow Canyon improvements and timing of construction deliveries) that lead to a finding of less than significant in these topic areas.

### **COMMENTS RECEIVED ON THE DRAFT MND**

Two comment letters were received on the Draft IS-MND and are provided as Appendix I of the Final IS/MND. One letter noted that records indicate there are no known oil wells located on the proposed project property and provides supplemental information in the event that such a well is located on the property. The other letter was provided by the applicant's agent and requested minor clarifications and corrections. Neither comment letter introduced significant new information or changed the conclusions of the Draft IS-MND. Corrections in the MND and the MMRP included:

- (1) Clarifying the nesting bird and associate buffer requirements (see edits on pages 32-34 of the Final IS/MND);
- (2) Clarifying that MM-CUL-1 requires the applicant make a good faith effort to work with a realtor (for a period no less than 90 days) to relocate the existing barn/storage garage (see page 40 of the Final IS/MND); and
- (3) Correcting language on pages 64 and 68 of the Final IS/MND to reflect the applicant's request to carry forward the ability to utilize the onsite well to supplement landscape irrigation as described in the project description.

### **PUBLIC NOTICE**

On November 27, 2019, notice for this hearing was published in the Santa Barbara Independent and mailed to property owners and tenants within 1000 feet of the project site.

### **CONCLUSION/RECOMMENDATION**

The project is consistent with the project site's Single Family (SF) land use designation and the Design Residential (DR-4.6) zoning district. The project will not result in significant effects on the environment with implementation of the Mitigation Measures and standard conditions of approval identified in the project's Final MND. All the required findings for the DP and the CUPRV can be made. Staff recommends that the Planning Commission adopt the MND and approve the DP with parking modification to require a carport with a



covered walkway and CUPRV, as outlined in the Planning Commission Resolution (Attachment 1).

### **APPEALS PROCEDURE**

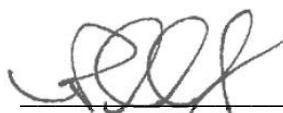
The Planning Commission is the decision-maker on this project. The Planning Commission will be the final decision-maker on land use entitlement components, unless an appeal is submitted to the City, at which time the City Council will become the final City decision-maker.

Legal Review By:

Approved By:



Winnie Cai  
Assistant City Attorney



Peter Imhof  
Planning Commission Secretary

### **ATTACHMENTS:**

1. Planning Commission Resolution No. 19-\_\_\_\_

Exhibit 1 Final Initial Study/Mitigated Negative Declaration with MMRP

Exhibit 2 Conditions of Approval

Attachment A: SBC Fire Department letter (June 14, 2019)

Attachment B: SBC Fire Department Red Curb Site Plan (June 11, 2019)

Attachment C: Goleta Sanitary District letter (February 22, 2018)

Attachment D: SBCAPCD letter (April 17, 2018)

Attachment E: Marborg Industries letter (June 13, 2019)

Exhibit 3 General Plan/Coastal Land Use Plan Consistency

Exhibit 4 Zoning Ordinance Consistency Analysis

2. DRB Minutes of December 11, 2018

3. Applicant's Modification Justification

4. Council Resolution 19-43

5. Project Plans

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## ATTACHMENT 1

### Planning Commission Resolution

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**RESOLUTION NO. 19 - \_\_\_\_**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF GOLETA, CALIFORNIA, ADOPTING THE FINAL MITIGATED NEGATIVE DECLARATION, ADOPTING THE MITIGATION MONITORING AND REPORTING PROGRAM, AND APPROVING THE DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT REVISION WITH A PARKING MODIFICATION FOR CHABAD OF SANTA BARBARA LOCATED AT 6045 STOW CANYON ROAD, GOLETA, CA; CASE NO.18-032-DP-CUPRV; APN 077-170-044**

The Planning Commission of the City of Goleta does resolve as follows:

SECTION 1: *Recitals:* The Planning Commission finds and declares that:

- A. On March 21, 2018, Steve Fort of Suzanne Elledge Planning and Permitting Services on behalf of Chabad of Santa Barbara ("Applicant"), submitted an application requesting approval of a Development Plan and a Conditional Use Permit Revision at 6045 Stow Canyon Road ("Property") to construction a new synagogue building, storage buildings, demolish an barn structure, and make other site improvements including landscaping, stormwater, and parking lot improvements (the "Project");
- B. The City reviewed the Project's environmental impacts in accordance with the California Environmental Quality Act (Public Resources Code §§ 21000 *et seq.*, "CEQA"), the regulations promulgated thereunder (14 California Code of Regulations §§ 15000 *et seq.*, the "CEQA Guidelines"), and the City's Environmental Review Guidelines ("Goleta Guidelines");
- C. On December 13, 2018, the City of Goleta deemed the application request complete;
- D. On January 3, 2019, requests for consultation with local Native American groups were mailed. The consultation period concluded on February 2, 2019 with no requests for formal consultation received;
- E. After preparing an Initial Study, the Director of Planning and Environmental Review determined that a Mitigated Negative Declaration (MND) should be prepared for the project pursuant to CEQA Guideline §§ 15063 and 15070;
- F. The Final MND for the Chabad of Santa Barbara Development Plan and Conditional Use Permit Revision Project with a parking modification attached as Exhibit 1 to this Resolution and incorporated by reference, was prepared in full compliance with CEQA, and concluded that the project will not have a significant effect on the environment;



- G. On December 9, 2019, the Planning Commission conducted a duly noticed public hearing at which all interested parties were heard. Further, the Planning Commission considered the entire administrative record including, without limitation, staff reports, and evidence submitted during the public hearing.

SECTION 2 *Factual Findings and Conclusions:* The Planning Commission finds as follows:

- A. The Project site is 3.3 acres in size (Assessor's Parcel No. 077-170-044). The Project site has General Plan land use designation of Single Family (S-F), and a Zoning designation of Design Residential (DR-4).
- B. Access to the Project site is from the Stow Canyon Road.
- C. On October 28, 1992, the County of Santa Barbara Board Planning Commission approved Conditional Use Permit (92-CP-018) for the construction and operation of an 873-square foot synagogue building, a 2,235-square foot daycare and school facility on the property. These improvements were in addition to the existing, 2,002-square foot residence (a single-family dwelling) and 1,440-square foot barn/storage garage on the site.

Subsequently, two Substantial Conformity Determinations (SCDs) for the project were approved. The first SCD increased the size of the proposed office/meeting room building and the proposed daycare and school facility building by 210 square feet each to a new total of 2,445 square feet. The second SCD, approved relocating the proposed driveway 18 to 20 feet to the east, and extended the parking turnaround a maximum of 26 feet to the south, to retain existing mature fruit trees and to retain the existing barn for storage.

Ultimately, only the approved 2,445-square foot daycare and school facility and revised parking lot were completed. The existing, 2,445-square foot daycare and school facility has been modified to serve all of the functions (daycare, school, office, meeting room, and synagogue worship space).

- D. Chabad of Santa Barbara requests approval of a Development Plan (DP) to allow the following:
- 1) Construction of a new, approximately 7,293-square foot, one-story synagogue with an 815-square foot roof terrace (8,108 square foot total). The building will include a sanctuary, event hall, kitchen, offices, conference rooms, bathrooms, storage, and mechanical and electrical space. The tallest proposed roof line is 24 feet, 6.5 inches, although the building also includes an architectural projection that would have a maximum height of 34 feet, 7.25 inches. Solar panels will also be included on the roof.
  - 2) Construction of a new, 841-square foot, one-story storage building. The maximum height would be approximately 16 feet.

- 3) Removal of the existing 1,440-square foot, approximately 80-year-old barn/storage garage.
  - 4) Approval of the existing, 176-square foot, open-sided, lean-to storage container.
  - 5) Removal/demolition of an existing, 120-square foot trailer and a 53- square foot shed.
  - 6) Consolidation of access to the site into a single driveway. This entails the removal of the existing residential driveway and the removal and relocation of the existing synagogue driveway to approximately 35 feet west of its current location.
  - 7) Replacement, reorientation, and expansion of the existing 24-space parking area (including 1 van-accessible space) with a 31-space parking area. The 31-space parking area would include 3 van-accessible spaces as well as 3 uncovered spaces designated for the residence.
  - 8) Addition of landscape improvements to integrate the proposed synagogue and storage building with existing buildings and provide landscape buffers for neighboring properties and the Las Vegas Creek. Plants are intended to be generally native and drought tolerant; 24 trees to be planted include coast live oak (6), California sycamore (5), Catalina ironwood (1), and New Zealand Christmas tree (12). The removal of 8 fruit trees (various types), 2 myoporum trees, and one pepper tree (11 trees total) are proposed.
  - 9) Addition of two, new bioretention basins totaling approximately 3,462 square feet (2.40% of the site).
  - 10) Additional improvements on the site to accommodate the above proposed changes, including hardscape, new site lighting, and a trash enclosure.
  - 11) No development is proposed within the General Plan required 100-foot Stream Protection Area setback from Las Vegas Creek.
- E. Chabad of Santa Barbara requests approval of a Revision to the existing Conditional Use Permit No. 92-CP-018 to permit the expanded synagogue and pre-school/daycare activities as described below:
- 1) Synagogue:
    - a) Services and programs associated with the synagogue include activities typically associated with a religious institution, including worship, fellowship and educational activities. Example services and programs include various weekly religious services, holiday services, weddings, funerals, coming-of-age events, after-school programs, adult education programs, teen events, and a seasonal summer camp.
    - b) Activities would occur throughout the year on varying days of the week and times of the day.
    - c) The number of people would fluctuate depending on the specific activity. The maximum number of attendees anticipated is 113 persons, including members, guests, and staff; such events are anticipated to occur on average once per week.

- d) The new synagogue has fixed seating for 81 people. Many members walk to services in observance of religious practice.
  - e) Services and programs described above would occur within the new, 7,293-square foot synagogue and support and ancillary activities may take place in a 1,980-square foot portion of the existing, 2,445-square foot synagogue/pre-school building. Outdoor facilities (refer to the Development Plan description above) may also be utilized.
  - f) Although synagogue services are open to the public, special events, such as interfaith-gatherings, open houses, carnivals, and similar events that would attract a larger group of people (that would exceed more than 113 persons, including members, guests, and staff) would be regulated separately as provided for in City regulations.
2. Pre-School/Daycare:
- a) The pre-school/daycare would operate year-round, Monday through Friday, from 9AM through 3PM.
  - b) Attendees would be limited to no more than 20 students and 4 teachers.
  - c) Events may utilize a 465-square foot portion of the existing, 2,445 square foot synagogue/pre-school building as well as outdoor facilities (refer to the Development Plan description above).
- F. The project includes a modification request to eliminate the requirement of Section 35-256 (1) of the Inland Zoning Ordinance to provide a garage for the three required parking spaces for the residence and allow the three residential parking spaces be provided as uncovered parking spaces within the synagogue parking lot.
- G. There are currently two (2) parking spaces provided in the residential driveway and 24 parking spaces provided for the synagogue and daycare/school uses. After implementation of the project, 31 parking spaces would be provided (all spaces would be uncovered as proposed) including the residential parking spaces, which is two (2) more than by existing regulations for combined uses on the site.
- H. The project site is generally level and consists of one Assessor Parcel.
- I. Ingress and egress to the project site is adequate to serve existing uses and those of the project. Ingress/egress is also in compliance with Santa Barbara County Fire Department requirements.
- J. The factual findings and conclusions in this Section are based upon substantial evidence found within the entirety of the administrative record.

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**SECTION 3:** *Environmental Assessment for the Project.* The Planning Commission makes the following environmental findings:

- A. The City completed a Final MND for the project in accordance with applicable law including, without limitation, CEQA Guidelines §§ 15070, 15071 and 15073.
- B. The City distributed the Draft MND for public review and comment for a period of 20 days from November 1, 2019 to November 20, 2019 and responded to all comments received in writing by updating the Final MND as appropriate.
- C. The Final MND and the Mitigation and Monitoring Reporting Program (MMRP) were presented to the Planning Commission, which reviewed the record of the proceedings and considered all information contained in the Final MND and its appendices, the MMRP, and the testimony and additional information presented at or before all public hearings in accordance with CEQA Guidelines §15074.
- D. Pursuant to CEQA Guidelines §15074, the Final MND reflects the City's independent judgment and analysis. The Planning Commission has independently reviewed and analyzed the Final MND prepared for the Project. The Final MND is an accurate and complete statement of the potential environmental impacts of the project. The Final MND was prepared by the City of Goleta Planning and Environmental Review Department and reflects its independent judgment and analysis of the environmental impacts.

**SECTION 4:** *Conditional Use Permit Revision Findings.* The Planning Commission makes the following findings pursuant to Section 35-315.8 of the Inland Zoning Ordinance:

- A. *The site is adequate in size, shape, locations, and physical characteristics to accommodate the type and use of development proposed.*

The uses are already occurring on the 3.3-acre site and are consistent with those allowed under the DR-4 zone district and the Single-Family General Plan land use designations either by right (the residence) or with approval of a Conditional Use Permit (Synagogue and Day Care/Preschool). Given the size, shape, location and physical characteristics of the project site, the existing structures that are to be retained, and the proposed buildings and improvements to be constructed can be easily accommodated on the property. The proposed site improvements, including landscaping, reconfigured parking, and drainage facilities are accessory and customary development and will not adversely affect the density or intensity of uses conducted on the project site. Therefore, the project site will remain adequate in size, shape, location and physical characteristics to accommodate the density and intensity of the proposed development.

*B. Significant impact has been mitigated to the maximum extent feasible.*

The project has been fully analyzed in the Final MND provided as Exhibit 1, which identified potentially significant project-related environmental impacts pertaining to cultural and tribal cultural resources (demolition of an 80-year-old barn and unlikely unanticipated impacts to buried cultural resources). With the implementation of mitigation measures identified by Final MND and other conditions of approval referenced within the Final MND, the proposed project's potentially significant environmental impacts would be reduced to a less than significant level. In addition, the project will observe the 100' Streamside Protection Area setback from Las Vegas Creek.

*C. The streets and highways are adequate and properly designed.*

Access to the project site is from Stow Canyon Road. Many of the uses planned to occur are already occurring (religious worship, social events, school programs and storage) and many attendees at services and holiday events often walk to the site in keeping with religious practices and observances. The closest intersection to the site, Fairview and Stow Canyon operate at Level of Service C (Associated Transportation Engineers 2018, Revised 2019). The streets and highways that serve the project site are adequate and properly designed to accommodate the project based on the improvements constructed since 2005 and those that are still anticipated to be made by the City in order to accommodate the traffic generated by Goleta Valley Jr. High School. The project is not anticipated to add substantial traffic overall or during peak time periods given the uses and the time of day those uses occur in relationship to the other uses that are served by Stow Canyon Road (same citation). Lastly, the project is eliminating and consolidating driveways into one location in keeping with City standards to have the site served by a single driveway. The consolidated driveway will improve traffic flow on Stow Canyon Road and reduce vehicle conflicts as all vehicles would be exiting the site in a forward fashion.

*D. There are adequate public services, including but not limited to fire protections, water supply, sewage disposal, and police protection to serve the project.*

There are adequate public services to serve the project and the site will continue to be served by the Santa Barbara County Fire Protection District (SBCOFD), Goleta Water District (GWD), Goleta Sanitary District (GSD), and Sheriff's Department. SBCOFD staff has indicated that through compliance with SBCOFD standard conditions (e.g., fire sprinklers, proper addressing, and payment of required development impact fees, etc.) the provision of services can be accommodated. GWD and GSD have indicated that adequate water and sewer facilities exist to serve the project based on the uses contained in the project description. The project would not result in an increase in population at the project site and would not result in an increased demand for Sheriff's Department services.



- E. The project will not be detrimental to the health, safety, comfort, convenience, and general welfare of the neighborhood and will be compatible with the surrounding area.*

The project would not be detrimental to the health, safety, comfort, convenience and general welfare of the neighborhood, and will not be incompatible with nearby religious, educational and residential uses. The project site has been developed and used for residential, religious, and education purposes since 1992 and would continue to be so. The proposed project would not substantially change the visual character of the site, would not increase the number of people located at the site, and would not result in substantial increase in existing traffic conditions.

While the square footage devoted to the worship and event space are being enlarged, the number of congregants is not expected to increase substantially. In addition, the number of students to be served by the day care/school is proposed to be reduced. The other ancillary activities (religious classes, youth activities, summer camp, etc.) are in keeping with the activities expected to occur at any house of worship and will be compatible with the surrounding area particularly given the adjacency of other religious institution and school campus. Lastly, the use, the type of activities of the property (residential, religious, school, day care) and the time of day that activities would occur will conflict or be detrimental to the health, safety, comfort, convenience and general welfare of the neighborhood based on the nature of the events.

- F. The project is in conformation with the applicable provisions and policies of the Inland Zoning Ordinance and the General Plan/Coastal Land Use Plan*

As evaluated in Exhibit 3 of this Resolution, the proposed site improvements are consistent with the General Plan land use designation of Single Family (S-F) and the applicable General Plan policies. In keeping with General Plan policies, the proposed project and improvements will not encroach into the 100' Streamside Protection Area setback, will not impact level of service on adjacent roads, and will continue to provide on-site housing for the Rabbi.

The on-site uses are either allowed as permitted use (residence) or with the approval of a Conditional Use Permit (Synagogue and day care/school) in the DR-4 zone. As evaluated in Exhibit 4 of this Resolution, the project complies with all setbacks (including SPA buffer requirements), height, landscaping, lot coverage, and number of parking spaces standards applicable to the site and the uses. However, implementation of the project as proposed, requires a modification as to how the parking for the residential use is provided. As part of the Development Plan findings, the Planning Commission will determine if the modification will be granted or if the applicant will need to revise the project to include a three-car garage for the residence.

*G. In designated rural areas, the use is compatible with and subordinate to the scenic and rural character of the area.*

This finding is not applicable as the site is not in a designated rural area.

**SECTION 5: Development Plan with Modifications Findings.** The Planning Commission makes the following findings pursuant to Section 35-317.17.7 of the Inland Zoning Ordinance:

*A. The site for the project is adequate in size, shape, locations and physical characteristics to accommodate the density and intensity of development proposed.*

The uses are already occurring on the 3.3-acre site and are consistent with those allowed under the DR-4 zone district and the Single-Family General Plan land use designations either by right (the residence) or with approval of a Conditional Use Permit (Synagogue and Day Care/Preschool). Given the size, shape, location and physical characteristics of the project site, the existing structures that are to be retained, and the proposed buildings and improvements to be constructed can be easily accommodated on the property. The proposed site improvements, including landscaping, reconfigured parking, and drainage facilities are accessory and customary development and will not adversely affect the density or intensity of uses conducted on the project site. Therefore, the project site will remain adequate in size, shape, location and physical characteristics to accommodate the density and intensity of the proposed development.

*B. Adverse impacts are mitigation to the maximum extent feasible.*

The project has been fully analyzed in the Final MND Exhibit 1, which identified potentially significant project-related environmental impacts pertaining to cultural and tribal cultural resources (demolition of an 80-year-old barn and unlikely unanticipated impacts to buried cultural resources). With the implementation of mitigation measures identified by Final MND and other conditions of approval, the proposed project's potentially significant environmental impacts would be reduced to a less than significant level. In addition, the project will observe the 100' Streamside Protection Area setback from Las Vegas Creek.

*C. Streets and highways are adequate and properly designed.*

Access to the project site is from Stow Canyon Road. Many of the uses planned to occur within the new buildings are already occurring (religious worship, social events, and storage) and many attendees at services and holiday events often walk to the site in keeping with religious practices and observances. The closest intersection to the site, Fairview and Stow Canyon operate at Level of Service C (Associated Transportation Engineers 2018, Revised 2019). The streets and highways that serve the project site are adequate and properly designed to

accommodate the project with improvements constructed since 2005 and those that are anticipated to be made in order to accommodate the traffic generated by Goleta Valley Jr. High School. The project is not anticipated substantial traffic overall or during peak time periods given the uses and the time of day those uses occur in relationship to the other uses that are served by Stow Canyon Road.

- D. There are adequate public services, including but not limited to, fire protection, water supply, sewage disposal, and police protection to serve the project.*

There are adequate public services to serve the project, and the project site will continue to be served by the Santa Barbara County Fire Protection District, Goleta Water District (GWD), Goleta Sanitary District (GSD), and Sheriff's Department. The Fire Protection District has indicated that through compliance with Fire Department standard conditions (e.g., fire sprinklers, proper addressing, and payment of required development impact fees) the project would not result in significant fire protection services impacts. GWD and GSD have indicated that adequate facilities and capacities exist to serve the project. The project would not result in an increase in population at the project site and would not result in an increased demand for Sheriff's Department services.

- E. The project will not be detrimental to the health, safety, comfort, convenience, and general welfare of the neighborhood and will not be incompatible with the surrounding areas.*

The project would not be detrimental to the health, safety, comfort, convenience and general welfare of the neighborhood, and will not be incompatible with nearby religious, educational and residential uses. The project site has been developed and use for residential, religious, and education uses since 1992 and would continue to be so. The proposed project would not substantially change the visual character of the site, would not increase the number of people located at the site, and would not result in substantial increase in existing traffic conditions.

- F. The project is in conformance with the 1) the Comprehensive Plan and 2) the applicable provisions of this Article and/or the projects falls with the limited exception allowed under Section 35-306.7.*

As evaluated in Exhibit 3 of this Resolution, the proposed site improvements are consistent with the General Plan land use designation of Single Family (S-F) and the applicable General Plan policies. In keeping with General Plan policies, the proposed improvements will not encroach into the 100' Streamside Protection Area setback, will not impact level of service on adjacent roads, and will continue to provide on-site housing for the Rabbi.

The uses are either allowed as permitted use (residence) or with the approval of a Conditional Use Permit (synagogue and day care/school) in the DR-4 zone. As

evaluated in Exhibit 4 of this Resolution, the project complies with all setbacks (including SPA buffer requirements), height, landscaping, lot coverage, and number of parking spaces standards applicable to the site and the uses. However, implementation of the project as proposed, requires modification to how the parking for the residential use is provided. The provision of the three parking spaces within a carport would be a compromise between the provision of garage spaces and fully uncovered spaces. The provision of a carport over the area already designated for the residential spaces should not be difficult to accomplish and would not be out of character for other residential units located in the DR-4 zoning district. Overall, the requested modification would be compatible with the adjacent neighborhood and would not create an adverse impact to community character, aesthetics or public views.

- G. In designated rural areas the use is compatible with and subordinate to the scenic and rural character of the area.*

This finding is not applicable as the site is not in a designated rural area.

- H. The project will not conflict with any easements required for public access through, or public use of a portion of the property.*

The project will not conflict with any easements on the property. The development has been designed to respect an easement for access to a water well that an adjacent property hold the rights for.

**SECTION 6:** *Actions.* The Planning Commission hereby takes the following actions:

- A. Adopt the MND and MMRP provided as Exhibit 1 to the Resolution, which is incorporated by reference and directs staff to file the Notice of Determination within five (5) business days.
- B. Approve the Development Plan and Conditional Use Permit Revision with a modification to allow a carport in lieu of a garage, Case No. 18-032-DP-CUPRV, based on the Findings above and the General Plan and Zoning Consistency analyses contained in Exhibit 3 and 4 to this Resolution, subject to the Conditions of Approval attached as Exhibit 2 to this Resolution, and incorporated by reference.

**SECTION 7:** *Reliance on Record.* Each and every one of the findings and determinations in this Resolution are based on the competent and substantial evidence, both oral and written, contained in the entire record relating to the Project. The findings and determinations constitute the independent findings and determinations of the Planning Commission in all respects and are fully and completely supported by substantial evidence in the record as a whole.

SECTION 8: *Limitations.* The Planning Commission's analysis and evaluation of the Project is based on the best information currently available. In all instances, best efforts have been made to form accurate assumptions. Somewhat related are the limitations on the City's ability to solve what are in effect regional, state and national problems and issues. The City must work within the political framework within which it exists and with the limitations inherent in that framework.

SECTION 9: *Summaries of Information.* All summaries of information in the findings, which precede this section, are based on the substantial evidence in the record. The absence of any particular fact from any such summary is not an indication that a particular finding is not based in part on that fact.

SECTION 10: This Resolution will remain effective until superseded by a subsequent Resolution.

SECTION 11: The City Clerk is directed to mail a copy of this Resolution to Steve Fort of Suzanne Elledge Planning and Permitting Services and to any other person requesting a copy.

SECTION 12: This Resolution will become effective immediately upon adoption.

SECTION 13: The City Clerk will certify to the passage and adoption of this resolution and enter it into the book of original resolutions.

**PASSED, APPROVED AND ADOPTED** this \_\_\_\_ day of \_\_\_\_\_ 2019.

\_\_\_\_\_  
JENNIFER R. SMITH, CHAIR

ATTEST:

APPROVED AS TO FORM:

\_\_\_\_\_  
DEBORAH LOPEZ  
CITY CLERK

\_\_\_\_\_  
WINNIE CAI  
ASSISTANT CITY ATTORNEY

STATE OF CALIFORNIA            )  
COUNTY OF SANTA BARBARA    )       ss.  
CITY OF GOLETA                 )

I, DEBORAH S. LOPEZ, City Clerk of the City of Goleta, California, DO HEREBY CERTIFY that the foregoing Resolution No. 19 -\_\_ was duly adopted by the City Council of the City of Goleta at a regular meeting held on the \_\_\_\_ day of \_\_\_\_\_, 2019 by the following vote of the City Council:

AYES:

NOES:

ABSENT:

(SEAL)

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DEBORAH S. LOPEZ  
CITY CLERK

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## ATTACHMENT 1

### EXHIBIT 1

Final Initial Study/Mitigated Negative Declaration with MMRP

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**CITY OF GOLETA  
FINAL INITIAL STUDY AND  
MITIGATED NEGATIVE DECLARATION**

The Draft Initial Study-Mitigated Negative Declaration (Draft IS-MND) was circulated for a 20-day public review period that commenced on November 1, 2019 and concluded on November 20, 2019. Two comment letters were received on the Draft IS-MND and are provided as Appendix I of this document. One letter noted that records indicate there are no known oil wells located on the proposed project property and provides supplemental information in the event that such a well is located on the property. The other letter was provided by the applicant's agent and requested minor clarifications and corrections. Neither comment letter introduced significant new information or changed the conclusions of the Draft IS-MND. Where changes have been made to the document in response to the applicant's letter and/or corrections by staff, these are indicated in ~~strikeout~~/underline format.

**1. PROJECT TITLE:**

Case No. 18-031-DP-CUPRV-DRB  
New Synagogue Buildings and Site Improvements for Chabad of Santa Barbara

**2. LEAD AGENCY NAME AND ADDRESS:**

City of Goleta  
Planning and Environmental Review  
130 Cremona Drive, Suite B  
Goleta, CA 93117

**3. CONTACT PERSON AND PHONE NUMBER:**

Chris Noddings  
Assistant Planner  
(805) 961-7566  
cnoddings@cityofgoleta.org

Mary Chang  
Senior Supervising Planner  
(805) 961-7567  
mchang@cityofgoleta.org

**4. APPLICANT:**

Chabad of Santa Barbara  
6045 Stow Canyon Road  
Goleta, CA 93117  
(805) 683-1544  
Attn: Rabbi Mendel Loschak

**AGENT:**

SEPPS  
1625 State Street, Suite 1  
Santa Barbara, CA 93101  
(805) 966-2758 X101  
Attn: Steve Fort

**5. PROJECT LOCATION:**

The project site is located at 6045 Stow Canyon Road, west of North Fairview Avenue in the City of Goleta (City). The property encompasses approximately 3.3 acres (see Figure 1). The Assessor Parcel Number (APN) is 077-170-044.

**Figure 1: Project Location and Vicinity**



Chabad of Santa Barbara has been using the project site since the existing Conditional Use Permit (92-CP-018) was approved in 1992. Current uses on the site include a residence (used by the Rabbi) as well as a building that is used as a synagogue, a school, and a day care. The existing Conditional Use Permit outlined the maximum number of individuals that may be in attendance at the project site as 50 individuals at the synagogue, 25 at the school, and 15 at the daycare. In addition, three full-time and three part-time staff may also be at the site. However, the Religious Land Use and Institutionalized Persons Act (RLUIPA), 42 U.S.C. §§ 2000cc, et seq., signed into law on September 22, 2000, effectively made limitations such as the number of attendees allowed unenforceable.

## **6. PROJECT DESCRIPTION:**

Chabad of Santa Barbara has requested approval of a Development Plan and a Revision to an Existing Conditional Use Permit (18-031-DP-CUPRV) that would add a new synagogue and storage building, remove an existing barn/storage garage, and make other site improvements. A request for one modification related to how parking is provided for the existing residence is also included.

The project consists of the following components as shown in the proposed site plan in Figure 2 and detailed in the project application:

1. A Development Plan (DP) to allow for the following:

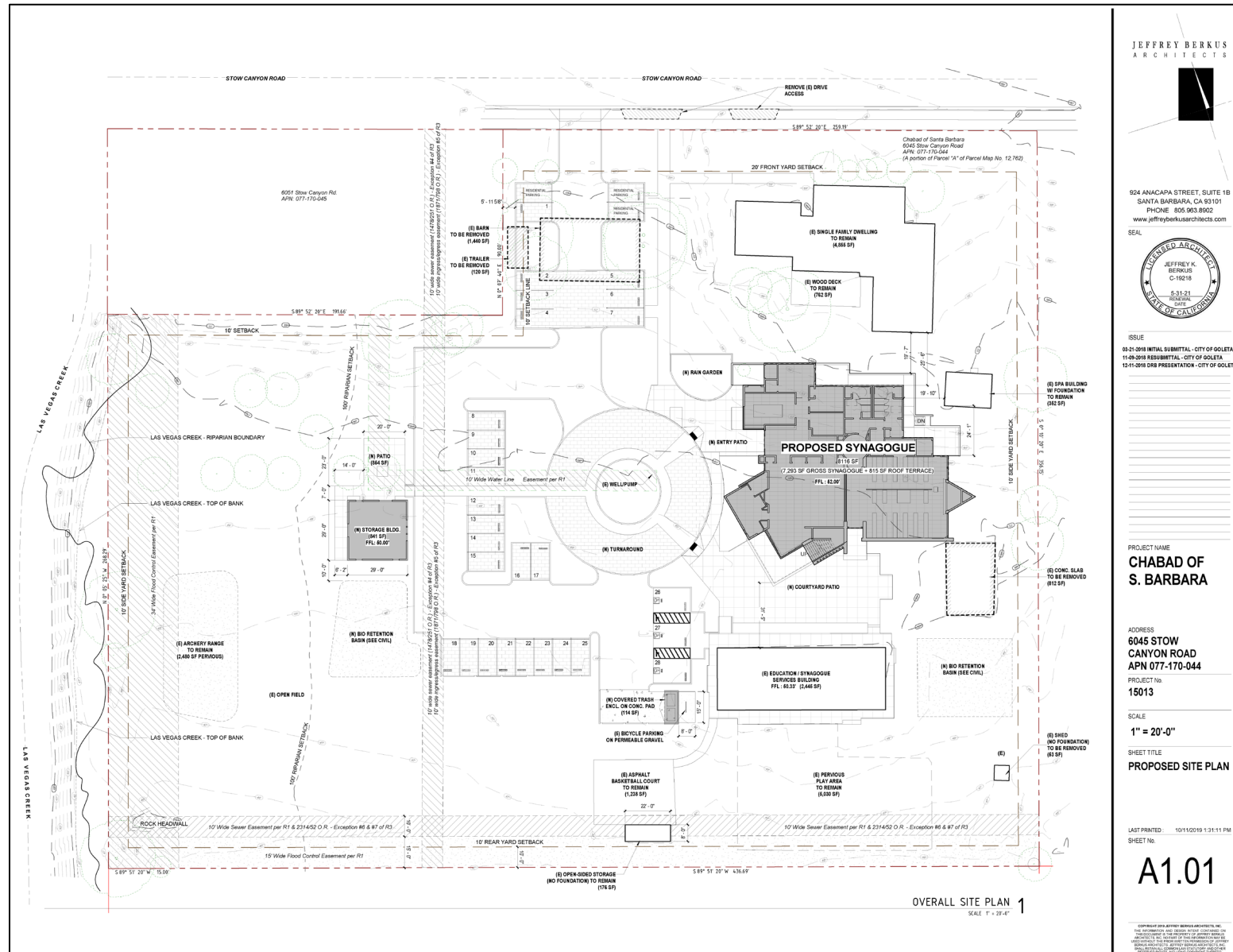
- Construction of a new, approximately 7,293 square foot (SF) one-story synagogue with a 815 SF roof terrace (8,108 SF total). The building will include a sanctuary, event hall, kitchen, offices, conference rooms, bathrooms, storage, and mechanical and electrical space. The tallest proposed roof line is 24 feet, 6.5 inches, although the building also includes an architectural projection that would have a maximum height of 34 feet, 7.25 inches. Solar panels will also be included on the roof.
- Construction of a new, 841 SF, one-story storage building. The maximum height would be approximately 16 feet.
- Removal of the existing 1,440 SF, approximately 80-year-old barn/storage garage.
- Approval of the existing, 176-SF, open-sided, lean-to storage container.
- Removal/demolition of an existing, 120-SF trailer and a 53-SF shed.
- Consolidation of access to the site into a single driveway. This entails the removal of the existing residential driveway and the removal and relocation of the existing Synagogue driveway to approximately 35 feet west of its current location.
- Replacement, reorientation, and expansion of the existing 24-space parking area (including 1 van-accessible space) with a 31-space parking area. The 31-space parking area would include 3 van-accessible spaces as well as 3 uncovered spaces designated for the residence.
- Addition of landscape improvements to integrate the proposed synagogue and storage building with existing buildings and provide landscape buffers for neighboring properties and the Las Vegas Creek. Plants are intended to be generally native and drought tolerant; 24 trees to be planted include coast live oak (6), California sycamore (5), Catalina ironwood (1), and New Zealand Christmas tree (12). The removal of 8 fruit trees (various types), 2 myoporum trees, and one pepper tree (11 trees total) are proposed.
- Addition of two new bioretention basins totaling approximately 3,462 SF (2.40% of the site).
- Additional improvements on the site to accommodate the above proposed changes, including hardscape, new site lighting, and a trash enclosure.
- No development is proposed within the General Plan required 100-foot Stream Protection Area setback from Las Vegas Creek.

As shown in Table 1 below, the total net additional building area after demolition and new construction as described above will be approximately 6,641 SF. The Goleta Water District and the Goleta Sanitary District would continue to provide water and sanitary sewer service to the proposed project. Currently, a portion of the on-site water usage (irrigation) is permitted to come from an on-site well (allowed as part of Conditional Use Permit 92-CP-018); no change is proposed.

2. A Modification request to eliminate the requirement of Section 35-256 (1) of the Inland Zoning Ordinance to provide a garage for the three required parking spaces for the residence. While the three parking spaces for the residence are proposed to be provided, they are proposed to be uncovered spaces located within the parking lot. The three residential parking spaces are proposed approximately 25 feet from the front property line (outside of the required 20-foot front yard setback), are approximately 2 feet below the elevation of Stow Canyon Road, and are screened from view from Stow Canyon Road by proposed landscaping.

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### Figure 2: Proposed Architectural Site Plan



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<b>Table 1</b> <b>Proposed Project and Existing</b> <b>Lot Coverage Summary</b>					
	<b>Existing</b>		<b>Proposed</b>		
<b>Case No: 18-031 DP and CUPRV</b> <b>APN: 077-170-044.</b>	SF	% Cover	SF	% Cover	<b>Proposed Change</b>
Lot: 3.29 Acres (Gross)	143,996	N/A	143,996	N/A	N/A
<b>Buildings</b>					
Residence (Single-family Dwelling) <sup>1</sup> (Includes 762 SF Exterior Deck)	4,855	3.37%	4,855	3.37%	None
Synagogue/Pre-School <sup>1</sup>	2,445	1.70%	2,445	1.70%	Convert religious service space to office use.
Spa <sup>1</sup>	352	0.24%	352	0.24%	None
Storage Shed <sup>2</sup>	53	0.04%	0	-	Building Demolition
Barn/Storage Garage <sup>1</sup>	1,440	1.00%	0	-	Building Demolition
New Synagogue <sup>3</sup> (815 SF roof terrace excluded)	0	-	7,293	5.06%	New Building Construction
New Storage Building <sup>3</sup>	0	-	841	0.58%	New Building Construction
<b>Total Buildings</b>	<b>9,145</b>	<b>6.35%</b>	<b>15,786</b>	<b>10.96%</b>	<b>+6,641 SF</b> <b>(+4.61%)</b>
<b>Accessory/Equipment</b>					
Open-sided storage <sup>3</sup>	176	0.12%	176	0.12%	None
Storage Trailer	120	0.08%	0	-	To be removed
Trash Enclosure <sup>3</sup>	0	-	114	0.08%	New Construction
<b>Other Impermeable Surfaces</b>					
Parking/Drive Aisles (Impervious)	16,360	11.36%	15,319	10.64%	-1,041 SF (-0.72%)
Pathways/Other Hardscape	1,629	1.13%	9,369	6.51%	+7,740 SF (+5.38%)
Half Basketball Court <sup>1</sup>	1,238	0.86%	1,238	0.86%	None
<b>Pervious Areas</b>					
Landscape/Open Space	107,818	74.88%	81,798	56.81%	-26,020 SF (-18.07%)
Parking/Drive Aisles (Pervious)	0	-	9,224	6.41%	+9,244 SF (+6.41%)
Archery Range <sup>2</sup>	2,480	1.72%	2,480	1.72%	None
Play Area <sup>1</sup>	5,030	3.49%	5,030	3.49%	None
Bioretention Basins	0	-	3,462	2.40%	+3,462 SF (+2.40%)
<b>Total Lot</b>	<b>143,996</b>	<b>100%</b>	<b>143,996</b>	<b>100%</b>	
Additional, Offsite Demolition (E.g., Parkway, Driveways)	975	N/A	525	N/A	Demolish and replace both existing driveways with curb and gutter; demolish existing curb and gutter and replace with a new driveway approach.
SF = square feet; % = percent; N/A = Not Applicable Notes: <sup>1</sup> Feature was previously permitted. <sup>2</sup> Feature does not require a permit. <sup>3</sup> Feature is proposed to be permitted.					

3. A Revision to the existing Conditional Use Permit (92-CP-018) to permit the expanded synagogue and pre-school/daycare activities as described below:

- Synagogue:
  - Services and programs associated with the synagogue include activities typically associated with a religious institution, including worship, fellowship and

educational activities. Example services and programs include various weekly religious services, holiday services, weddings, funerals, coming-of-age events, after-school programs, adult education programs, teen events, and a seasonal summer camp.

- Activities would occur throughout the year on varying days of the week and times of the day.
- The number of people would fluctuate depending on the specific activity. The maximum number of attendees anticipated is 113 persons, including members, guests, and staff; such events are anticipated to occur on average once per week.
- The new synagogue has fixed seating for 81 people. Many members walk to services in observance of religious practice.
- Services and programs described above would occur within the new 7,293-SF Synagogue and support and ancillary activities may take place in a 1,980-SF portion of the existing 2,445-SF synagogue/pre-school building. Outdoor facilities (refer to the Development Plan description above) may also be utilized.
- Although synagogue services are open to the public, special events such as interfaith-gatherings, open houses, carnivals, and similar events that would attract a larger group of people (that would exceed more than 113 persons, including members, guests, and staff) would be regulated separately as provided for in City regulations.

○ Pre-School/Daycare:

- The pre-school/daycare would operate year-round, Monday through Friday, from 9AM through 3PM.
- Attendees would be limited to no more than 20 students and 4 teachers.
- Events may utilize a 465-SF portion of the existing 2,445-SF synagogue/pre-school building as well as outdoor facilities (refer to the Development Plan description above).

4. As shown in Table 2, a total of 29 parking spaces are required under the City of Goleta's Inland Zoning Ordinance to simultaneously support all existing and proposed uses on the site: a residence (a single-family dwelling), a synagogue, and a pre-school/daycare.

<b>Table 2 Existing, Required, and Proposed Parking</b>	
Existing Parking Spaces	<p>24 spaces, including 1 van-accessible space, provided in the existing lot as required by the existing Conditional Use Permit (92-CP-18).</p> <p>2 spaces provided in the residential driveway; the permit for the garage conversion included the construction of a detached garage.</p>
Parking Required for the Proposed Project <sup>1</sup>	<p><u>Existing Residence over 3,000 SF:</u> 3 spaces (within a garage)</p> <p><u>Synagogue*:</u> One per four fixed seats 81 fixed seats/4 = 21 spaces *The synagogue includes office and storage space, an event hall, after-school and adult religious/educational programs, and a seasonal summer camp.</p>

<b>Table 2 Existing, Required, and Proposed Parking</b>	
	<u>Pre-School/Daycare:</u> One space per two employees 4 employees/2 = 2 spaces One space per ten students 30 students/10 = 3 spaces  <b><u>Total Required Parking:</u></b> <b>3 + 21 + 2 + 3 = 29</b>
Proposed Parking Spaces	<u>Residence:</u> 3 Standard <sup>2</sup>  <u>Synagogue and Pre-School/Daycare:</u> 25 Standard and 3 Van-accessible  <b>Total Provided: 31<sup>2</sup></b> <b>Total Required: 29, including 3 within a garage</b>
<sup>1</sup> Planning Commission to determine if there is a need to provide for bicycle parking; the applicant is proposing <u>5</u> spaces. <sup>2</sup> The proposed residential parking is uncovered, not located within a garage, and subject to approval of a Modification.	

## 7. BACKGROUND INFORMATION

Historically, the proposed project site was used for cattle ranching starting in the mid-1800's when it was purchased by Colonel W.W. Hollister from Nicholas A. Den, an Irish physician who had obtained rights to the 15,000-acre parcel in 1842. Subsequent owners continually raised the elevation of the farmland, re-routed Las Vegas Creek within the project boundary between 1929 and 1938, and planted avocado and citrus orchards (Stone 2018). However, there is no evidence that the site was still used for cultivation in the 1970s and 1980s, and it is known that commercial vehicles were stored on the site as part of a backhoe business without permits during a portion of this time (Bookspan 2007). Today, only a few of the lemon and avocado trees remain on the western section of the property.

On October 28, 1992, the Santa Barbara County Planning Commission approved 92-LA-013 and 92-CP-018 for the "Loschak Synagogue, School, [and Lot] Line Adjustment" for APNs 077-170-031 and -032 for 6045 and 6051 Stow Canyon Road, respectively. Today, the APNs for these properties are 077-140-044 and 45, respectively, and the Synagogue is known as "Chabad of Santa Barbara."

The Conditional Use Permit (92-CP-018) allowed for the construction and operation of an 873 SF synagogue, 2,235 SF office/meeting room, and a 2,235 SF daycare and school facility on the property. These improvements were in addition to the existing 2,002-SF residence (a single-family dwelling) and a 1,440-SF barn/storage garage on site. As part of the proposed project, the barn was to be removed to allow for the construction of a new driveway. Subsequently, the Deputy Director of Santa Barbara County's Development Review Division approved two Substantial Conformity Determinations (SCDs) for the project. The first SCD, approved on May 26, 1994, increased the size of the proposed office/meeting room building and the proposed daycare and school facility buildings by 210 SF each to a new total of 2,445 SF each. The second SCD, approved on July 1, 1999, reoriented the proposed driveway 18 to 20 feet to the east, and extended the parking turnaround a maximum of 26 feet to the south, to retain existing mature fruit

trees and to retain the existing barn for storage; the total number of parking spaces proposed (24) would not change.

Ultimately, only the approved 2,445 SF daycare and school facility and revised parking lot were constructed. Since the 873 SF synagogue and the 2,445 SF office/meeting room were never constructed, the existing 2,445 SF daycare and school facility has been modified to serve all of these functions (daycare, school, office, meeting room, and synagogue worship space). Occasionally, a temporary tent is placed on the concrete slab that was constructed for the synagogue.

Concurrent with the Santa Barbara County Planning Commission's approval of the aforementioned Lot Line Adjustment and Conditional Use Permit, the Santa Barbara County Planning Department approved a permit (93-LUS-028) for a residential remodel (including the conversion of the attached garage) and addition. Specifically, the project would increase the size of the 2,002-SF residence by 2,142 SF (to 4,144 SF total) and provide replacement parking in a new, detached, 630-SF garage. As part of this project, the driveway serving the former, attached garage was proposed to be removed and a new driveway on the eastern side of the property was proposed to be constructed to serve the new, detached garage. While the former, attached garage has since been converted, the former driveway has not been removed and neither the new driveway nor detached garage have been constructed.

On June 4, 1996, the Santa Barbara County Planning Department approved a permit for a 352 SF spa with bathroom (tub and shower) mechanical room and entry area. A second permit (99-LUS-024) was issued for an identical spa on February 17, 1999. It is presumed the second permit was issued because the original permit expired. In any event, the spa was ultimately constructed.

## **8. APPROVAL REQUIRED BY OTHER PUBLIC AGENCIES:**

Central Coast Regional Water Quality Control Board  
 Santa Barbara County Fire Department  
 Goleta Water District  
 Goleta Sanitary District

## **9. SITE INFORMATION:**

Site Information	
Existing General Plan Land Use Designation	Single-Family (SF)
Zoning Ordinance, Zone District	Design Residential (DR)-4.6
Site Size	143,996 SF (3.3 Acres)
Present Use and Development	Residence, Synagogue, Education, School/Daycare Center, and Ancillary Uses (Spa/Bath House, Barn/Storage Garage, Parking, Half Basketball Court, Children's Playground)

Site Information	
Surrounding Uses/Zoning	North: Single-family residential development / 8-R-1 South: Goleta Union School District Office & Montessori Center School of Santa Barbara / DR-4.6 East: Church / DR-4.6 West: Single-family residential development / DR-4.6
Access	Existing: Stow Canyon Road (two aprons) Proposed: Stow Canyon Road (one apron)
Utilities and Public Services	Water Supply: Goleta Water District Sewage: Goleta Sanitation District Power: Southern California Edison Natural Gas: Southern California Gas Cable: Cox Cable Telephone: Verizon Fire: Santa Barbara County Fire School Districts: Goleta Union Elementary and Santa Barbara High School District

## 10. ENVIRONMENTAL SETTING

The environmental setting of the project site is urban. The project site is located at 6045 Stow Canyon Road, approximately 370 feet west of North Fairview Avenue and 900 feet south of Cathedral Oaks Road. The project is located on the existing Chabad of Santa Barbara property in a residential area of the City of Goleta. The project site is 3.3 acres in size on Assessor Parcel Number (APN) 077-140-044. Las Vegas Creek, a highly seasonal, unlined drainage channel, runs along the western boundary and The Meadows condominium complex is located west of Las Vegas Creek. The Montessori Center School of Santa Barbara and the Goleta Union School District Office (formerly Fairview Elementary School) are located to the south of the project site; Goleta Valley Church is located immediately to the east of the project site; and single-family residences are located to the north of the project site on the opposite side of Stow Canyon Road. Access to the project site is provided from Stow Canyon Road.

## 11. CALIFORNIA NATIVE AMERICAN TRIBES

The City made a request to the Native American Heritage Commission (NAHC) on December 20, 2018 for the Sacred Lands File related to the project per Public Resources Code section 5097.96 and Native American Contacts list. The City received a response from the NAHC on December 27, 2018 that provided a Tribal Consultation List and also stated that the Sacred Lands File (SLF) check was negative.

On January 3, 2019, the City sent letters inviting consultation to the tribal representatives identified on the list provided by the NAHC as having a traditional and cultural association with the geographic area of the proposed project pursuant to Public Resources Code Section 21080.3.1. On February 8, 2019, Santa Ynez Band of Chumash Indians indicated that they did not want to formally consult but would like the City to impose the requirement of a "discovery condition." Such a condition requires that in the event resources are found, work halts or moves to another portion of the site until the find can be evaluated. Please refer to Section 15.E, *Cultural Resources*, for details on the Mitigation Measure.

**12. ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:**

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist and analysis on the following pages.

- ☐ Aesthetics
- ☐ Agriculture and Forestry Resources
- ☐ Air Quality
- ☐ Biological Resources
- ☒ Cultural Resources
- ☐ Energy
- ☐ Geology/Soils
- ☐ Greenhouse Gas Emissions
- ☐ Hazards and Hazardous Materials
- ☐ Hydrology/Water Quality
- ☐ Land Use/Planning
- ☐ Mineral Resources
- ☐ Noise
- ☐ Population/Housing
- ☐ Public Services
- ☐ Recreation
- ☐ Transportation
- ☒ Tribal Cultural Resources
- ☐ Utilities/Service Systems
- ☐ Wildfire
- ☐ Mandatory Findings of Significance

**13. DETERMINATION**

On the basis of this environmental checklist/initial study:

- ☐ I find that the proposed project COULD NOT have a significant effect on the environment and a NEGATIVE DECLARATION will be prepared.
- ☒ I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- ☐ I find that the proposed project MAY have a significant effect on the environment and an ENVIRONMENTAL IMPACT REPORT is required.
- ☐ I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect (a) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (b) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.

- ☐ I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier environmental impact report or negative declaration/mitigated negative declaration pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier environmental impact report or negative declaration/mitigated negative declaration document, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

MaryChang for Lisa Prasse  
Lisa Prasse, Current Planning Manager

11/26/19  
Date

**14. EVALUATION OF ENVIRONMENTAL IMPACTS:**

- (a) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
- (b) All answers must take into account the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- (c) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- (d) "Negative Declaration: Less Than Significant With Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from "Earlier Analysis," as described in (e) below, may be cross-referenced).
- (e) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration (Section 15063(c)(3)(D)).
- (f) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances).

- (g) Supporting Information Sources: A source list, including individuals contacted, should be attached and cited in the discussion.
- (h) Lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected. The explanation of each issue should identify:
  - 1) the significance criteria or threshold, if any, used to evaluate each question; and
  - 2) the mitigation measure identified, if any, to reduce the impact to a less than significant level.



**15. ISSUE AREAS:****A. AESTHETICS**

<b>Except as provided in Public Resources Code Section 21099, would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Have a substantial adverse effect on a scenic vista?			X		
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?			X		
c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?			X		
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?			X		

**i. Existing Setting**

The proposed project site is located within a developed neighborhood with a mix of attached and detached residences, religious institutions, and schools, as described below. The site is graded and developed with structures and ancillary improvements as outlined in the project description.

The project site has a gentle slope from the northeast corner to the southwest corner (approximately 6% maximum). Most of the property boundary is lined with hedges and mature, skyline trees; some of this vegetation is located on neighboring property. Las Vegas Creek, a highly seasonal, unlined drainage channel, runs along the western boundary and The Meadows condominium complex is located west of Las Vegas Creek. The other, adjacent uses include the Montessori Center School of Santa Barbara and the Goleta Union School District Office to the south; Goleta Valley Church to the east; and single-family residences and Goleta Valley Jr. High School and Santa Barbara Charter School north of the project site on the north side of Stow Canyon Road.

The surrounding area does not have any scenic corridors or scenic view points as referenced on Figure 6-1 of the General Plan/Coastal Land Use Plan (GP/CLUP) Visual and Historical Resources Element. The nearest Local Scenic Corridors are Fairview Avenue and Cathedral Oaks Road, respectively located approximately 390 feet east and 900 feet north of the nearest property boundary.

The project is required to comply with the City's *Outdoor Lighting Guidelines*, which have been adopted to achieve a high standard of quality and efficiency in lighting and obtaining "Dark Sky" standards Citywide. The Dark Sky standards are intended to reduce light glare from impacting views of the night sky. The City's *Outdoor Lighting Guidelines* and the *Architectural and Design Standards for Commercial Projects* require Design Review Board review of the proposed lighting to ensure that outdoor lighting used for project.

The City's Design Review Board (DRB) is required to review the project and grant approval. Aspects of the DRB review relevant to this project include building height, bulk and scale; colors and types of building materials and application; physical relation to the immediately affected surrounding area; site layout, orientation and location of buildings, and relationship with open areas; architectural style; on-site lighting; and location and type of landscaping. Ordinance No. 15-04 includes 19 findings related to the aspects described above that must be made by the DRB during Design Review to grant project approval. On December 11, 2018, the DRB performed Conceptual review of the project and generally provided positive comments and support.

ii. Thresholds of Significance

A significant impact would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist or the County of Santa Barbara's Environmental Thresholds and Guidelines Manual (published May 1992 and revised January 1995, October 2001, and October 2002), adopted by the City of Goleta on August 19, 2008 (hereinafter referred to as the *City of Goleta Environmental Thresholds and Guidelines Manual*). A discussion of the following thresholds occurs in the Project Specific Impacts analysis below. The *Environmental Thresholds and Guidelines Manual* has not been updated since it was adopted by the City and may not reflect current CEQA, General Plan, and other regulations enacted in the ensuing years.

**Threshold AES-1.** Does the project site have significant visual resources by virtue of surface waters, vegetation, elevation, slope or other natural or man-made features which are publicly visible? If so, does the project have the potential to degrade or significantly interfere with the public's enjoyment of the site's existing visual resources?

**Threshold AES-2.** Does the project have the potential to impact visual resources of the Coastal Zone or other visually important area (i.e., mountainous area, public park, urban fringe, or scenic travel corridor)? If so, does the project have the potential to conflict with the policies set forth in the Local Coastal Plan, the Comprehensive Plan or any applicable community plan to protect the identified views?

**Threshold AES-3.** Does the project have the potential to create a significantly adverse aesthetic impact through obstruction of public views, incompatibility with surrounding uses, structures, or intensity of development, removal of significant amounts of vegetation, loss of important open space, substantial alteration of natural character, lack of adequate landscaping, or extensive grading visible from public areas?

iii. Project Specific Impacts

**a-c, AES-1,-2,-3) Less Than Significant Impact.** The City's GP/CLUP Visual and Historical Resources Element Figure 6-1 does not identify any scenic corridors or scenic view points in the project site area. The project site is not visible from coastal and mountain areas, or the Fairview Avenue or Cathedral Oaks travel corridors due to surrounding development. Public views of the project site would generally be limited to those from Stow Canyon

Road, and any views of the project from Stow Canyon Road would largely be screened given existing site characteristics (e.g., topographical slope, existing vegetation) and the proposed project design (e.g., location of the synagogue behind the residence, proposed vegetation).

The proposed storage building will not obstruct scenic vistas as seen from locations in the project vicinity given its proposed height (approximately 16 feet) and existing site characteristics (e.g., existing development and grade). The proposed Synagogue building would have a minimal impact, if any, on views in the project vicinity given it would largely be screened by the existing site features, including the residence, the building currently used for synagogue and educational services, and existing and proposed vegetation (hedges and trees) that line the majority of the property's boundary (existing vegetation and buildings are located adjacent to Stow Canyon Road and on neighboring property).

The proposed project does not lie within, or affect any views from, a Scenic Highway as designated by the State of California or the City of Goleta. As such, the project would not result in any impacts on scenic resources within a Scenic Highway view shed. Removal of the barn/storage garage (believed to have been constructed by 1940 and is presumed to be over 80 years old [Bookspan 2007]) would result in a minor impact on the view of passing motorists and pedestrians on Stow Canyon Road, which is not a State Scenic Highway, given it is not prominently visible from the roadway and its lack of importance to the streetscape. Therefore, the project would result in less than significant impacts to scenic views and scenic corridors.

***d, AES-3) Less Than Significant Impact.*** The project will not create substantial light glare or result in a light related aesthetic incompatibility impact as discussed by checklist item d, and Threshold AES-3, given the characteristics of the lighting plan (minimal light fixtures, directed downward, dark sky compliant etc.). Further, the new improvements would be largely screened from view by the existing site conditions. As part of the design review for the project, the applicant is required to submit an outdoor lighting plan. As part of this process, the project will undergo Design Review Board review to ensure the project complies with the City's exterior lighting dark sky standards, established lighting intensity maximums, as well as shielding and light angle requirements detailed in the City's *Outdoor Lighting Guidelines*, Section VI, *Exterior Lighting*.

The outdoor lighting plans must include a site plan with the proposed locations of all proposed lighting fixtures and angles in relation to buildings and landscaping; photometric diagrams and data may also be required. Section VII of the Outdoor Lighting Guidelines details Parking Lot lighting requirements. Mandatory compliance with city lighting and design standards and completion of design review of the project will ensure the project's new light sources for nighttime illumination of parking areas and/or loading areas, or for security, associated with the new Synagogue and storage buildings will meet City standards. With implementation of design review, no further mitigation would be required to address the potential lighting and glare impacts of the project and impacts would be less than significant.

iv. Cumulative Impacts

Proposed development at the project site would not be prominently visible from viewpoints along Stow Canyon Road or other locations near the project site. Since construction of the original

project, the City of Goleta incorporated and adopted a General Plan with new aesthetics policies that are applicable to the proposed project site. The project is required to comply with the City's *Outdoor Lighting Guidelines* and the *Architectural and Design Standards for Commercial Projects* which require Design Review Board review of the proposed lighting plan and ensures the building lighting and design are compatible with the adjacent community. Given the purpose of the proposed lighting and with the implementation of a lighting plan that complies with City requirements, the project's lighting-related impacts would not be cumulatively considerable. Therefore, the project's cumulative aesthetic impacts would be less than significant as it would not substantially contribute to changes in the visual character of the area.

v. Required/Recommended Mitigation Measures

No mitigation measures are proposed or needed. However, the following condition of approval will be imposed on the project.

**Lighting Specifications.** The applicant must secure Design Review Board approval of all exterior lighting fixtures to be installed on the project site. The site lighting must be:

- a. controlled and directed away from the Stream Protection Area and its associated 100-foot buffer;
- b. low intensity;
- c. low glare design;
- d. hooded to direct light downward onto the subject parcel and prevent spill-over onto adjacent parcels; and
- e. otherwise meet dark sky requirements.

Exterior lighting fixtures must be kept to the minimum lighting level and intensity needed to ensure public safety. These lights must be dimmed after 11 PM to the maximum extent practical without compromising public safety as determined by the Planning and Environmental Review Director, or designee. Lighting fixtures must be appropriate for the architectural style of the structure and surrounding area. The final lighting plan must include identification of all types, sizes, and intensities of wall mounted building lights and landscape accent lighting and a photometric map must be provided. "Moonlighting" type fixtures that illuminate entire tree canopies should also be avoided.

The Applicant/Permittee shall secure DRB approval of the lighting plan prior to issuance of the Land Use Permit for the project.

The Planning and Environmental Review Director, or designee, must verify plan compliance before issuance of the Land Use Permit and site installation at time of Final Building Inspection Clearance.

**Vegetation Screening.** Vegetation and/or fencing/walls shall be installed such that on-site parking areas and structures are partially screened and in substantial conformity with the approved plans. Vegetation shall be maintained in substantial conformity with the approved Landscape Plans. Substantial conformity must be determined by the Planning and Environmental Review Director, or designee.

vi. Residual Impact

The project would result in a new source of light or glare that would create minimal impacts that would have a less than significant impact on light or glare with implementation of the existing standards.

**B. AGRICULTURE AND FOREST RESOURCES**

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?				X	
b. Conflict with existing zoning for agricultural use or a Williamson Act contract?				X	
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code Section 12220(g)), timberland (as defined by Public Resources Code Section 4526), or timberland zoned Timberland Production (as defined by Government Code Section 51104(g))?				X	
d. Result in the loss of forest land or conversion of forest land to non-forest use?				X	
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of farmland, to non-agricultural use or conversion of forest land to non-forest use?				X	

i. Existing Setting

The proposed project site is located within an urbanized area and consists of developed land with a residence, a building used for synagogue and educational services that was approved in 1992 (92-CP-018) and other ancillary uses/structures as described in the project description. No agricultural use, forest lands, or timberlands occur on the project site or in the immediate vicinity.

Historically, the proposed project site was used for cattle ranching starting in the mid-1800's when it was purchased by Colonel W.W. Hollister from Nicholas A. Den, an Irish physician who had obtained rights to the 15,000-acre parcel in 1842. Subsequent owners continually raised the elevation of the farmland, re-routed Las Vegas Creek within the project boundary between 1929 and 1938, and planted avocado and citrus orchards (Stone 2018). However, there is no evidence that the site was still used for cultivation in the 1970s and 1980s, and it is known that commercial vehicles were stored on the site as part of a backhoe business (operated without permits) during a portion of this time (Bookspan 2007).

Currently, the site includes ornamental fruit and avocado trees, some of which are likely remnants from the previous agricultural use of the site. The State of California Department of Conservation (CDC) Farmland Mapping and Monitoring Program (FMMP) designated the project site and surrounding areas as Urban and Built-Up Lands (CDC 2018). The nearest Unique Farmland and Prime Farmland are respectively located approximately 450 feet and 1,100 feet east of the property's eastern boundary, on the east side of Fairview Avenue.

ii. Thresholds of Significance

A significant impact to Agriculture and Forest Resources would occur if the proposed project resulted in any of the impacts noted in the above checklist. Additionally, according to the City of Goleta's *Environmental Thresholds and Guidelines Manual*, a project may pose a significant environmental effect on agricultural resources if it converts prime agricultural land to non-agricultural use or impairs the agricultural productivity of prime agricultural land.

iii. Project Specific Impacts

**a-e) No Impact.** The site is designated as "Urban Built Up land" and is not designated as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance as mapped by the California Department of Conservation. There are no agriculturally zoned properties or properties under a Williamson Act contract in the vicinity of the project site. The proposed project would not result in any environmental changes that would involve the conversion of any farmland to non-agricultural uses. Additionally, the site has not been used for agriculture since before the existing Conditional Use Permit for the synagogue was approved in 1992, approximately 27 years ago. Further, there are no lands that contain or are zoned as forest lands or timberlands on the project site or in its immediate vicinity. Additionally, the proposed project would not result in any other environmental changes that would involve the conversion of forest lands to non-forest uses. Therefore, the proposed project would have no impact on agricultural or forest resources in the area.

iv. Cumulative Impacts

The proposed project would not contribute to any cumulative impact on agriculture or forest resources within the City

v. Required/Recommended Mitigation Measures

Based on the above analysis, no mitigation measures are necessary.

vi. Residual Impact

No residual impacts (either project specific or cumulative) on agriculture or forest resources would occur as a result of project implementation.



**C. AIR QUALITY**

<b>Where available, the significance criteria established by the applicable air quality management district or air pollution control district may be relied upon to make the following determinations. Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Conflict with or obstruct implementation of the applicable air quality plan?			X		
b. Result in cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard.			X		
c. Expose sensitive receptors to substantial pollutant concentrations?			X		
d. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?			X		

This section incorporates the results of air quality modeling prepared using CalEEMod Version 2016.3.2 for construction and operations. The modeling results are included in this document as Appendix C.

i. Existing Setting*Meteorological Setting*

The project site is located on the coastal plain in the City of Goleta (City). The climate in and around the City of Goleta, as well as most of Southern California, is dominated by the strength and position of the semi-permanent high-pressure center over the Pacific Ocean near Hawaii. It creates cool summers, mild winters, and infrequent rainfall. It drives the cool daytime sea breeze, and it maintains a comfortable humidity range and ample sunshine after the frequent morning clouds dissipate. However, the same atmospheric processes that create the desirable living climate combine to restrict the ability of the atmosphere to disperse the air pollution generated by the population attracted in part by the desirable climate.

Temperatures in the Goleta area average 59 degrees annually. Daily and seasonal oscillations of mean temperature are small because of the moderating effects of the nearby oceanic thermal reservoir. In contrast to the steady temperature regime, rainfall is highly variable. Measurable precipitation occurs mainly from early November to mid-April, but total amounts are generally small. Goleta averages 18 inches of rain annually with January as the wettest month.

Based on typical wind patterns, locally generated air pollutant emissions are carried offshore at night, and toward inland Santa Barbara County by day. Dispersion of pollutants is restricted when the wind velocity for nighttime breezes is low. The lack of development in inland Santa Barbara County, however, causes few air quality problems during nocturnal air stagnation. Daytime ventilation is usually much more vigorous. Both summer and winter air quality in the project area is generally very good.

### *Air Pollutants*

The U.S. Environmental Protection Agency (U.S. EPA) and the California Air Resources Board (CARB) establish health-based ambient air quality standards to identify outdoor pollutant levels that are considered safe for the public - including those individuals most sensitive to the effects of air pollution, such as children and the elderly. U.S. EPA has set National Ambient Air Quality Standards (NAAQS) for six pollutants, including ozone (O<sub>3</sub>), nitrogen oxides (NO<sub>x</sub>), Carbon Monoxide (CO), Sulfur Oxides (SO<sub>x</sub>) and particulate matter (PM<sub>10</sub> and PM<sub>2.5</sub>). These are referred to as the “criteria” pollutants. CARB has set California Ambient Air Quality Standards (CAAQS) for the same six pollutants, as well as for four additional pollutants.

CARB also identifies other air pollutants as toxic air contaminants (TACs) - pollutants that may cause serious, long-term effects, such as cancer, even at low levels. Most air toxics have no known safe levels, and some may accumulate in the body from repeated exposures. CARB has identified about 200 pollutants as air toxics, and measures continue to be adopted to reduce emissions of air toxics. Both criteria pollutants and toxic air contaminants are measured statewide to assess the adequacy of programs for cleaning the air. CARB works with local air pollution control districts to reduce air pollution from all sources.

#### ii. Regulatory Framework

##### *Ambient Air Quality Standards (AAQS)*

Federal and state law regulates Ambient Air Quality Standards (AAQS) and emergency episode criteria for various pollutants. Generally, state regulations have stricter standards than those at the federal level. AAQS are set at concentrations that provide a sufficient margin of safety to protect public health and welfare. Federal standards are established by the US Environmental Protection Agency (EPA) and are termed the National Ambient Air Quality Standards (NAAQS). The State standards are established by the California Air Resources Board (CARB) and are called the California Ambient Air Quality Standards (CAAQS). The APCD is required to monitor air pollutant levels to assure that Federal and State air quality standards are being met.

The significance of a pollutant concentration is determined by comparing the concentration to an appropriate federal and/or state ambient air quality standard. The region generally has good air quality, as it attains or is considered in maintenance status for most ambient air quality standards as described below.

##### *Air Quality Planning*

State and federal laws require jurisdictions that do not meet clean air standards to develop plans and programs that will bring those areas into compliance. These plans typically contain emission reduction measures and attainment schedules to meet specified deadlines. If and when attainment is reached, the attainment plan becomes a “maintenance plan.”

In 2001, the CARB developed an attainment plan that was designed to meet both federal and state planning requirements. The federal attainment plan was combined with those from other statewide non-attainment areas to become the State Implementation Plan (SIP). The 2001 Clean Air Plan (CAP) was adopted as the County portion of the SIP, designed to meet and maintain clean air standards. A 2013 CAP was adopted by the SBAPCD in March 2015.

The 2016 Ozone Plan (2016 Plan) was developed in 2016. The 2016 Plan is the eighth triennial update to the initial state Air Quality Attainment Plan that was originally adopted by the District Board in 1991 (other updates were done in 1994, 1998, 2001, 2004, 2007, 2010, and 2013). Based on the region's nonattainment status for ozone, each of the Santa Barbara County plan updates have included an "every feasible measure" strategy to ensure continued progress toward attainment of the state ozone standards. The 2016 Plan addresses the state ozone standard only and does not address the federal ozone standard.

When the 2016 Plan was adopted, the District was designated as a nonattainment area for the state ozone standard. However, the District was aware that this designation might soon change to be nonattainment-transitional. A region is designated Nonattainment-transitional when the ozone standard has not been exceeded more than three times at any one location during the last year. The Board adoption included a commitment to review the 2016 Ozone Plan if the District's designation were to change to nonattainment-transitional and determine whether the control measures scheduled for adoption or implementation within the next three years are needed (SBCAPCD August 2017).

The change to a nonattainment-transitional designation means that, prior to implementing new control measures, the District must review the plan and determine whether the stationary source control measures scheduled for adoption or implementation within the next three years are needed to accomplish expeditious attainment of the state ozone standard. The District may modify the control measure schedule if it determines that modifications will not slow progress toward achieving or maintaining the state ozone standard. Available data at the SBCAPCD website, shows that the ozone standards have not been exceeded thus far in 2019 (SBCAPCD 2019).

Santa Barbara County is designated as a federal ozone attainment area for the 8-hour ozone National Ambient Air Quality Standard (the 1-hour federal standard was revoked for Santa Barbara County). "Attainment" means those areas of the country where air pollution levels are persistently below the national ambient air quality standards. A new California 8-hour ozone standard was implemented in May 2006, which the County has violated. At this time, the County is classified as "Nonattainment-transitional" for both the state 8-hour and 1-hour ozone standard.

The County also continues to violate the state standard for PM<sub>10</sub>, therefore Santa Barbara County is a non-attainment area for the State standards for PM<sub>10</sub>. The County is in attainment for the federal PM<sub>2.5</sub> standard and is designated "unclassified" for the State PM<sub>2.5</sub> standard, and is designated "attainment" or "unclassified" for other state standards and for all federal clean air standards. "Unclassified" means that there is currently no quantifiable data to measure ambient air quality standards in that area. Those jurisdictions that are designated both as "attainment" or "unclassified" are considered to be in attainment of ambient air quality standards even though there is currently no quantifiable data to measure its specific ambient air quality levels.

iii. Thresholds of Significance—Criteria Pollutants

A significant air quality impact could occur if the proposed project resulted in any of the impacts noted in the above checklist.

In addition, pursuant to the City's *Environmental Thresholds and Guidelines Manual*, a significant adverse air quality impact may occur when a project, individually or cumulatively, triggers either of the following as summarized below:

**Threshold AQ-1.** Interfere with progress toward the attainment of the ozone standard by releasing emissions which equal or exceed the established long-term quantitative thresholds for NO<sub>x</sub> (nitrogen oxides) and ROC (reactive organic compounds; same as reactive organic gases [ROG]). Thresholds are 25 pounds/day of ozone precursors (NO<sub>x</sub> and ROC, combined).

**Threshold AQ-2.** Adds sufficient carbon monoxide (CO) to existing background levels to create a “hot-spot” where the one-hour standard of 20 parts per million carbon monoxide is exceeded. This typically occurs when adding 800 peak hour trips to a severely congested intersection.

**Threshold AQ-3.** Equals or exceeds the state or federal ambient air quality standards for any criteria pollutant (as determined by modeling).

**Threshold AQ-4.** Results in toxic or hazardous pollutants in amounts which may increase cancer risks for the affected population.

**Threshold AQ-5.** Causes an odor nuisance problem impacting a considerable number of people.

Cumulative air quality impacts and consistency with the policies and measures in the City's General Plan and the Air Quality Attainment Plan (AQAP) should be determined for all projects (i.e., whether the project exceeds the AQAP standards).

The following significance thresholds have been established by the APCD (*Scope and Content of Air Quality Sections in Environmental Documents*, SBCAPCD June 2017). While the City of Goleta has not yet adopted any new threshold criteria, these APCD thresholds are considered appropriate for use as a guideline for the impact analysis.

#### *APCD Operational Impacts Thresholds*

Based on APCD Thresholds, a project would result in a significant impact, either individually or cumulatively, if it would:

- a) Emit 240 pounds per day or more of ROC and NO<sub>x</sub> from all sources;
- b) Emit 25 pounds per day or more of unmitigated ROC from any motor vehicle trips only;
- c) Emit 25 pounds per day or more of unmitigated NO<sub>x</sub> from any motor vehicle trips only;
- d) Emit 80 pounds per day or more of PM<sub>10</sub>;
- e) Cause or contribute to a violation of any California or National Ambient Air Quality standard (except ozone);
- f) Exceed the APCD health risk public notification thresholds adopted by the APCD Board (10 excess cancer cases in a million for cancer risk and a Hazard Index of more than 1.0 for non-cancer risk); or
- g) Be inconsistent with Federal or State air quality plans for Santa Barbara County.

The cumulative contribution of project emissions to regional levels should be compared with existing programs and plans, including the most recent Ozone Plan (SBCAPCD 2016).

- h) Due to the County's non-attainment-transitional status for ozone and the regional nature of ozone as a pollutant, if a project's emissions from traffic sources of either of the ozone

precursors (NO<sub>x</sub> or ROC), exceed the operational thresholds, then the project's cumulative impacts are considered significant.

- i) For projects that do not have significant ozone precursor emissions or localized pollutant impacts, if emissions have been taken into account in the 2016 Ozone Plan growth projections, regional cumulative impacts may be considered to be less than significant.

#### ***APCD Construction Impacts Thresholds***

Quantitative thresholds of significance are not currently in place for short-term emissions. However, CEQA requires that the short-term impacts such as exhaust emissions from construction equipment and fugitive dust generation during grading must be analyzed. The APCD recommends that construction-related NO<sub>x</sub>, ROC, PM-10, and PM-2.5 emissions, from diesel and gasoline powered equipment, paving, and other activities, be quantified.

- j) APCD uses 25 tons per year for NO<sub>x</sub> and ROC as a guideline for determining the significance of construction impacts.

Under SBAPCD Rule 202 D.16, if the combined emissions from all construction equipment used to construct a stationary source which requires an Authority to Construct permit, have the potential to exceed 25 tons of any pollutant, except carbon monoxide, in a 12-month period, the permittee shall provide offsets under the provisions of Rule 804 and shall demonstrate that no ambient air quality standard will be violated.

#### iv. Project Specific Impacts

- a) ***Less than Significant Impact.*** The criteria pollutant emission projections used to develop the SBCAPCD 2016 Ozone Plan are based on population, vehicle trends, and planned land use. As such, projects that propose development that is consistent with the growth anticipated by the City's General Plan would be consistent with the Clean Air 2016 Ozone Plan.

The 2016 Ozone Plan uses the years 2025 and 2035 to estimate future emission inventories. The proposed project would result in the construction of a 7,293 SF synagogue and 841 SF storage building. The proposed project would not substantially change the existing uses conducted at the project site. Due to the relatively small size of the project, existing use of the site for religious services, and limited potential to substantially increase employment opportunities or the population of the City, it is not anticipated that the project would result in an increase in the City's population that exceeds the forecasts used in the 2016 Ozone Plan. In addition, the project would be consistent with the project site's existing General Plan land use designation (Single Family) and zoning classification (DR-4.6). Therefore, the proposed project's emissions are accounted for in the 2016 Ozone Plan growth projections and the project would be consistent with the 2016 Ozone Plan.

#### ***b-d, AQ-1, AQ-2, AQ-3, AQ-4, AQ-5) Less than Significant Impact.***

##### ***Construction Impacts:***

Construction of the proposed project would result in a temporary addition of pollutants to the local airshed caused by three general categories: soil disturbance and subsequent

entrained dust emissions, equipment and vehicle exhaust emissions, and architectural coatings. Pollutant emissions associated with demolition and construction activity were quantified using CalEEMod (Version 2016.3.2) based on default project demolition and construction assumptions, except as noted on Page 1 of Appendix C (e.g., total disturbed area is 1.53 acres and the construction would be phased based on the applicant-provided construction schedule). Table AQ-1, below, shows the estimated maximum unmitigated annual construction emissions associated with the project for ROC, NO<sub>x</sub>, CO, SO<sub>2</sub>, PM<sub>10</sub>, and PM<sub>2.5</sub>.

<b>Table AQ-1</b> <b>Maximum Annual Construction Unmitigated Emissions</b> <b>Fugitive and Exhaust Sources</b> <b>(tons/year)</b>						
	ROC	NO <sub>x</sub>	CO	SO <sub>2</sub>	PM <sub>10</sub>	PM <sub>2.5</sub>
<b>Construction Emissions</b>	<b>0.1561</b>	<b>1.1019</b>	<b>1.0039</b>	<b>1.7700e-003</b>	<b>0.0778</b>	<b>0.0576</b>
Threshold	25	25	None	25	25	25
Potential Impact?	No	No	No	No	No	No
Source: CalEEMod v. 2016.3.2 model						

As previously mentioned, although the SBCAPCD does not currently have quantitative thresholds of significance in place for short-term or construction emissions, it uses 25 tons per year for ROC, NO<sub>x</sub>, SO<sub>2</sub>, as a guideline for determining the significance of construction impacts. For this analysis, the City has chosen to rely on the SBCAPCD's guideline (25 tons/year for ROC, NO<sub>x</sub>, SO<sub>2</sub>, PM<sub>10</sub>, and PM<sub>2.5</sub>) as the threshold to determine the significance of project-related construction emissions. As shown in Table AQ-1, the construction emissions do not exceed these guidance thresholds. In addition, the project site is developed and does not involve a significant amount of grading. Moreover, the SBCAPCD provided suggested conditions for the project on April 17, 2018; these suggested conditions were confirmed on November 29, 2018 based upon review of project changes proposed by the applicant on November 13, 2018.

Therefore, project-related construction emissions would be substantially below the APCD's short-term emission guideline of 25 tons per year for construction projects. As such, the project's construction emissions would be less than significant and reduced to the extent feasible with the implementation of standard APCD conditions that have been included as conditions of approval for the project.

#### **Operational Impacts:**

The unmitigated operational mobile, area, and energy source emissions for the project were calculated using the CalEEMod computer model (version 2016.3.2). The model assumes that operation of the project would begin in 2022 and the results are shown below in Table AQ-2. The operational emissions provided in Table AQ-2 are the additional emissions associated with proposed project and would be in addition to the existing (baseline) emissions. Mobile emissions identified in Table AQ-2 are derived from CalEEMod's default values based on the ITE Manual (9<sup>th</sup> edition [CAPCOA 2017]). As such, mobile emissions are over-estimated, given that many attendees at services and holiday events often do not drive but will walk to the site in keeping with religious practices and observances (Associated Transportation Engineers 2018) and because proposed to changes to ongoing activities are minor. Emissions resulting from the project would not

exceed the significance thresholds of 25 pounds per day for total ozone precursor emissions (ROC and NO<sub>x</sub> combined); 25 pounds per day of ROC and NO<sub>x</sub> for mobile emissions (individual); 240 pounds per day for total ozone precursor emissions (ROC and NO<sub>x</sub> combined); or 80 pounds per day for PM<sub>10</sub> emissions. Therefore, the project would result in a less than significant long-term air quality impact.

The proposed synagogue, storage building, and other project-related site improvements would not be a substantial long-term source of odors given the nature of the use. Therefore, the project would result in less than significant odor-related impacts.

<b>Table AQ-2</b>						
<b>Project Operations – Unmitigated Area, Energy, and Mobile Source Emissions</b>						
	<b>Emissions (lbs./day)</b>					
	<b>ROC</b>	<b>NO<sub>x</sub></b>	<b>CO</b>	<b>SO<sub>2</sub></b>	<b>PM<sub>10</sub></b>	<b>PM<sub>2.5</sub></b>
Area Sources	0.2722	4.0000e-005	4.3500e-003	0.0000	2.0000e-005	2.0000e-005
Energy Sources	5.6400e-003	0.0513	0.0431	3.1000e-004	3.9000e-003	3.9000e-003
Mobile Sources	0.5237	1.7021	4.4518	0.0113	1.0253	0.2823
<b>Total</b>	<b>0.8016</b>	<b>1.7534</b>	<b>4.4992</b>	<b>0.0116</b>	<b>1.0293</b>	<b>0.2863</b>
City Threshold	25 (ROC and NO <sub>x</sub> combined)		“Hotspot” created?	N/A	N/A	N/A
SBCAPCD Thresholds	25/240 <sup>a</sup>	25/240 <sup>a</sup>	N/A	N/A	80	N/A
Exceed Threshold?	No	No	No	N/A	No	N/A
a) 25 lbs per day for mobile emissions only and 240 lbs per day for all project sources (area, energy, and mobile) Totals may vary due to rounding. Source: CalEEMod v.2016.3.2 Model						

Pursuant to checklist items b, c, and d, and Thresholds AQ-1 through AQ-5 (inclusive), project operation would not exceed CEQA thresholds, SBCAPCD operational thresholds of significance, and City of Goleta Thresholds. The project, therefore, would have a less than significant impact due to the project operational mobile and area source emissions.

v. Cumulative Impacts

The significance thresholds used for air quality analysis on a project level (25 lbs per day of NO<sub>x</sub> or ROC from transportation sources only) are also intended to address cumulative air quality impacts. The project's operational emissions as outlined in Table AQ-2 would not exceed these thresholds; therefore, the project's contribution to cumulative air quality impacts are considered less than significant.

For projects that do not have significant ozone precursor emissions or localized pollutant impacts, if emissions have been taken into account in the most recent Ozone Plan growth projections, regional cumulative impacts may be considered to be insignificant. When a project's emissions exceed the thresholds and are clearly not accounted for in the most recent Ozone Plan growth

projections, then the project is considered to have significant cumulative impacts. As described in response "a" above, the proposed project's emissions are accounted for in the 2016 Ozone Plan growth projections and the project would be consistent with the Ozone Plan. Therefore, the project's contribution to regional cumulative air quality impacts is considered less than significant.

vi. Required/Recommended Mitigation Measures

No air quality impacts are identified; therefore, no mitigation is necessary.

vii. Residual Impact

The project would result in less than significant impacts, inclusive of residual air quality impacts.



**D. BIOLOGICAL RESOURCES**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?			X		
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?			X		
c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?			X		
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?			X		
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?			X		
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?				X	

i. Existing Setting

The project site is located within an urbanized area and consists of developed open space/landscaping and play areas (approximately 80.2% of the site) as well as impervious development, including buildings, parking, and walkways (approximately 19.8% of the site). Existing structures include a residence, a building used for synagogue and educational services

that was approved in 1992 (92-CP-018), and other ancillary uses/structures as described in the project description. The majority of the property boundary is lined with hedges and mature, skyline trees; some of this vegetation is located on neighboring property. The site also includes ornamental fruit trees, some of which are likely remnants from the previous agricultural use of the site that likely ended by the 1970s.

Pursuant to the City's adopted General Plan/Coastal Land Use Plan, Conservation Element Map (June 2016) (Figure 4-1) for Special-Status Species and Environmentally Sensitive Habitat Areas (ESHA), the closest identified ESHA is Las Vegas Creek (a highly seasonal, unlined drainage channel) and the associated riparian habitat (a mixture of ornamental and native species that have colonized the creek's eastern bank) located on and immediately west of the property's western boundary. The portion of Las Vegas Creek adjacent to the project site is channelized, approximately 18-20 feet wide, and has a relatively flat bed gradient. The banks have been graded to about 45-degrees and pipe-and-revetment runs along the toe of both banks. The eastern bank of the creek is a mixture of ornamental and native species that have colonized this area, including Monterey cypress (*Cupressus macrocarpa*), willow rhus (*Rhus lancea*), black cottonwood (*Populus balsamifera* subsp. *trichocarpa*), fan palm (*Washingtonia* sp.), waxleaf Texas privet (*Ligustrum japonicum*), California blackberry (*Rubus ursinus*), and coast live oak (*Quercus agrifolia*) saplings and seedlings. Eastward of this line of riparian vegetation is a mowed grass path, chain-link fence, and oleander (*Nerium oleander*) hedge (Hunt & Associates 2017).

GP/CLUP Policy CE 2.2 establishes a 100-foot stream protection area (SPA) on both sides of creeks, including Las Vegas Creek, measured from the top of the bank or the outer limit of wetlands and/or riparian vegetation, whichever is greater. The City may consider increasing or decreasing the width of the SPA upland buffer on a case-by-case basis at the time of environmental review. The City may allow portions of a SPA upland buffer to be less than 100 feet wide, but not less than 25 feet wide, based on a site specific assessment if (1) there is no feasible alternative siting for development that will avoid the SPA upland buffer; and (2) the project's impacts will not have significant adverse effects on streamside vegetation or the biotic quality of the stream.

GP/CLUP Policy CE 1.2.k designates "nesting and roosting sites and related habitat areas for various species of raptors" as ESHA. No special status species have been identified on the project site or project vicinity (GP/CLUP 2016), and it is unlikely that any of the 11 ornamental and fruit trees proposed for removal (refer to plan set Sheet L-1) would be used as a nesting or roosting site for raptors. However, it is possible that these 11 ornamental and fruit trees could be used by nesting migratory birds. Furthermore, given the number and variety of skyline trees on and immediately adjacent to the site, it is assumed that raptor nesting or roosting sites (ESHA) are also located on or immediately adjacent to the project site. The overall wildlife habitat values of the project area are relatively low due to the historic agricultural use of the site, and existing urban development on and in the vicinity of the site. It is expected that Las Vegas Creek supports habitat for resident and migratory wildlife that are common to the project area.

ii. Thresholds of Significance

A significant impact on Biological Resources would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist. In addition, the City of Goleta's Environmental Thresholds and Guidelines Manual defines the following thresholds of significance:

*1. Types of Impacts to Biological Resources*

Disturbances to habitats or species may be significant, based on substantial evidence in the record, if they substantially impact significant resources in the following ways:

- a. Substantially reduce or eliminate species diversity or abundance.
- b. Substantially reduce or eliminate quantity or quality of nesting areas.
- c. Substantially limit reproductive capacity through loss of individuals or habitat.
- d. Substantially fragment, eliminate, or otherwise disrupt foraging areas and/or access to food resources.
- e. Substantially limit or fragment range and movement (geographic distribution of animals and/or seed dispersal routes).
- f. Substantially interfere with natural processes, such as fire or flooding, upon which the habitat depends.

*2. Less Than Significant Impacts*

The *Environmental Thresholds and Guidelines Manual* provides examples of areas in the City of Goleta where impacts to habitat are presumed to be less than significant, including:

- a. Small acreages of non-native grassland if wildlife values are low.
- b. Individuals or stands of non-native trees if not used by important animal species such as raptors or monarch butterflies.
- c. Areas of historical disturbance such as intensive agriculture.
- d. Small pockets of habitats already significantly fragmented or isolated, and disturbed or degraded.
- e. Areas of primarily ruderal species resulting from pre-existing man-made disturbance.

iii. Project Specific Impacts

**a) *Less Than Significant Impact.*** Construction would result in the direct removal of approximately 26,000 SF (18% of the site) of a developed, open field and ornamental landscaping, including 11 ornamental and fruit trees (1 apple, 1 apricot, 3 avocado, 2 citrus, 1 pomegranate shrub, 2 myoporum, and 1 pepper) that are not used by important animal species and have a low value for wildlife. No specimen or native trees would be removed. Such habitat modification would have little to no direct, long-term impact on any special status species.

Further, the California Invasive Plant Council classifies the invasiveness of the pepper tree as “limited” and “moderate” for the myoporum tree. These trees are not skyline trees and are unlikely to be used by raptors for nesting or roosting, especially when considering the many skyline trees available for nesting or roosting within or adjacent to the project site. As such, the removal of the three existing, limited- or moderately-invasive trees would eliminate the potential for them to spread into the adjacent riparian ESHA and is therefore considered a small but beneficial long-term impact to biological resources.

Wildlife present near construction activities, however, would be temporarily exposed to auditory and visual disturbance from human presence and construction equipment, as well as vibration, dust, and noise, during construction activities. In response, mobile species such as birds and mammals may temporarily leave the site until construction activities cease.

GP/CLUP Policy CE 1.9 (h) requires that the timing of grading and construction activities be controlled to minimize potential disruption of wildlife during critical time periods such as nesting or breeding seasons, and GP/CLUP Policy CE 8.4 requires a buffer area for raptor species. Specifically, development shall be designed to provide a 100-foot buffer around active and historical nest sites for protected species of raptors when feasible. Furthermore, whenever feasible, no development activity (e.g., grading, construction) shall be allowed within a 300-foot radius of the nest site during the nesting and fledgling season. GP/CLUP Policies CE 1.2(k) and 8.1(e) designate raptor roosts as Environmentally Sensitive Habitat Areas, and CE Policy 8.2 requires that all development be located, designed, constructed, and managed to avoid disturbance of adverse impacts to special status species and their habitats, including raptors roosts. Additionally, all raptors are specifically protected under California Department of Fish and Wildlife Code and all migratory birds are protected by the Federal Migratory Bird Treaty Act; these laws include the protection of raptor and migratory bird nests, respectfully, during the active nesting season (Fish and Game Code, § 1 et seq.; 16 Unites States Code, § 703 et seq.).

There are no known roosts/nests on the property or adjacent to the property. However, the potential exists for hawks and migratory birds to use the existing trees on or adjacent to the property site. If there are nesting birds within 300 feet of site, project construction has the potential to temporarily disturb/impact nesting birds (e.g., on the skyline trees within and around the project site). Therefore, in an abundance of caution, a Condition of Approval (*Nesting Birds*) is proposed to avoid the potential impacts resulting from construction of the proposed project. Specifically, preconstruction surveys for special status breeding/nesting birds, breeding/nesting birds protected by the Migratory Bird Treaty Act, and/or raptor nests or roosts will be performed and buffer areas will be implemented if such activities are ~~an active nest is~~ detected. The *Nesting Birds* Condition of Approval was proposed by the City and agreed to by the applicant. Implementation of the *Nesting Birds* Condition of Approval will be verified by the City ~~through a Mitigation Monitoring and Reporting Program (MMRP)~~. Therefore, ~~with the implementation of the Nesting Birds Condition of Approval and the MMRP, the City has a mechanism to verify that any impacts to protected migrating and nesting birds would be reduced.~~

The project complies with GP/CLUP Policy CE 2.2 by providing a 100-foot SPA buffer between the proposed new development (including the bioretention basin, storage building, and patio) and the top of the bank or the outer limit of wetlands and/or riparian vegetation (whichever is greater). The synagogue would be located approximately 180 feet from the buffer. Therefore, project would not result in any direct impacts to Las Vegas Creek or its SPA buffer). Potential indirect impacts to Las Vegas Creek or its 100-foot SPA buffer would be minimal. The project would comply with short- and long-term water quality requirements, in part by including several stormwater control measures would be implemented to reduce the peak and volume of runoff while also providing treatment of runoff before it is discharged off-site; see Section 15.J, *Hydrology and Water Quality*, for additional details. Additionally, Condition of Approval *Lighting Specifications* (refer to Section 15.A.v) would minimize potential lighting impacts. Finally, the number of people on the project site that could result in indirect impacts would not be substantially increased.

**b-e) Less Than Significant Impact.** As stated above, (Section “a”), Las Vegas Creek is located adjacent to the project site and it is assumed that the creek serves as a wildlife migration corridor. The proposed improvements are located at least 100’ feet away from

the top of Las Vegas Creek bank in accordance with General Plan/Coastal Land Use Plan Policy CE 2.2.

Given the project's location (a minimum of 100 feet from riparian vegetation), previous development located on and adjacent to the project site, and the implementation of conditions of approval to minimize new lighting on the project site, the project would not significantly impede short- or long-term wildlife access to foraging habitat, breeding habitat, water resources, or other areas necessary for reproduction. Consistent with existing City standards, new exterior building, landscaping, and parking lot lighting would be pointed downward, dark-sky compliant, primarily support the safety of users of the site, and would have a minimal impact on riparian habitat located over 100 feet from any new lighting. Therefore, impacts to wildlife movement and nursery sites would be less than significant.

The construction and on-going use of the and site improvements for synagogue and educational services will not entail the removal, filling, hydrological interruption of any wetland, marsh, or vernal pool given the project's location and lack of wetlands in the vicinity as well as the existing condition of the site and the low-impact design of the proposed project. Therefore, there would be no impact to federally or state-protected wetlands.

- f) ***No Impact.*** While adjacent to Las Vegas Creek, the project site does not contain habitat elements protected under City of Goleta's GP/CLUP Conservation Element plans and policies and development as designed would not conflict with local policies protecting biological resources. The project site is not within the coverage area of any approved federal, state, or local Habitat Conservation Plan or Natural Community Conservation Plan as described in checklist item f. Therefore, implementation of the proposed project would not result in any impacts related to consistency with these types of plans. The proposed buildings and site improvements will not conflict with any local policies or adopted conservation plans regarding biological resources as there are none applicable to the site or the area in which the project is located. As such, no impact will occur.

iv. Cumulative Impacts

The project's potential impacts to potential raptor roosts and raptor or migratory bird nesting sites during construction would be less than significant and would be further reduced by implementation of the *Nesting Birds* and *Lighting Specifications* Conditions of Approval. Because construction would pose only a short-term impact to protected birds ~~potential raptor nesting sites~~ during the 16-20-month construction period, the project's short-term contributions to cumulative impacts would not be cumulatively considerable. No other component of the project would result in long-term impacts to biological resources that would be cumulatively considerable. Therefore, the project's contributions to cumulative impacts to biological resources would not be considerable or significant.

v. Required/Recommended Mitigation Measures

No mitigation measures are proposed or needed. However, the following condition of approval will be imposed on the project.

**Nesting Birds.** At the Permittee's expense, the Applicant/Permittee must retain a City-approved biologist to conduct a survey to determine if special status breeding/nesting birds, breeding/nesting birds protected by the Migratory Bird Treaty Act, and/or raptor nests or roosts exist on or adjacent to the project site within 300 feet. The survey must be conducted prior to commencement of any demolition, grading, and/or construction activities. The survey must establish the breeding and roosting status of any nesting protected birds found on the site or within 300' of the site and designate a 300-foot buffer from any nest/roost, if found. The survey must include recommendations to minimize impacts to nesting protected birds during construction, to including but not limited to, imposing setbacks, installing fence protection, and restricting the construction schedule. The survey must take into account expected increases and decreases in nesting protected birds over the construction period and must include a map showing known roosting and nesting sites of protected bird species.

Construction within the 300-foot buffer must be avoided during the nesting season (e.g. February 1<sup>st</sup> through July 31<sup>st</sup>, but is variable based on seasonal climatic conditions). In addition, construction must not occur until the City-approved biologist has notified the City that all young birds have successfully fledged, and the nests/roosts are no longer active.

The 300-foot buffer(s) must be shown on all grading and construction plans where applicable. The survey must be conducted no more than 14 days and no less than 7 days prior to commencement of any demolition, grading and/or construction activities. Survey conclusions must be reviewed and approved by the Planning and Environmental Review Director, or designee, prior to the issuance of Grading/Building permits.

The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Grading/ Building Permit.

**Stream Protection Area.** Grading, construction activities, and structural development shall occur outside of a 100-foot SPA/riparian buffer measured from the top-of-bank or edge of riparian vegetation, whichever is greater, of Las Vegas Creek. Under no circumstances shall any construction equipment or construction worker vehicles be allowed inside the buffer; mechanized equipment for the purpose of mowing the existing grass field is allowed.

The 100-foot SPA buffer, the location of construction fencing, and fencing materials shall be shown on the project plans submitted for approval of any LUP or the issuance of any building or grading permit for the project and approved by the PER Director. The fence and fencing material must be installed in accordance with the approved plans prior to commencement of grading/ground disturbing activities.

City staff shall review plans, confirm fence installation prior to the commencement of construction activities (including grading), and perform site inspections throughout the construction phase.

**Invasive Species.** Non-native, invasive plant species cannot be included in any erosion control seed mixes and/or landscaping plant palette. The California Invasive Plant Council maintains an Inventory database of non-native, invasive plants.

The Applicant/Permittee shall secure DRB approval of the planting plan prior to issuance of the Land Use Permit for the project. The prohibition of the use of non-native invasive plant species must be printed on all Landscape plans.

The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Land Use Permit and that the requirement has been satisfied at the time of Final Building Inspection Clearance.

vi. Residual Impact

With implementation of the mitigation measure above, residual project impacts on biological resources during construction would be less than significant because construction would not occur within 300 of bird nesting activities. Once construction is complete, no significant contribution to cumulative biological resource impacts will occur with day-to-day operations of the project as it would result in only a small expansion of ongoing synagogue and educational services.

**E. CULTURAL RESOURCES**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Cause a substantial adverse change in the significance of a historical resource pursuant to in §15064.5?		X			
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?		X			
c. Disturb any human remains, including those interred outside of dedicated cemeteries?		X			

This section incorporates the analysis, findings, and recommendations in the *Phase I Archaeological Investigation for 6045 Stow Canyon Road, APN 077-170-044, Goleta, California* (Stone 2018) as well as a supplemental memorandum (Stone 2019) in response to the City of Goleta's request for additional information. These documents are referenced herein as Appendix D and contain confidential information that is kept on file with the City of Goleta and may be reviewed with prior authorization by the City of Goleta Planning and Environmental Review Department in accordance with applicable law. The historic significance of the barn/storage garage to be demolished was assessed in the *Historic Resources Evaluation for Out-Building at 6045 Stow Canyon Road, City of Goleta, California, Assessor's Parcel Number 007-717-044* (Shelley Bookspan, January 26, 2007), which is included herein as Appendix E. Tribal Cultural Resources are also addressed in Section R below.

i. Existing Setting*Prehistoric Setting*

Evidence exists for the presence of humans in the Santa Barbara coastal area for thousands of years. While some researchers have proposed that the Santa Barbara Channel area may have been settled as early as 40,000 years ago, only limited evidence for occupation much earlier than 9,500 years has been discovered. Even so, human prehistory along the Santa Barbara channel area coast may extend back as much as 12,000 years. Beginning approximately 7,500 years ago, prehistoric human settlement in the local area apparently increased rapidly with a number of sites dating to approximately this time, and many more dating subsequent to it (General Plan Final EIR [GP FEIR]).

*Ethnographic and Historic Setting*

Historically, settlement in the vicinity of the project site was defined by three periods: the Mission Period (AD 1769 to 1830), the Rancho Period (AD 1830 to 1865), and the American Period (AD 1865 to 1915). The first European contact to the Santa Barbara coastal region was by Portuguese explorers in 1542, followed by the Spanish in 1602. At the time of this first European contact in 1542, the Goleta area was occupied by a Native American group speaking a distinct dialect of the Chumash Language (General Plan Final EIR [GP FEIR]). This group later became known as the



Barbareno Chumash. The Chumash were hunters and gatherers who lived in areas surrounding the much larger prehistoric Goleta Slough. At the time of Spanish contact, the prevalent Chumash population had at least 10 Chumash villages in the Goleta Area and immediate vicinity (GP FEIR).

As provided in the City's General Plan Final EIR (Section 3.5, Cultural Resources), the City contains prehistoric, ethnographic, historical, and paleontological resources. GP/CLUP Figure 6-2 (November 2009) shows areas that contain sensitive historic/cultural resources and identifies 46 historic resource locations.

A Phase 1 Archaeological Investigation was completed in June 2018 for the proposed project that included an archaeological site records and literature search as well as an intensive surface survey of the property (Stone 2018). The literature and records search identified (1) two previously recorded archaeological sites are within a 0.25-mile radius, (2) two previous investigations conducted within the project area (a ground surface survey and subsequent Extended Phase 1 backhoe testing program), and (3) thirteen other investigations conducted within a 0.25-mile radius of the project site. Past and present on-site surveys identified low densities of estuarine shellfish fragments and fire altered rock within the southeastern portions of the project site and three estuarine shellfish fragments within the western portion of the project site. The report concluded that:

“The presence of surface shellfish fragments is a function of placement of fill when Las Vegas Creek was realigned from within the site to outside its western boundary between 1929 and 1938, and/or slough fill soils that were imported to the property when existing avocado trees were planted. The shell fragments on the ground surface are therefore not associated with a prehistoric or historic archaeological site.”

Additionally, a supplemental memorandum (Stone 2019) concluded that “soils below 2 feet from the surface were formed prior to Native American occupation in this area” and that disturbance of soils below 2 feet from the surface “would not have the potential to disturb unknown buried archaeological resources.”

Existing structures include a residence in the northeastern portion of the property, a building used for synagogue and educational services that was approved in 1992 (92-CP-018), and other ancillary uses/structures as described in the project description. The small storage shed and the storage garage are proposed to be removed. Of these, only the storage garage has potential to be a historically significant. The storage garage appears to be in poor to fair condition (portions of the foundation are crumbling, window frames are broken, window panes are missing, some of the metal cladding and roofing is misshapen, limited water damage can be seen in the interior, and newer crossbeams have been added to provide support). Bookspan (2007) rated the potential for the structure to be historically significant as a 7 out of a possible 40 based on the City of Goleta's *Environmental Thresholds and Guidelines Manual*. The *Environmental Thresholds and Guidelines Manual* does not provide a threshold value to define the significance of a potential historic resource.

As mentioned in Section 11 above, the City of Goleta provided an opportunity for consultation with the designated Tribes as required by law. On February 8, 2019, the Santa Ynez Band of Chumash Indians responded to the request and asked that a “discovery clause” be made a part of the project requirements (email from F. Romero).

ii. Thresholds of Significance

A significant impact on cultural resources would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist. Additional thresholds are contained in the City's Environmental Thresholds and Guidelines Manual. The City's adopted thresholds indicate that a project would result in a significant impact on a cultural resource if it results in the physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of such a resource would be materially impaired.

iii. Project Specific Impacts

a) ***Less Than Significant With Mitigation Incorporated.*** Construction would result in the removal of a structure (the barn/storage garage) that is at least 80 years old and is associated with the prior agrarian activities that occurred on the site; and is somewhat associated with the Sexton family, which has historical importance in the community. The historical report prepared by Bookspan (2007) found that the barn is not an exceptional example and is in disrepair. Further, the report finds that the barn is not particularly historic given its simple utilitarian design, and its context and setting have been lost. What is notable about the barn is that it is an older structure in a modern neighborhood. The report concluded that removal of the barn would result in possible impact and recommends relocation or documentation as mitigations to reduce the impact to less than significant. The implementation of Mitigation Measures MM-CUL-1 and -2, *Historic Building Relocation* and *Historic Building Documentation*, are proposed to avoid the potential impacts resulting from construction of the proposed project. These mitigation measures are proposed by the City and agreed to by the applicant. Implementation of Mitigation Measures CUL-1 and -2 will be verified by the City through a Mitigation Monitoring and Reporting Program (MMRP). Therefore, with the implementation of Mitigation Measures CUL-1 and -2, any impacts to the historic barn would be reduced to less than significant. While the successful relocation of the building is preferable, relocation is not required to reduce the impact to less than significant given the documentation that would be achieved through implementation of MM-CUL-2.

b,c) ***Less Than Significant With Mitigation Incorporated.*** A Phase I archaeological survey was prepared for the proposed project (Stone 2018). This survey included an intensive inspection of the project site on December 27, 2017 in accordance with the City of Goleta Cultural Resource Guidelines requirements, as adopted by the *County of Santa Barbara Regulations Governing Archaeological and Historical Projects Undertaken in Conformance with the California Environmental Quality Act (CEQA) and Related Laws: Cultural Resource Guidelines* (revised January 1993). Earlier on-site investigations include a Phase 1 Archaeological Investigation (Santoro and Hazeltine 1992, as cited in Stone 2018) as well as Extended Phase 1 Testing (Santoro 1992, as cited in Stone 2018). As noted above, these surveys identified low densities of estuarine shellfish fragments and fire altered rock within the southeastern portions of the project site and estuarine three shellfish fragments within the western portion of the project site. However, the presence of these materials is a result of the import of fill during earlier agricultural use of the site and is not associated with a prehistoric or historic archaeological site occurring on this property.

The potential for unknown, intact, buried prehistoric deposits is unlikely, as the project site is heavily disturbed by previous agricultural uses and is substantially filled. Therefore, it is

improbable that intact prehistoric resources exist or are likely to exist within the project site and within the proposed impacted areas extending an estimated 8 feet below the existing project ground surface at the uphill side of the proposed synagogue building. However, given the proximity to Las Vegas Creek and that the imported fill contains shellfish fragments and fire-altered rock, there is limited potential for archaeological resources, including isolated human remains, to have been redistributed throughout the project site during previous grading activities, including within areas that the Phase 1 Study has determined to be no longer intact. Therefore, the project may have the potential to significantly impact an archaeological resource or disturb human remains. As indicated above, the Santa Ynez Band of Chumash Indians commented that the potential for impacts to significant resources would be low and requested that a mitigation measure regarding the discovery of resources be included. The potential for the project to impact significant cultural resources is considered to be unlikely, however, the project's potential impacts would be reduced to less than significant with the implementation of Mitigation Measure MM-CUL-3, Discovery of Cultural Resources. This mitigation measure is proposed by the City and agreed to by the applicant. Implementation of Mitigation Measure CUL-3 will be verified by the City through a Mitigation Monitoring and Reporting Program (MMRP). Therefore, with the implementation of Mitigation Measure CUL-3 and the MMRP, the City has a mechanism to verify that any potentially significant impacts to the historic barn would be reduced to less than significant.

#### iv. Cumulative Impacts

Construction of the proposed project would not impact any known archaeological sites. The project would have the potential to result in cumulative impacts to archaeological resources if it were to disturb previously undetected and intact resources, however, the potential for such an impact to occur at the project site is considered to be low. In the unlikely event that intact resources are encountered during the construction of the project, potentially significant impacts would be reduced to a less than significant level with the implementation of the proposed project-specific mitigation measure CUL-3. Therefore, the project's cumulative impacts to archaeological resources are not cumulatively considerable and not significant.

The project would result in the removal of a small (1,440 sq ft) barn/storage garage that is considered to be historical primarily because of its connection to Goleta's agrarian past. The historical agricultural context of the project area, however, no longer exists. The structure is in poor condition and does not exhibit other important historical characteristics such as a design of historical interest, association with a significant architectural style, it does not embody unique construction methods or technology, is not culturally important, and it is not likely to yield important information about the community (Bookspan, 2007). The removal of the barn/storage garage would result in the loss of a utilitarian structure associated with Goleta's past, however, proposed mitigation measures would reduce this impact to a less than significant level, and the removal of the structure would not facilitate or encourage the removal of other historical structures in the City.

Impacts to historic resources tend to be site-specific and are generally assessed on a case-by-case basis. However, if a project were to result in development features or changes to existing environmental conditions that are incompatible with historical resources that exist in the vicinity of the project site, a potentially significant cumulative impact to historical resources could result. There are no other historical resources located on or adjacent to the project site that could be directly or indirectly affected by the proposed project. Therefore, the project's cumulative impacts to historical resources are not cumulatively considerable and not significant.

v. Required/Recommended Mitigation Measures

Portions of the project may have the potential to affect intact cultural resources. Project Environmental Conditions of Approval/Mitigation Measures for these potential affects will be included in the project approval and have been agreed to by the applicant as follows:

**MM-CUL-1: Historic Building Relocation.** The Applicant/Permittee, at its sole expense, shall make a good faith effort to relocate the barn/storage building ~~not less than 90 days~~. Prior to the issuance of a demolition permit, the Applicant shall offer the building to be moved, at the expense of the recipient, to another site including a good-faith effort (as determined by the Planning and Environmental Review Director, or designee) to working with a Realtor to advertise the building. The order of preference for relocation is 1) in Goleta; 2) in the South Coast area of Santa Barbara County; 3) in Santa Barbara County; and 4) outside of the County. This offer will be made through an advertisement placed in a paper general circulation twice within a 60-day time period, and a direct offer to local preservation groups including the Goleta Valley Historical Society.

**Plan Requirements & Timing:** Prior to publishing the notice, the notice shall be submitted to the City for review and approval. Prior to the issuance of a demolition permit, the applicant must provide evidence of the good faith outreach effort (for a period of no less than 90 days) to relocate the building including seeking the services of a Realtor to assist in finding a receiver for the building to the satisfaction of the Planning and Environmental Review Director, or designee.

**Monitoring:** The Planning and Environmental Review Director, or designee, shall verify compliance before issuance of the demolition permit.

**MM-CUL 2: Historic Building Documentation.** The Permittee/Applicant shall have a City-approved architectural historian provide written and photographic documentation of the barn/storage garage's historical features and former setting using historic photographs, measured drawings, and archival quality photography. The City shall review and approve the documentation prior to issuance of demolition permit. One copy of the historical documentation shall be kept with the City of Goleta and copies shall be deposited with the Goleta Valley Historical Society and in the Special Collections Department of the Davison Library of the University of California, Santa Barbara.

**Plan Requirements & Timing:** The applicant shall secure approval of the architectural historian by the City of Goleta prior to the documentation phase. Further, the review of the written and photographic history of the site must be completed, approved, and submitted to the designated repositories prior to the issuance of a demolition permit.

**Monitoring:** The Planning and Environmental Review Director, or designee, shall verify compliance before issuance of the demolition permit.

**MM-CUL-3: Discovery of Cultural Resources.** If archaeological resources are encountered during grading, work must be stopped immediately or redirected until a City-approved archaeologist and local Chumash Native American consultant can evaluate the significance of the find pursuant to the Phase 2 investigation standards set forth in the City Archaeological Guidelines. The Phase 2 study must be funded by the Applicant/Permittee at his sole expense. If resources are found to be significant, they must be subject to a Phase 3 mitigation program

consistent with City Archaeological Guidelines. The Phase 3 mitigation program must be funded by the Applicant/Permittee.

**Timing:** If archaeological resources are encountered during grading, the identification of the City-qualified archaeologist and Chumash Native American consultant shall be approved by the City prior to additional grading in the vicinity of the find. The monitors must be onsite during all project excavation, grading or other soil disturbance required to conduct the Phase 2, and if necessary, Phase 3 investigations.

**Monitoring/Reporting Party(ies):** The Planning and Environmental Review Director, or designee, must verify compliance before grading/construction in the vicinity of the find may be resumed.

vi. Residual Impact

Implementation of the Mitigation Measures would reduce the proposed project's impacts to less than significant.

**F. ENERGY**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?			X		
b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?			X		

i. Existing Setting

The project site is currently developed with residential, office, religious service, educational service, storage, parking, and landscaping uses. The current uses at the site were developed with approval of 92-CP-18 and subsequent permits (refer to Section 7, *Background Information*) by the County and prior to incorporation of the City. Baseline energy use at the site was estimated as part of the air quality modeling using CalEEMod Version 2016.3.2; refer to Appendix C.

Energy is provided by the Southern California Gas Company and by Southern California Edison (SCE). In addition to electrical distribution lines, several SCE substations are located within the city, including the Hollister Avenue and Glen Annie substations. The only electrical generating station in the city is GenOn's Energy Inc's "peaking station" on Las Armas between Hollister Avenue and the railroad tracks, which generates electrical power only during emergencies and peak-use periods.

The City's General Plan Conservation Element Implementation Action 5 (CE-IA-5), 2014 Climate Action Plan (CAP), and 2012 Energy Efficiency Action Plan identify measures to effectively meet State of California established greenhouse gas (GHG) reduction targets and energy efficiency goals, as articulated in Assembly Bill 32 (AB 32) and the California Public Utilities Commission's (CPUC) Long-Term Energy Efficiency Strategic Plan and implemented in the California Building Code Titles 20 and 24.

ii. Thresholds of Significance

Thresholds of significance for energy use have not been established in the City's Environmental Thresholds and Guidelines Manual. The project would be expected to have a significant impact on energy use if it demonstrably resulted in wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation or conflict or obstruct a plan for renewable energy or energy efficiency as discussed in the CEQA Guidelines Appendix G Checklist above.

iii. Project Specific Impacts

**a,b) Less Than Significant Impact.** The project is expected to utilize electricity, natural gas, and diesel and gasoline fuels as energy during the primary construction and operational phases. CalEEMod V. 2016.3.2 was used to estimate the baseline, construction, and annual operational energy use of all project phases and components (including transportation-related energy) during construction and operations to assess the annual air quality, greenhouse gas emissions, and energy used for the project. The results of the modeling are provided in Appendix C. Construction energy use would be temporary and less than significant. Based on the default assumptions and project-specific land use data in the CalEEMod model, operations would entail the use of 191,005 kBTU/year of natural gas and 85,856 kW-hours/year of electricity. Moreover, the new development shall be required by the California Building Code to be constructed to current energy standards and the project would be consistent with the CPUC's Energy Efficiency Strategic Plan (CPUC 2008, 2011). As such, the project's energy use would not be substantial, the project would use energy in an inefficient or wasteful manner, and impacts would be less than significant.

iv. Cumulative Impacts

The proposed project would incrementally increase energy use onsite. However, it would have a less than significant cumulative impact as it would meet required energy efficiency standards. The project would also be consistent with the CPUC Long-Term Energy Efficiency Strategic Plan as implemented in the California Building Code, resulting in a less than significant impact.

v. Required/Recommended Mitigation Measures

No energy efficiency impacts are identified; therefore, no mitigation is necessary.

vi. Residual Impact

The project would result in less than significant impacts, inclusive of residual energy impacts.

**G. GEOLOGY AND SOILS**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:			X		
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.			X		
ii. Strong seismic ground shaking?			X		
iii. Seismic-related ground failure, including liquefaction?			X		
iv. Landslides?			X		
b. Result in substantial soil erosion or the loss of topsoil?			X		
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?			X		
d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?			X		
e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?				X	
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?			X		

i. Existing Setting

The underlying geologic structure of the proposed project site is predominately Recent Quaternary Age Younger Alluvium, although Pleistocene Age Older Alluvium occupies a small portion of the northeastern corner of the property (GP/CLUP FEIR Figure 3.6-1, September 2006). Soils within the northeastern two-thirds of the Project site are characterized Milpitas Positas fine sandy loam (Stone 2018). They are found on alluvial fans and low terraces. The representative



profile is an A Horizon of brown to dark brown fine sandy loam to 25 inches. The B Horizon is yellow brown to brown clay and clay loams to 54 inches. The C Horizon is alluvial gravelly sandy loam. The balance of the soils along the western, southwestern, and southeastern project areas is Goleta loam, associated with broad flood plains. The surface A Horizon is a dark grayish brown fine sandy loam that extends to 20 inches. The subsequent C Horizon is brown fine sandy loam that extends to 72 inches below surface, including loamy sand, sandy loam, loam, and clay loam (Stone 2018). The project site has a gentle slope from the northeast corner to the southwest corner (approximately 6% maximum). Over the entire site there is an elevation range of approximately 15 feet (see Appendix B, Civil Sheet 2 of 4).

The project site is located in a seismically active region of Southern California that has experienced ground motion in response to earthquakes in the past. All of the City of Goleta is located within Seismic Zone D as designated by the California Uniform Building Code. The potential for liquefaction on the project site has a moderate problem rating as identified in Santa Barbara County Comprehensive Plan (SBCCP) Seismic Safety and Safety Element Liquefaction Map (2015).

No known unique paleontological resource or site has been identified onsite. Additionally, the project site does not contain any unique geologic features.

ii. Thresholds of Significance

A significant impact on geology/soils would occur if the proposed project resulted in any of the impacts noted in the above checklist. The City's *Environmental Thresholds and Guidelines Manual* stipulates that a proposed project would result in a potentially significant impact on geological processes if:

**Threshold GEO-1.** the project, and/or implementation of required mitigation measures, could result in increased erosion, landslides, soil creep, mudslides, and/or unstable slopes.

In addition, impacts related to geology have the potential to be significant if the project involves any of the following characteristics:

**Threshold GEO-2.** The project site or any part of the project is located on land having substantial geologic constraints, as determined by the City of Goleta. Areas constrained by geology include parcels located near active or potentially active faults and property underlain by rock types associated with compressible/collapsible soils or susceptible to landslides or severe erosion.

**Threshold GEO-3.** The project results in potentially hazardous geologic conditions such as the construction of cut slopes exceeding a grade of 1.5 horizontal to 1 vertical.

**Threshold GEO-4.** The project proposes construction of a cut slope over 15-feet in height as measured from the lowest finished grade.

**Threshold GEO-5.** The project is located on slopes exceeding 20% grade.

iii. Project Specific Impacts

**a,c, GEO-1, GEO-2) Less Than Significant Impact.** There are no Alquist-Priolo mapped earthquake faults or zones identified on the project site or in the immediate project area. Pursuant to checklist items a and c, and Threshold GEO-2, the closest faults include a segment of the Carneros Fault approximately 1,400 feet north of the site, an unnamed fault approximately 0.6 mile north of the site, the More Ranch Fault approximately 1.8 miles south of the project site, the Glen Annie Fault located approximately 0.7 mile west of the project site, and the Pedro Fault approximately 0.9 miles northeast of the project site (USGS 2009).

Strong ground shaking during seismic activity is a potential hazard common to the entire City and most of California. Project construction would be subject to compliance with the seismic safety standards of the California Building Code (CBC), which is adopted and incorporated into the Goleta Municipal Code. The CBC includes excavation and re-compaction measures to ensure structural stability in the event of a seismic event.

The topography of the inland site and surrounding developed parcels is gently sloped and the site is not mapped in an area of moderate or high landslide potential, as cited by checklist item c and Thresholds GEO-1 and GEO-2 above (GP/CLUP Figure 5-1, Geologic Hazards Map dated Nov. 2009). The absence of mountains or cliffs adjacent to the project site results in a less than significant landslide hazard.

The potential for liquefaction on the project site has a moderate problem rating as identified in Santa Barbara County Comprehensive Plan (SBCCP) Seismic Safety and Safety Element Liquefaction Map (2015). As part of the City's standard conditions of approval, the applicant is required to submit a soils and geotechnical report to the City that details compliance with City standards for grading and construction of the new synagogue and storage building. Conditions of approval are memorialized in the resolutions of approval for a project and are binding on the project. The soils and geotechnical report are required to be prepared by a licensed certified geotechnical engineer and reviewed by the City Building and Safety Department to minimize risks associated with soil stability prior to project approval and construction. Compliance with City standards for preparation of soils and geotechnical reports will ensure the report includes the appropriate structural-design parameters for the synagogue and storage building, including soils compaction ratios and for construction of the foundation and building structural components to address potential hazards from liquefaction and/or seismic-related settlement during implementation of the project. Therefore, implementation of City standard conditions of approval for soils and geotechnical reports will ensure proper soils and geotechnical engineering design in accordance with the current City and California Building Code and that the potential impacts associated with liquefaction, seismic activity or unstable slopes and soils would be less than significant.

**b, GEO-1, GEO-3, GEO-4, GEO-5) Less Than Significant Impact.** The proposed project would be located on a developed site with gently sloped topography consisting of open fields, existing structures, ornamental landscaping, and asphalt parking. Grading/excavation to accomplish the project would be limited, with an estimated earthwork quantity that includes 660 cubic yards of cut and 575 cubic yards of fill for a net export of 85 cubic yards. All areas disturbed by project-related construction that are not covered by structures, impermeable surface or permeable parking areas would be

landscaped. Based on the existing topography of the site and the design of the project, the proposed development would not result in substantial soil erosion, result in cut slopes exceeding 1.5 horizontal to 1 vertical, or 15 feet in height, result in slopes exceeding 20% grade, or cause a loss of topsoil that would result in a potentially significant geologic impact. As such, the potential impacts considered under checklist item “b” and Thresholds GEO-1, GEO-3, GEO-4, and GEO-5 would be less than significant.

- d) ***Less Than Significant Impact.*** The Goleta loam and Milpitas-Positas fine sandy loam soils found on the site are typically not expansive in nature. In any event, all new construction is required to adhere to local and state mandated grading and construction requirements, including but not limited to the California Building Code and City ordinances and engineering standards. Additionally, the City GP/CLUP EIR Figure 3.6-4, Topography and Landslides, identifies the project site as having a low landslide potential (GP/CLUP EIR 2006). Structural engineering and foundation reports are required to be provided by a licensed certified geotechnical engineer and reviewed by the City Building and Safety Department to minimize risks associated with soil stability prior to project approval and construction. Therefore, through existing regulatory processes, standard conditions, and City policies, potential impacts related to unstable or expansive soils that might be located on the project site would be less than significant.
- e) ***No Impact.*** The project site contains existing connections to the Goleta Sanitary District sewer system which will continue to be used. Pursuant to above checklist item e above, septic systems and drywells are not used on the property and are not planned to be used as the site is connected to the sanitary sewer system. The synagogue will include restrooms and a new sewer line lateral would be connected to the existing Goleta Sanitary District sewer system installed to California Building Code and Goleta Sanitary District standards. Therefore, no impact associated with geologic hazards related to the use of alternative waste water would exist.
- f) ***Less Than Significant Impact.*** The project site is underlain by Recent Quaternary Age Younger Alluvium. As such, the potential for significant paleontological resources is low. The presence of surface shellfish fragments is a function of placement of fill when Las Vegas Creek was realigned from within the site to outside its western boundary between 1929 and 1938, and/or slough fill soils that were imported to the property when existing avocado trees were planted (Stone 2018). Moreover, such shellfish fragments are not potentially significant paleontological resources. Therefore, impacts to significant paleontological resources would be less than significant.

iv. Cumulative Impacts

Cumulative development in the City would expose new residents and property to geologic and soil-related hazards in the area. However, such impacts would be addressed on a project-by-project basis through preparation of required soils and geotechnical engineering studies and adherence to the recommendations therein, as well as adherence to existing City and state regulations including the California Building Code. Because the potential impacts associated with the proposed project would be less than significant and impacts from future projects would be addressed on a case-by-case basis, the project’s contribution to cumulative impacts would be less than significant.

v. Required/Recommended Mitigation Measures

No mitigation measures are proposed or needed. However, the following standard condition of approval will be imposed on the project.

**Geotechnical and Soils Engineering Report.** The owner/applicant shall submit a Geotechnical and Soils Engineering Report related to soil engineering associated with the demolition, grading, and construction of the new synagogue and storage buildings. The recommendation of the Geotechnical and Soils Engineering Report must be incorporated into the Project's grading and building plans. The Geotechnical and Soils Engineering Report must meet the City of Goleta standards for engineering documents and address potential for liquefaction and/or seismic-related settlement and identify appropriate structural-design parameters and soils compaction ratios to address potential hazards.

Grading and building plans must be submitted for review and approval by the Planning and Environmental Review Director, or designee, the Building Official, or designee, and the Public Works Director, or designee, before the City issues grading and building permits.

The Project soils engineer must observe all excavations before soil modification (including placement of compacted soil), gravel backfill, or rebar and concrete and report observations to the City. Building Inspectors and/or Public Works Inspectors will conduct field inspections as needed.

vi. Residual Impact

Based on the above analysis and implementation of standard conditions of approval, potentially significant project-specific impacts on Geology and Soils would be avoided. The project would not result in significant residual soils and geology impacts.

**H. GREENHOUSE GAS EMISSIONS**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?			X		
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?			X		

i. Existing Setting

As described in greater detail in Section 7, *Background*, and Section 10, *Environmental Setting*, the project site was previously developed for cattle ranching, agriculture, and a backhoe business before the existing synagogue, daycare, and school facility was permitted in 1992.

*Climate Change Background*

Parts of the Earth's atmosphere act as an insulating "blanket" for the planet. This "blanket" of various gases traps solar energy, which keeps the global average temperature in a range suitable for life. The collection of atmospheric gases that comprise this blanket are called "greenhouse gases" (GHGs) based on the idea that these gases trap heat like the glass walls of a greenhouse. These gases, mainly water vapor, carbon dioxide (CO<sub>2</sub>), methane (CH<sub>4</sub>), nitrous oxide (N<sub>2</sub>O), ozone (O<sub>3</sub>), and chlorofluorocarbons (CFCs), all act as effective global insulators as they do not absorb visible light from the sun but do absorb heat (e.g., infrared light) radiated from the earth's surface. The US Environmental Protection Agency (EPA) defines a GHG as any gas that absorbs infrared radiation in the atmosphere. California law defines GHG to include the following: carbon dioxide (CO<sub>2</sub>), methane (CH<sub>4</sub>), nitrous oxide (N<sub>2</sub>O), hydrofluorocarbons (HFCs), perfluorocarbons (PFCs), and sulfur hexafluoride (SF<sub>6</sub>) (Health and Safety Code, § 38505(g)). Most scientists agree that human activities, such as habitat conversion (including deforestation) and combustion of fossil fuels (e.g., for electricity production, driving internal combustion vehicles), have contributed to the elevated concentration of these gases in the atmosphere that is slowly raising global temperatures.

The effect each GHG has on climate change is measured as a combination of the volume of its emissions and its global warming potential (GWP); it is expressed as a function of how much warming would be caused by the same mass of CO<sub>2</sub>. Thus, GHG emissions are typically measured in terms of pounds or tons of CO<sub>2</sub> equivalent (CO<sub>2</sub>e) and are often expressed in metric tons of CO<sub>2</sub> equivalent (MT CO<sub>2</sub>e) or millions of metric tons of CO<sub>2</sub> equivalents (MMT CO<sub>2</sub>e).

Climate change could impact the natural environment in California by triggering, among other things:

- Rising sea levels along the California coastline;
- Extreme-heat conditions, such as heat waves and very high temperatures, which could last longer and become more frequent;

- Increase in heat-related human deaths, an increase in infectious diseases, and a higher risk of respiratory problems caused by deteriorating air quality;
- Reduced snow pack and stream flow in the Sierra Nevada mountains, affecting winter recreation and water supplies;
- Potential increase in the severity of winter storms, affecting peak stream flows and flooding;
- Increased risk of drought (due to excess heat, reduced rainfall, and consolidation of rain events), which increases the risk of fire (and subsequent flood events) and habitat conversion;
- Changes in growing season conditions that could affect California agriculture, causing variations in crop quality and yield; and
- Changes in distribution of plant and wildlife species due to changes in temperature, competition from colonizing species, changes in hydrologic cycles, changes in sea levels, and other climate-related effects.

Global climate change issues are addressed through the efforts of various federal, state, regional, and local government agencies as well as national and international scientific and governmental conventions and programs. These agencies work jointly and individually to understand and regulate the effects of greenhouse gas emissions and resulting climate change through legislation, regulations, planning, policy-making, education, and a variety of programs. Some of the significant agencies, conventions, and programs focused on global climate change are listed below, and list of climate change legislation in California is available online at <https://www.climatechange.ca.gov/state/legislation.html>.

- Federal U.S. Environmental Protection Agency
- California Air Resources Board
- California Executive Order S-3-05
- California Executive Order S-13-08
- California Global Warming Solutions Action of 2006 (AB 32)
- Senate Bill (SB) 97 (enacted in 2007), SB 375 (enacted in 2008), SB 350 (enacted in 2015), and SB 32 (enacted in 2016).
- State of California Climate Change Proposed Scoping Plan
- Santa Barbara County Air Pollution Control District (APCD)
- 2006 City of Goleta General Plan Conservation Element
- 2012 City of Goleta Energy Efficiency Action Plan
- 2014 City of Goleta Climate Action Plan
- City of Goleta Resolution 17-52 (Adoption of Renewable Energy Goals)
- City of Goleta Energy Efficiency Standards (reach code)

The City's General Plan Conservation Element Implementation Action 5 (CE-IA-5), 2014 Climate Action Plan (CAP), and 2012 Energy Efficiency Action Plan identify measures to effectively meet State of California established greenhouse gas (GHG) reduction targets and energy efficiency goals, as articulated in Assembly Bill 32 (AB 32) and the California Public Utilities Commission's (CPUC) Long-Term Energy Efficiency Strategic Plan and as implemented in the California Building Code Titles 20 and 24.

According to the CAP, energy consumption by the City's built environment will represent 43 percent community emissions in 2020. Implementation of measures reducing electricity usage and improving energy performance, therefore, are vital to the City's CAP. The CAP identifies 13

building energy measures (eight energy efficiency measures) with the goal of reducing GHG emissions through lower electricity and natural gas use. The measures include implementing the City's "reach code" (adopted November 2010) which required new residential and commercial buildings to exceed the then-existing California Title 24 standards by 15 percent, financing programs for both residential and commercial energy retrofits, urban forest management, programs for residential and commercial solar, and Community Choice Aggregation (CCA) to encourage use of renewable energy use and the resultant realization of a reduction in GHG. The "reach code" sunset in July 2014 when the more stringent Title 24 standards took effect.

Additionally, on December 5, 2017, the City of Goleta committed to moving the City's municipal facilities and community-wide electricity supply to 100% renewable power by 2030 (Resolution 17-52). More information about the City of Goleta's Sustainability and Climate Adaptation programs can be found online at <https://www.cityofgoleta.org/projects-programs/sustainability-climate-adaptation>.

ii. Thresholds of Significance

Consistent with recent case law, CEQA Guidelines section 15126.2(a) amendments clarify that an EIR shall focus analysis on the significant effects of a proposed project on the environment. The CEQA Guidelines section 15064.4 requires a lead agency to make a good-faith effort, based to the extent possible on scientific and factual data to describe, calculate, or estimate the amount of GHG emissions resulting from a project. They give discretion to the lead agency to determine whether to:

1. Quantify GHG emissions resulting from a project, and/or
2. Rely on a qualitative analysis or performance-based standards.

The State Natural Resources Agency adopted amendments to the CEQA Guidelines for GHG emissions that became effective on December 28, 2019. These CEQA Guidelines amendments provide regulatory guidance on the analysis of GHG emissions in CEQA documents.

The revisions to CEQA Guidelines section 15064.4(2)(b) clarify that in determining the significance of a project's greenhouse gas emissions, the lead agency should focus its analysis on the reasonably foreseeable incremental contribution of the project's emissions to the effects of climate change.

A project's incremental contribution may be cumulatively considerable even if it appears relatively small compared to statewide, national or global emissions. The agency's analysis should consider a timeframe that is appropriate for the project. The agency's analysis also must reasonably reflect evolving scientific knowledge and state regulatory schemes. In addition, section 15064.4(2) (b) and (c), in summary, state that a lead agency should consider the following factors, among others, when assessing the significance of impacts from GHG emissions on the environment:

1. The extent to which the project may increase or reduce GHG emissions as compared to the existing environmental setting;
2. Whether the project emissions exceed a threshold of significance that the lead agency determines applies to the project; and
3. The extent to which the project complies with regulations or requirements adopted to implement a statewide, regional, or local plan for the reduction or mitigation of GHG emissions.

In determining the significance of impacts, the lead agency may consider a project's consistency with the State's long-term climate goals or strategies, provided that substantial evidence supports the agency's analysis of how those goals or strategies address the project's incremental contribution to climate change and its conclusion that the project's incremental contribution is not cumulatively considerable.

A lead agency may use a model or methodology to estimate greenhouse gas emissions resulting from a project. The lead agency has discretion to select the model or methodology it considers most appropriate to enable decision makers to intelligently take into account the project's incremental contribution to climate change. The lead agency must support its selection of a model or methodology with substantial evidence. The lead agency should explain the limitations of the particular model or methodology selected for use.

CEQA Guidelines section 15064.7(b) call on Lead Agencies to establish significance thresholds for their respective jurisdictions. Lead agencies may also use thresholds on a case-by-case basis as provided in Section 15064(b)(2).

Currently, neither the State of California nor the City of Goleta has established CEQA significance thresholds for GHG emissions. Indeed, many regulatory agencies are sorting through suggested thresholds and/or making project-by-project analyses. This approach is consistent with that suggested by the Governor's Office of Planning and Research in its technical advisory entitled "CEQA and Climate Change: Addressing Climate Change Through the California Environmental Quality Act Review (Governor's Office of Planning and Research 2008):

*...In the absence of regulatory standards for GHG emissions or other specific data to clearly define what constitutes a 'significant project', individual lead agencies may undertake a project-by-project analysis, consistent with available guidance and current CEQA practice.*

In June 2010, the Bay Area Air Quality Management District (BAAQMD) became the first regulatory agency in the nation to approve guidelines that establish thresholds of significance for GHG emissions. Since adoption, the BAAQMD GHG thresholds have withstood.<sup>1</sup> These thresholds are summarized in Table GHG-1 below.

On June 10, 2010, the Santa Barbara County Planning & Development Department produced a memorandum "*Support for Use of Bay Area Air Quality Management District Greenhouse Gas Emissions Standards*,"<sup>2</sup> which states, "While Santa Barbara County land use patterns differ from those in the Bay Area as a whole, Santa Barbara County is similar to certain Bay Area counties (in particular, Sonoma, Solano, and Marin) in terms of population growth, land use patterns, General Plan/Coastal Land Use Plan policies, and average commute patterns and times. Because of these similarities, the methodology used by BAAQMD to develop its GHG emission

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<sup>1</sup> On December 17, 2015, the California Supreme Court reversed the Trial Court ruling on *California Building Industry Association v. Bay Area Air Quality Management District* (2015) 62 Cal.4<sup>th</sup> 369 and remanded the substantive question of whether the BAAQMD's 2010 Air Quality CEQA Guidelines were valid back to the Court of Appeals for a decision. The BAAQMD published a new version of the Guidelines dated May 2017, which includes revisions made to address the Supreme Court's opinion. The GHG thresholds remained unchanged from the previous version.

<sup>2</sup> Santa Barbara County Planning & Development Department, *Support for Use of Bay Area Air Quality Management District Greenhouse Gas Emissions Standards. Interim GHG Emissions – Evidentiary Support*, June 10, 2010.



significance thresholds, as well as the thresholds themselves, have applicability to Santa Barbara County and represent the best available interim standards for Santa Barbara County.” In accordance with CEQA Guidelines §§15064.4(b)(2), and 15064.7(c), the City has consistently relied upon Santa Barbara County’s “*Support for Use of Bay Area Air Quality Management District Greenhouse Gas Emissions Standards*,” as the expert recommended threshold for establishing greenhouse gas impacts of a project.

<b>Table GHG-1</b> <b>Bay Area Air Quality Management District</b> <b>GHG Thresholds of Significance</b>	
<b>GHG Emission Source Category</b>	<b>Operational Emissions</b>
Land Use Development Projects <sup>a</sup>	1,100 Metric Ton (MT) CO <sub>2</sub> e/yr. or 4.6 MT CO <sub>2</sub> e/SP/yr. <sup>b</sup>
Stationary Sources <sup>c</sup>	10,000 MT CO <sub>2</sub> e /yr.
Source: Santa Barbara County Planning & Development Department, <sup>a</sup> Land use development projects include residential, commercial, industrial, and public land uses and facilities. <sup>b</sup> SP = Service Population (residents + employees). <sup>c</sup> Stationary Sources include land uses that would accommodate processes and equipment that emit GHG emissions and would require an Air District permit to operate	

The City of Goleta is located in Santa Barbara County and shares meteorological attributes as well as similar land use patterns and policies. As such, thresholds deemed applicable in Santa Barbara County would also reasonably apply to projects within the City Goleta. Therefore, this analysis uses the BAAQMD/Santa Barbara County Interim Thresholds of Significance to determine the significance of GHG emissions related to this project, based on the 1,100 MT CO<sub>2</sub>e/year or 4.6 MT CO<sub>2</sub>e per service population per year threshold for commercial and residential land uses. There is no BAAQMD threshold of significance for construction emissions.

According to the applicable thresholds for this project, the project would result in a significant impact if it:

- A. Generates operational emissions in an amount more than 1,100 MT CO<sub>2</sub>e/yr., and/or results in significant construction or operational GHG emissions based on a qualitative analysis.
- B. Fails to employ reasonable and feasible means to minimize GHG emissions in a manner that is consistent with the goals and objectives of AB 32.

It is also noted that the use of the BAAQMD threshold does not imply that it is a threshold that the City has formally adopted or should adopt as a GHG emissions significance threshold.

### iii. Project Specific Impacts

**a,b) Less Than Significant Impact.** As discussed in Section F. *Energy*, above, the energy analysis for this project is based on an analysis of energy use for all project phases and components, including transportation-related energy, during construction and operation as modelled using the CalEEMod V. 2016.3.2. The project is expected to utilize electricity, natural gas, and diesel and gasoline fuels as energy during the primary construction and operational phases. CalEEMod V. 2016.3.2 estimates the baseline, construction, and

annual operational energy use of the project's components to assess the air quality and greenhouse gas emissions of the project. The results of the modeling are provided in Appendix C.

Given the global nature of climate change resulting from GHG emissions, GHG emission impacts are inherently cumulative in nature. Accordingly, the determination of whether a project's GHG emissions impacts are significant depends on whether those emissions would make a cumulatively considerable contribution to a significant cumulative impact. This is assessed in the Cumulative Impacts section below.

iv. Cumulative Impacts

The project's "unmitigated GHG emissions" have been calculated for the project (see below and Appendix C). These emissions include operation of the project and forecast trip generation, as well as the GHG emissions from project construction. The CalEEMod v.2016.3.2 computer model was used to calculate direct and indirect project-related emissions. Table GHG-2 presents the estimated carbon dioxide equivalents (CO<sub>2</sub>, N<sub>2</sub>O, and CH<sub>4</sub> emissions) of the project.

*Construction.* Project construction activities would generate approximately 279.5 MT CO<sub>2</sub>e. Construction GHG emissions are typically summed and amortized over the lifetime of the project (assumed to be 30 years), then added to the operational emissions. Construction GHG emissions have been amortized, and would result in 139.3 MT CO<sub>2</sub>e/yr.

*Area.* Based on the default assumptions and project-specific land use data in the CalEEMod model, annual emissions associated with architectural coatings, consumer products, and landscaping would be 0.00081 MT CO<sub>2</sub>e/yr.

*Energy Consumption.* Energy Consumption emissions were calculated using the CalEEMod model and project-specific land use data. Electricity would be provided to the project site via Southern California Edison, and natural gas would be provided by Southern California Gas. The project would result in 37.7 MT CO<sub>2</sub>e/yr due to energy consumption.

*Mobile Source.* The CalEEMod model relies upon project-specific land use data to calculate mobile source emissions. The proposed project would directly result in 66.9 MT CO<sub>2</sub>e/yr of mobile source-generated GHG emissions. This analysis is considered to be conservatively high, however, as many attendees at services and holiday events often do not drive but will walk to the site in keeping with religious practices and observances (Associated Transportation Engineers 2018) and because proposed changes to ongoing activities are minor.

*Solid Waste.* The project is anticipated to generate approximately 52.1 tons of tons of solid waste per year, based on CalEEMod's default assumptions and assuming no waste is recycled or composted, thereby resulting in 24.2 MT CO<sub>2</sub>e/yr. This estimate is considered conservatively high, however, as the analysis provided in Section 15.S, Utilities and Services, identifies 6.75 tons/year as a conservatively-high estimate of waste generated annually, and at least half of this waste would be diverted (recycled or composted).

*Water Demand.* The project's water supply would be groundwater and imported sources provided by the Goleta Water District. The estimated annual water required for the project, based on CalEEMod's default assumptions, is 2.25 acre-feet, and emissions from indirect energy impacts due to water use would be 1.2 MT CO<sub>2</sub>e/yr. This estimate is conservatively high, however, as the

Goleta Water District has determined that the new synagogue building (with proposed property development, activities [e.g., religious and educational services], and retrofits of existing structures) would require 0.95 acre-feet of water annually (Goleta Water District: July 11, 2019 email from Jim Heaton).

<b>Table GHG-2 Business as Usual/Unmitigated Greenhouse Gas Emissions<sup>1</sup></b>	
<b>Source</b>	<b>Total Metric Tons of CO<sub>2</sub>e</b>
Construction (amortized over 30 years)	139.3
Area	0.00081
Energy	37.7
Mobile Source <sup>2</sup>	66.9
Solid Waste <sup>2</sup>	24.2
Water Demand <sup>2</sup>	1.2
<b>Total Project Emissions<sup>2,3</sup></b>	<b>269.3 MT CO<sub>2</sub>e/yr</b>
<b>GHG Significance Threshold<sup>4</sup></b>	<b>1,100.00 MT CO<sub>2</sub>e/yr</b>
<b>GHG Significance Threshold Exceeded?</b>	<b>No</b>
Notes: 1. Emissions calculated using CalEEMod v.2016.3.2 computer model. 2. Emissions estimates are expected to be conservatively high for reasons provided in text. 3. Total may appear to be slightly off due to rounding. 4. If annual emissions of operational-related GHGs exceed these levels, the proposed project would result in a cumulatively considerable contribution of GHG emissions and a cumulatively significant impact to global climate change.	

**Total Project-Related Sources of Greenhouse Gases.** As shown in Table GHG-2, the total amount of project-related “business as usual/unmitigated” GHG emissions from all sources combined would total 269.3 MT CO<sub>2</sub>e/year. For reasons provided above, however, these emissions are expected to be conservatively high. In any case, the total project-related unmitigated operational GHG emissions would not exceed the 1,100 MT CO<sub>2</sub>e/year threshold utilized by the City of Goleta, resulting in a greenhouse gas emissions impact to global climate change that would be less than significant.

**Compliance with other regulations or requirements.** All of the new construction (i.e., the proposed synagogue and storage building) will be required to incorporate existing energy efficient fixtures and equipment required by the California Building Code. Additionally, the City CAP programs are available to the applicant to help reduce the cost of installing solar and energy efficient fixtures onsite. The project would also be required to be consistent with the CPUC Long-Term Energy Efficiency Strategic Plan as implemented in the California Building Code (CBC). All project construction components must comply with the CBC prior to issuance of building permits by the City. Therefore, the project will be consistent with, and result in a less than significant impact to, the local CAP and the CPUC Long-Term Energy Efficiency Strategic Plan.

v. Required/Recommended Mitigation Measures

No impacts are identified. Therefore, mitigation is not necessary.

vi. Residual Impact

No impacts are identified. Therefore, residual cumulatively considerable impacts to global climate change would not occur.

**I. HAZARDS AND HAZARDOUS MATERIALS**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?			X		
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?			X		
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?			X		
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?				X	
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?				X	
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?				X	
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?				X	

**i. Existing Setting**

The City contains various sources of hazardous wastes/materials, such as industrial facilities, laboratories, and gas stations. Predominant land use in the site vicinity is residential, school, and religious institution. The existing facilities on the project site include a residence, a building used for synagogue and educational services that was approved in 1992 (92-CP-018), a spa/bath house, a small storage shed, and a barn/storage garage that is proposed to be removed on the northern portion of the property.

A records search through the State of California's GeoTracker tool for sites contaminated or Leaking Underground Storage Tanks (LUST) sites within a 0.5-mile radius of the project was conducted. Thirteen (13) sites were located within the 0.5-mile radius of the project site, but none have an effect on the project and none are located within a 1,000-foot radius of the project site.

The project site lies approximately 1.0 mile north of the Santa Barbara Municipal Airport (SBMA), outside of the Clear Zone and Approach Zone for the SBMA (GP/CLUP Figure 5-3, November 2009). There are no other airports or airstrips within two miles of the project site. The nearest schools from the project site are the Montessori Center School of Santa Barbara (formerly Fairview School) located immediately to the south of the project site and the Goleta Valley Junior High School and Santa Barbara Charter School located northwest of the project site on the opposite side of Stow Canyon Road.

ii. Thresholds of Significance

A significant impact with regards to hazards and hazardous materials would be expected to occur if the project resulted in any of the impacts noted in the above checklist. In addition, the City's Thresholds Manual addresses public safety impacts resulting from the involuntary exposure to hazardous materials. These thresholds focus on the activities that include the installation or modification to facilities that handle hazardous materials, transportation of hazardous materials, or non-hazardous land uses in proximity to hazardous facilities. Since the project is not a hazardous materials facility, the City's risk-based thresholds are not applicable.

iii. Project Specific Impacts

**a,b) Less Than Significant Impact.** The proposed project would not involve the routine transport, use or disposal of hazardous substances, other than minor amounts typically used for maintenance (including landscaping) and cleaning products. Existing and proposed uses onsite will remain as residential, religious services, and educational services. There are adequate state regulations in place to protect public safety. At the local level, the County Fire Department and Health Department screens inventories and inspects sites permitted to use or store hazardous materials regularly. The SBCAPCD also regulates projects with possible toxic emissions.

Given the age of the existing barn/storage garage and trailer to be removed or demolished, there is the potential for hazardous materials that were previously common in building materials to be present. These materials may include asbestos, which could become airborne if disturbed and requires special handling procedures including the use of protective clothing and respirators during removal, transport, and disposal. Removal of asbestos containing materials is governed by Santa Barbara Air Pollution Control District regulations; review and approval by the City of Goleta Building and Safety Department are also required. Standard conditions of project approval require (1) testing for asbestos prior to issuing a demolition permit for the barn/storage garage and (2) proper disposal of the trailer at a licensed or certified wrecking yard if it is not legally transferred to a third party and removed from the property.

Therefore, based on the above analysis, the proposed project would not create a significant hazard to the public or environment through the routine transport, use, or disposal of hazardous materials, and impacts from the proposed project on the risk of

upset involving the release of hazardous materials into the environment would be less than significant.

- c) ***Less Than Significant Impact.*** The project site is located within 0.25 mile of three existing schools: the Montessori Center School of Santa Barbara, the Goleta Valley Junior High School, and the Santa Barbara Charter School.

Since the proposed project would not involve the routine transport, use or disposal of hazardous substances, other than minor amounts typically used for maintenance (including landscaping) and cleaning products, the greatest potential for impacts to these schools would be during demolition and construction activities. Given the age of the existing storage garage to be removed or demolished, there is the potential for asbestos to be present. This could be hazardous if not removed and disposed of properly. Satisfying the requirements of the standard conditions of project approval (discussed in response to CEQA items “a,b”, above) would ensure that the potential impacts to nearby schools associated with construction (including the demolition or removal of the storage garage and trailer) would be less than significant.

- d) ***Less Than Significant Impact.*** A hazardous waste site records search was completed in March 2019, using Geotracker, an online database of hazardous site records maintained by the California State Water Resources Control Board. No open or closed cases occur on the project site or within 1,000 feet of the project site. Additionally, considering the nature of the project proposal (construction of a new synagogue and a new storage building, with landscaping and parking redesign), the project would not create a significant hazard to the public or environment. Any future proposals to use potentially hazardous materials on the site would be subject to review and approval by the Santa Barbara County Fire Department and City of Goleta Building and Safety Department prior to project clearance and would undergo a separate review process on a case-by-case basis. Therefore, project impacts associated with hazardous materials sites would be less than significant.

- e) ***Less than Significant Impact.*** The project site lies approximately 1.0 mile north of the SBMA and is located outside of the Clear Zone and Approach Zone. Although the project site is located within two miles of the SBMA, the project would not result in a safety or excessive noise hazard for people residing or working in the project area. As such, impacts would be less than significant.

- f,g) ***No Impact.*** The project would not result in the construction of any new facilities or establishment of new uses that could impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan. The project site is located well outside of the City’s Wildland Fire Hazard Area (GP/CLUP Figure 5-2, June 2016); therefore, no impact from exposure to wildlife fires would occur. As such, impacts would be less than significant.

iv. Cumulative Impacts

With the implementation of the City’s standard conditions of approval regarding the abatement of potential asbestos hazards within the existing building, and the proper disposal of the trailer at a licensed or certified wrecking yard if it is not legally transferred to a third party and removed from the property, the project would not have any impacts related to hazardous materials. As such,

the proposed project, combined with other similar projects, would not result in any cumulatively considerable impacts related to hazardous materials.

v. Required Mitigation Measures

No mitigation measures are proposed or needed. However, the following conditions of approval will be imposed on the project due to the date that the existing building was constructed and use of potentially hazardous building materials commonly in use on those dates.

**Asbestos Abatement.** Before the City issues a demolition permit for the existing barn/storage garage (if the barn is not relocated to another site), the Applicant/Permittee must notify the Santa Barbara Air Pollution Control District and test for asbestos. If asbestos is found, then the Applicant/Permittee must abate and dispose of the materials in a manner consistent with the California Building Code, Santa Barbara County Air Pollution Control District requirements, and any other regulatory requirements.

Prior the issuance of the demolition permit, the Building Official or designee must receive the appropriate paperwork confirming the abatement. The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Land Use Permit.

**Trailer Disposal/Removal.** Before the City issues a demolition permit for the existing parking lot, the Applicant/Permittee must provide proof that the trailer has been either (1) properly recycled or disposed at a licensed or certified wrecking yard or disposal site (2) legally transferred to a third party and removed from the property.

Prior the issuance of the demolition permit, the Building Official or designee must receive the appropriate paperwork confirming the proper disposal or legal transfer and removal of the trailer. The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Land Use Permit.

vi. Residual Impact

With implementation of the conditions of approval, the project would have a less than significant residual impact related to potential hazards and hazardous materials. This standard condition provides the mechanism for verification and additional certainty that any asbestos in the existing building will be handled in a safe manner consistent with all state and local rules, as is required by the City for all demolition of buildings from this time period, and that the trailer will be properly disposed or legally transferred and removed.



**J. HYDROLOGY AND WATER QUALITY**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?			X		
b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?			X		
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:			X		
i. result in substantial erosion or siltation on- or off-site;			X		
ii. substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site;			X		
iii. create or contribute runoff water which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff; or			X		
iv. impede or redirect flood flows?			X		
d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?			X		
e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?			X		

The hydrology and water quality analysis in this section is based on the *Preliminary Stormwater Control Plan For Chabad Synagogue, 6045 Stow Canyon Rd.*, MAC Design Associates, March 29, 2019 (Preliminary Stormwater Control Plan) which is Appendix F to this document. The stormwater control plan provides the applicant-recommended Best Management Practices (BMP) Plan to address compliance with drainage and surface water quality requirements of the Santa Barbara County Stormwater Technical Guide for Low Impact Development (2<sup>nd</sup> Edition, dated February 3, 2017).

i. Existing Setting

The federal Clean Water Act and the California Water Code mandate controls on discharges from municipal separate storm sewer systems (MS4s). The California State Water Board issues National Pollutant Discharge Elimination System (NPDES) permits that require cities, towns, and counties to regulate activities which can result in pollutants entering their storm drains and/or surface and sub-surface drainage features. Municipalities implement comprehensive stormwater pollution-prevention programs. Municipal staff use Best Management Practices (BMPs) when maintaining their own streets, storm drains, and municipal buildings. In addition, Chapter 13.04 of the Goleta Municipal Code contains the City's storm water requirements that are applicable to the development of the site.

Approximately 19.8% of the project site is covered with impervious surfaces (refer to Table 1), including buildings, paved parking areas, walkways, and the half basketball court. Most of these impervious surfaces (all but the existing barn/storage garage and a portion of the residence) were constructed after the synagogue was approved in 1992. The total impermeable surface paving and concrete onsite is 19,277 SF, or 13.35% of the site. Permeable landscaping (including the archery range) onsite currently accounts for 110,298 SF, or 76.60% of the site and is comprised of ornamental trees, shrubs, grasses. The permeable play area accounts for 5,030 SF, or 3.49% of the site.

The project site's elevation ranges from approximately 45 feet in the southwestern portion of the site to 60 feet in the northeastern portion of the site (see Appendix B, Civil Sheet 2 of 4). Although stormwater runoff, as well as tailwater from landscape irrigation onsite, can flow off the property in three different directions (west, south, or southeast), depending on the specific location of origination, all water eventually flows to Las Vegas Creek before flowing to San Pedro Creek and ultimately to the confluence of San Jose Creek and Atascadero Creek located east of the Goleta Slough. Runoff from all roofs would discharge directly or indirectly to onsite pervious areas. Small portions of the project site (i.e., the entrance to the existing and proposed synagogue parking lot, as well as the westernmost corners of the property [north and south]) are located within the 100-year flood zone (FEMA 2012).

ii. Thresholds of Significance

A significant impact on Hydrology & Water Quality would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist. In addition, the City's *Environmental Thresholds & Guidelines* Manual assumes that a significant impact on hydrology and water resources would occur if a project would:

**Threshold HYD-1:** Result in a substantial alteration of existing drainage patterns.

**Threshold HYD-2:** Alter the course of a stream or river.

**Threshold HYD-3:** Increase the rate of surface runoff to the extent that flooding, including increased erosion or sedimentation, occurs.

**Threshold HYD-4:** Create or contribute to runoff volumes exceed existing or planned stormwater runoff facilities, or substantially degrade water quality.

iii. Project Specific Impacts

**a-c,e, HYD-1, HYD-2, HYD-3, HYD-4) Less than Significant Impact.** The proposed project will demolish or remove the existing small (53 SF) storage shed, a 1,440 SF

barn/storage garage, and 120 SF trailer; construct a new synagogue and storage building; and redesign the existing parking lot and landscaping. The existing areas of impervious surfaces on the project site will now be reconfigured to consist of the following, totaling approximately 42,002 SF (a net increase of 13,454 SF – refer to Table 1):

- existing residence (a single-family dwelling),
- existing educational services building,
- existing spa building,
- existing open-sided storage structure
- existing half basketball court
- new synagogue building,
- new storage building,
- new trash enclosure
- new patio & walkways, and
- new drive aisles & parking areas.

To offset additional storm water runoff from the new impervious areas listed above, several stormwater control measures will be implemented to reduce peak and volume of runoff and provide treatment of runoff before it is discharged off-site:

- vegetation swales,
- bioretention basins,
- minimization of impervious areas (partially through use of permeable pavers set on a gravel base),
- dispersal of runoff to pervious areas (all roof and hardscape areas will drain to landscaped areas before being discharged offsite), and
- large vegetated buffers (all proposed development will be set back a minimum of 100 feet from the riparian habitat along Las Vegas Creek).

Las Vegas Creek is located on the property's western boundary. Expected pollutants of concern for a development of this type, including parking lots, are suspended solids/sediments, nutrients, oil and grease/hydrocarbons (from motor vehicles), pesticides and herbicides from landscaped areas, and trash and debris from social activities and refuse areas.

The Santa Barbara County Stormwater Technical Guide for Low Impact Development (2<sup>nd</sup> Edition, dated February 3, 2017), identifies four tiers of Post Construction Requirements (PCRs) for projects. Since the Project proposes to replace more than 22,500 square feet of impervious surface, it must evaluate Tier 4 requirements, which also include Tiers 1 through 3, consistent with City Threshold HYD-1 and checklist item a.

To comply with the requisite PCRs, post construction runoff from impervious areas (including existing and proposed buildings, the proposed parking lot, and proposed hardscape) will be directed to landscaped areas, permeable pavers, or the proposed bioretention basins. The project will also include vegetated swales and large vegetated buffers (all proposed development will be set back a minimum of 100 feet from the riparian habitat along Las Vegas Creek). The standard Conditions of Approval include a requirement to develop a stormwater facility operations and maintenance agreement and stormwater control plan that will address the operation and maintenance of these features.

Preliminary calculations indicate that project implementation would reduce the amount of runoff water from the site due to the proposed stormwater control measures. As such, the preliminary drainage analysis for all storm events (2-, 5-, 10-, 25-, and 100-year) indicates the project will result in reduced flows leaving the site than currently occur during peak flow events (Appendix F, Section III.C).

Prior to occupancy, the applicant will be required to execute a stormwater facility operations and maintenance agreement and secure approval of a Storm Water Control Plan from the City, consistent with City Threshold HYD 3 and checklist item “e” above. In addition, temporary construction-related water quality impacts could result if associated pollutants enter Las Vegas Creek or the storm water system that ultimately drains to the confluence of San Jose Creek and Atascadero Creek located east of the Goleta Slough. However, the project will be required to comply with adopted standards that include protective requirements such as an Erosion and Sediment Control Plan (ESCP) or Stormwater Pollution Prevention Plan (SWPPP), as appropriate based on the amount of disturbed area. The stormwater quality measures shall be prepared and submitted in conformance with the City Municipal Code, consistent with City Threshold HYD-4 and City Threshold “c.” The project will also be subject to conditions of approval that require construction washing areas to be located outside of the Las Vegas Creek riparian setback and at least 100 feet from any storm drain, waterbody, or sensitive biological resources.

Pursuant to checklist item “b,” the Applicant ~~has~~ is not requesting to increase the previously-approved ~~also agreed to relinquish existing rights to use (under 92-CP-018) of up to 2.4 acre-feet of water per year from the onsite well to supplement irrigation water, and the project will not substantially change water use onsite (see Section 15.S, *Utilities and Services* for more information). Moreover, as mentioned above, preliminary calculations indicate that project implementation would reduce the amount of runoff water from the site due to the proposed stormwater control measures, thereby increasing groundwater available for municipal use. Under the proposed project, all water would be supplied by the Goleta Water District. Therefore, the project will not substantially decrease groundwater supplies or interfere substantially with groundwater recharge.~~

Implementation of standard conditions of approval will ensure that the project complies with federal and state water quality standards, waste discharge requirements, and protect surface and ground water quality. Therefore, with implementation of the standard conditions of approval regarding construction washing areas and storm water control plans, project impacts to surface and groundwater quality, erosion, runoff, and stormwater pollutants and the potential to impede or redirect flood control capacity described in checklist item “d” above would be less than significant.

***d.) Less than Significant.*** All proposed structures lie outside of the 100-year Flood Zone and the Tsunami Inundation Zone (FEMA 2012, 2017; GP/CLUP Figure 5-2, June 2016). Therefore, risk of release of pollutants due to inundation associated with a mapped flood hazard, or because of a tsunami, would be less than significant.

iv. Cumulative Impacts

As discussed above, implementation of the stormwater control measures (bioretention basins, vegetated swales, permeable pavers set on a gravel base) would result in reduced peak flows and for all storm events (2-, 5-, 10-, 25-, and 100-year) will not exceed pre-project flows.

Implementation of the conditions of project approval will ensure that the project would not contribute incremental water runoff or pollutant discharge that would result in having cumulative hydrology and water quality impacts in the receiving flood control system (including Las Vegas Creek) that drains to the confluence of San Jose Creek and Atascadero Creek located east of the Goleta Slough. In addition, similar short-term and post-construction requirements would be applied to cumulative development located in the City, which would reduce the potential for cumulative stormwater runoff and quality impacts. Therefore, the project's cumulative hydrology and water quality impacts are not cumulatively considerable and potential cumulative impacts would be less than significant.

v. Required/Recommended Mitigation Measures

No mitigation measures are proposed or needed. However, the following standard conditions of approval will be imposed.

**Storm Water Control Plan.** The Applicant/Permittee shall submit to, and receive approval from, the Public Works Director, or designee, of a Storm Water Control Plan/~~Erosion Sediment Control Plan~~ (SWCP/~~ESCP~~) to treat and control off-site discharge of stormwater ~~during and~~ following construction of the project. The SWCP/~~ESCP~~ shall be prepared in compliance with the Central Coast Regional Water Board's Post-Construction Stormwater Management Requirements for Development Projects in the Central Coast Region, Resolution No. R3-2013-0032, and shall use the *Stormwater Technical Guide for Low Impact Development: Compliance with Stormwater Post-Construction Requirements in Santa Barbara County*. The SWCP shall be prepared in clear correlation and continuity with the Project's Final Drainage Study/Report.

The SWCP/~~ESCP~~ must receive approval from the Public Works Director, or designee, prior to the issuance of the Land Use Permit. The Planning and Environmental Review Director, or designee, must verify compliance prior to issuance of the Land Use Permit. City Planning and Environmental Review as well as Public Works staff will verify compliance with the provisions of the SCWP periodically and respond to instances of non-compliance with the SWCP/~~ESCP~~ during and after project construction.

**Stormwater Facility Operations and Maintenance Agreement.** The Applicant shall enter into and record with the County a Stormwater Facility Maintenance Agreement with the City's Public Works Director, or designee. The City's Public Works Director, or designee, shall develop and provide to the applicant a draft Stormwater Facility ~~Operations and~~ Maintenance Agreement in a form approved by the City Attorney. The Stormwater Facility ~~Operations and~~ Maintenance Agreement, shall, require, in perpetuity, that project owners, and their successors in interest, to regularly inspect, maintain, and when necessary, repair or replace stormwater treatment, retention and detention Stormwater Control Measures and Best Management Practices that are incorporated into the project. The Stormwater Facility ~~Operations and~~ Maintenance Agreement shall include a legal description of the project's location, a vicinity map, and the project's approved Stormwater Operations and Maintenance Plan. All costs associated with the preparation and recordation of said Agreement shall be borne by the Applicant. Applicant shall also post a Bond in a form acceptable to the City's Public Works Director, or designee, and in an amount of 110% of the estimated costs of maintaining Stormwater Control Measures and Best Management Practices incorporated into the Project for an initial period of two (2) years.

The Stormwater Facility ~~Operations and~~ Maintenance Agreement must receive approval from the Public Works Director, or designee, and be recorded prior to the issuance of the Certificate of Occupancy.

**Washing and Fueling of Construction Equipment and Materials.** During construction, washing and fueling of construction equipment and materials (including concrete and paint) can occur only in areas where polluted water and materials can be contained for subsequent removal from the site on a regular basis. The washing and fueling areas shall be located at least 100 feet from any storm drain, waterbody or sensitive biological resources unless permitted by PER Director, or designee, due to site constraints. Areas designated for washing and/or fueling functions must be identified on all plans submitted for issuance of any grading and/or building permit(s).

Prior to the issuance of any grading or building permits, designated fueling and wash off area(s) must be specified on the all grading and building plans. The fueling and wash-off area(s) must be in place throughout all applicable phases of construction.

The Public Works Director, or designee, and the Building Official, or designee, must verify compliance before issuance of the Grading and Building Permits. Subsequently, the Public Works Director, or designee, and the Building Official, or designee, must conduct site inspections during construction to verify compliance.

vi. **Residual Impact**

The project would not result in a residual significant hydrology or water quality impact with implementation of standard conditions of approval in accordance with the applicable stormwater requirements and Goleta Municipal Code Section 13.04.

**K. LAND USE AND PLANNING**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Physically divide an established community?				X	
b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for purpose of avoiding or mitigating an environmental effect?			X		

i. Existing Setting

GP/CLUP Land Use Element Figure 2-1 designates the project site as Single-family (R-SF). The R-SF land use designation is intended to identify and protect appropriately located land areas for family living in low-density residential environments. Uses in the R-SF designation include single-family attached and detached dwellings, second (accessory) residential units, religious institutions, small-scale residential care facilities, small-scale day care centers, public and quasi-public uses, and home occupations.

The project site is zoned Design Residential (DR) pursuant to the Article III, Chapter 35, Goleta Municipal Code (Inland Zoning Ordinance) Zoning Map. The purpose of the DR zone is to provide standards for traditional multiple residences as well as allowing flexibility and encouraging innovation and diversity in the design of residential developments by allowing a wide range of densities and housing types while requiring the provisions of a substantial amount of open space within new residential developments.

Table LUP-1 provides the land use and zoning designations of the adjacent properties, all of which are developed consistent with the General Plan Land Use and Inland Zoning Ordinance designations.

No changes to the Land Use Designation or Zoning are proposed with the project.

**Table LUP-1**  
**Adjacent Land Use and Zoning Designations**

<b>Direction from Project Site</b>	<b>Land Use Designation</b>	<b>Zoning Designation</b>	<b>Existing Development</b>
North & Northeast	Residential – Single-family	R-1	Residential – Single-family
East	Residential – Single-family	DR	Church
South & Southeast	Public / Quasi-Public	DR	School/Administrative
West & Southwest	Single-family	DR	Residential – Single-family Planned Unit Development
Northwest	Public / Quasi-Public	DR	School

ii. Thresholds of Significance

A significant land use and planning impact would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist.

iii. Project Specific Impacts

a) **No Impact.** The proposed development would not result in the physical division of any established community or neighborhood. The proposal represents an infill project within the urban area of the City and existing parcel boundaries. The project site is surrounded by a mix of residential, religious, and educational uses. In addition, the project does not involve modifications to the existing circulation network. Therefore, there would be no impact related to dividing an established community.

b) **Less Than Significant Impact.** The proposed project seeks approval of a Development Plan (18-031-DP) for the existing and proposed site improvements as well as a Conditional Use Permit Revision (18-031-CUPRV) to the existing Conditional Use Permit (92-CP-018). Pursuant to GP/CLUP Land Use Element Table 2-1, Allowable Uses and Standards for Residential Use Categories (Land Use Table), religious institutions, small-scale day care centers, and one single-family detached dwelling per lot are allowed in the R-SF land use designation. Pursuant to Inland Zoning Ordinance Sections 35-315.4 (Minor Conditional Use Permits), 35-315.5 (Major Conditional Use Permits), and 35-222.4 (Permitted Uses), the proposed child care facility, church, and single-family dwelling are allowed, respectively, with a Minor Conditional Use Permit, with a Major Conditional Use Permit, and by right.

~~Although the existing Conditional Use Permit (92-CP-018) allows up to 2.4 acre-feet of water to supplement onsite irrigation every year, and the proposed project would continue to allow this authorization. The new synagogue building will be required to obtain all water from the Goleta Water District, consistent with General Plan Policy PF 4.1(I). The well would not be abandoned and closed, however, as an adjacent property has an easement for access to the well.~~

The project does not involve any General Plan amendment or Specific Plan amendment and would not conflict with any adopted land use plan. The project site is not located within the local coastal zone and does not require a rezone that would conflict with the City's zoning ordinance. All development would be located outside of the 100-foot streamside protection area required by GP/CLUP Policy CE 2.2. Land use regulations related to biological, cultural, and tribal cultural resources are discussed in their respective sections. Therefore, the project does not have the potential to adversely impact applicable regulations and policies and impacts would be less than significant.

iv. Cumulative Impacts

Due to the project's consistency with the applicable use standards and policies described above, it can be found that the proposed project was anticipated in the GP/CLUP build-out scenario and would therefore not pose any cumulative land use impacts.



v. Required/Recommended Mitigation Measures

Based on the above analysis, there are no potentially significant impacts; therefore, no mitigation measures are required.

vi. Residual Impact

Based on the above analysis, no residual impacts to Land Use and Planning would occur.

**L. MINERAL RESOURCES**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?				X	
b. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan?				X	

i. Existing Setting

No known mineral resources have been identified on the project site nor would the project result in the loss of locally important mineral resources.

ii. Thresholds of Significance

A significant impact on mineral resources would be expected to occur if the proposed project resulted in any of the impacts in the checklist above.

iii. Project Specific Impacts

**a,b) No Impact.** The proposed project would not result in the loss of mineral resources that are of value to the region or the state and would not otherwise interfere with or preclude access to mineral resources as none have been mapped within the city by the State of California Department of Conservation. Therefore, the project would result in no impacts to mineral resources.

iv. Cumulative Impacts

The project would have no impact on any cumulative loss on mineral resources or resource recovery sites.

v. Required/Recommended Mitigation Measures

Based on the above analysis, there are no potentially significant impacts; therefore, no mitigation measures are required.

vi. Residual Impact

Based on the above analysis, no residual impacts to Mineral Resources would occur.

**M. NOISE**

Would the project result in:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?			X		
b. Generation of excessive groundborne vibration or groundborne noise levels?			X		
c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public use airport, would the project expose people residing or working in the project area to excessive noise levels?				X	

i. Existing Setting

Since the project site lies outside the approach zone of the Santa Barbara Municipal Airport (SBMA), is well removed from the railroad, and is not located on a major thoroughfare, the primary sources of noise in the area are vehicular traffic on Stow Canyon Road and operations at the adjacent schools. As such, and as shown on GP/CLUP Noise Element Figures 9-1 through 9-4, noise levels at the project site are predicted not to exceed the 60 dB Community Noise Equivalent Level (CNEL) noise contour for the existing and future (2030) airport, railroad, or roadway noise.

Noise is defined as unwanted or objectionable sound. The measurement of sound takes into account three variables: 1) magnitude, 2) frequency, 3) duration, and 4) time of day.

Magnitude is the measure of a sound's "loudness" and is expressed in decibels (dB) on a logarithmic scale. Decibel levels diminish (attenuate) as the distance from the noise source increases. For instance, the attenuation rate for a point noise source is 6dB every time the distance from the source is doubled. For linear sources such as Highway 101 or the railroad tracks, the attenuation is 3 dB for each doubling of distance from the source.

The frequency of a sound relates to the number of times per second the sound vibrates. One vibration/second equals one hertz (Hz). Normal human hearing can detect sounds ranging from 20 Hz to 20,000 Hz.

Duration is a measure of the time to which the noise receptor is exposed to the noise. Because noise levels in any given location fluctuate during the day, it is necessary to quantify the level of variation to accurately describe the noise environment.

One of the best measures to describe the noise environment is the Community Noise Equivalent Level (CNEL). CNEL is a noise index that attempts to take into account differences in the intrusiveness of noise between daytime hours and nighttime hours. Specifically, CNEL penalizes noise generated during the evening and nighttime as follows:

Daytime—7 am to 7 pm	Weighting Factor = +0 dB
Evening—7 pm to 10 pm	Weighting Factor = +5 dB
Nighttime—10 pm to 7 am	Weighting Factor = +10 dB

The Noise Element in the GP/CLUP sets the noise and land use standards for the maximum noise exposure to certain land uses. For example, pursuant to Table 9-2 in the Noise Element, noise exposure levels such as 50-67.5 A-Weighted Level Decibel (dBA) are considered normal and acceptable for commercial related uses. Figures 9-1 and 9-3 display the existing and future (2030) roadway noise levels for the project site and both the existing and future noise levels are projected not to exceed 65 dBA, which meets noise and land use compatibility criteria in Table 9-2.

ii. Thresholds of Significance

A significant noise impact would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist. In addition, Section 12, *Noise Thresholds*, of the City of Goleta's *Environmental Thresholds and Guidelines Manual* provides the following thresholds to determine whether significant noise impacts would occur:

**Threshold NOI-1.** A development that would generate noise levels in excess of 65 dBA CNEL and could affect sensitive receptors would generally be presumed to have a significant impact.

**Threshold NOI-2.** Outdoor living areas of noise sensitive uses that are subject to noise levels in excess of 65 dBA CNEL would generally be presumed to be significantly impacted by ambient noise. A significant impact would also generally occur where interior noise levels cannot be reduced to 45 dBA CNEL or less.

**Threshold NOI-3.** A project would generally have a significant effect on the environment if it would increase substantially the ambient noise levels for noise sensitive receptors in adjoining areas. Per Threshold 1 above, this may generally be presumed to occur when ambient noise levels affecting sensitive receptors are increased to 65 dBA CNEL or more. However, a significant affect may also occur when ambient noise levels affecting sensitive receptors increase substantially but remain less than 65 dBA CNEL, as determined on a case-by-case level.

**Threshold NOI-4.** Noise from grading and construction activity proposed within 1,600 feet of sensitive receptors, including schools, residential development, commercial lodging facilities, hospitals or care facilities, would generally result in a potentially significant impact. According to the US EPA guidelines, the average construction noise is 95 dBA at a 50-foot distance from the source. A 6 dB drop occurs with a doubling of the distance from the source. Therefore, locations within 1,600 feet of the construction site would be affected by noise levels over 65 dBA. Construction within 1,600 feet of sensitive receptors on weekdays outside of the hours of 8:00AM to 5:00PM and on weekends would generally be presumed to have a significant effect. Noise attenuation barriers and muffling of grading equipment may also be required. Construction equipment generating noise levels above 95 dBA [at 50 feet] may require additional mitigation.

Thresholds 1 and 3 are generally applied to long-term, operational noise that would be generated by a proposed project; short-term noise is specifically addressed in Threshold 4. "Outdoor living areas" referenced in Threshold 2 typically refer to patios, decks, yards, and similar locations.

With regard to Threshold 3, the term "substantial increase" is not defined within the Thresholds Manual. The limits of perceptibility by ambient grade instrumentation (sound meters) or by humans in a laboratory environment is around 1.5 dB. Under ambient conditions, people generally do not perceive that noise has clearly changed until there is a 3 dB difference. A threshold of 3 dB is also commonly used to define "substantial increase." Therefore, for purposes of this analysis, an increase of +3 dBA CNEL in traffic noise would be a significant impact. Increases of +3.0 dB require a doubling of traffic volumes on already noise-impacted roadways. Projects usually do not, by themselves, cause traffic volumes to double. Offsite traffic noise impacts are, therefore, almost always cumulative in nature rather than individually significant.

iii. Project Specific Impacts

**a, NOI-1 through NOI-4)    *Less Than Significant.*** As noted above, the project site is not located within the 60 dB CNEL noise contour for the existing and future (2030) airport, railroad, or roadway noise (GP/CLUP Figures 9-1 through 9-4). The GP/CLUP Noise Element Table 9-2 Noise and Land Use Compatibility Criteria sets a normally acceptable threshold of 60 dBA CNEL, and a conditionally acceptable threshold of 65 dBA CNEL, for residential (both low density and multiple family), school, library, church, hospital, and nursing home uses. The project's existing and proposed uses will remain as residential, religious services, and educational services, and will primarily occur indoors. The proposed synagogue and storage buildings would not result in a substantial increase in existing noise conditions at the project site given the intended use. The existing activities (inclusive of large events) and the proposed project's activities operational impacts will not exceed established noise levels or result in a substantial permanent increase in ambient noise levels in the project vicinity and impacts would be less than significant.

Further, it is presumed that both ongoing and large events may utilize the proposed courtyard located south of the synagogue building. Use of this site, however, would be secondary to use of the synagogue and any noise generated at this location would be substantially reduced by both the distance to nearby sensitive receptors and the shielding that would be provided by existing and proposed buildings and vegetation. Furthermore, the frequency of large events would not change, most large events would occur indoors, and the activities would be subject to existing City noise requirements like any other organization/institutions in Goleta. As such, the proposed project would have a less than significant effect on the existing noise environment.

As described in the Transportation section below, the project would not add a substantial amount of traffic onto streets in the vicinity of the project site, including Fairview Avenue and Stow Canyon Road. Similarly, the project would not result in a substantial increase in the number of cars parking on-site, and parking areas would be more than 100 feet from adjacent uses. Moreover, parking lot-related noise would not be substantially different from existing conditions. As designed and based on the activity levels anticipated, the project would not result in change that would create significant traffic-related or parking lot-related noise impacts.

Consistent with Threshold NOI-4 above, construction noise associated with the proposed project would generally result in a potentially significant impact on sensitive receptors, including the adjacent residences, church, and schools (refer to Section 9, *Site Information*; Section 10, *Environmental Setting*; and Table LUP-1), within 1,600 feet of the construction site. The average noise associated with heavy equipment operation and construction activities is 95 dBA measured 50 feet from the source. The construction activities anticipated to generate the greatest noise include grading activities, beeping associated with backup/reverse signals on trucks, and nail guns during structure framing. With the implementation of conditions of approval implemented during construction as required by the City, however, short term construction impacts would be less than significant. These standard conditions are outlined below.

- b) *Less Than Significant Impact.*** The project would not result in generation of excessive groundborne vibration or groundborne noise levels during operation of the synagogue. There may be some increase in vibration and noise generated by construction; however, construction would be short-term, temporary, and cease upon project completion. Therefore, impacts would be less than significant.
- c) *No Impact.*** Although the project site lies within the area of influence of the Santa Barbara Municipal Airport as defined by the Santa Barbara County Airport Land Use Plan, it is outside of any airport noise contour. In addition, there are no private airports or airstrips in the vicinity of the project site. As such, no noise impacts from airport operations on the proposed project would occur.

iv. Cumulative Impacts

Because project related construction noise would pose only a short-term noise impact and there is no other project related noise impact as addressed above, the project contributions to cumulative noise impacts would be less than significant.

v. Required/Recommended Mitigation Measures

No mitigation measures are proposed or needed. However, the following standard conditions of approval will be imposed.

**Noise Attenuation – Construction Noise.** The following measures must be incorporated into grading and building plan specifications to reduce the impact of construction noise:

- a) All construction equipment, fixed or mobile, must be equipped with properly operating and maintained mufflers. Noise attenuation barriers and mufflers on grading equipment must be required for construction equipment generating noise levels above 95dB at 50 feet from the source;
- b) Construction noise reduction methods such as but not limited to shutting off idling equipment, installing acoustic barriers around significant sources of stationary construction noise sources, maximizing the distance between equipment and staging areas occupied residential areas, and use of electric air compressors and similar power tools (rather than diesel equipment) must be used when feasible;
- c) Whenever possible, construction activities must be scheduled to avoid operating several pieces of equipment simultaneously, which causes high noise levels.
- d) During construction, stationary construction equipment must be placed such that emitted noise is directed away from sensitive noise receivers;

- e) During construction, stockpiling and vehicle staging areas must be located as far as practicable from noise sensitive receptors;
- f) Earthmoving equipment operating on the construction site must be as far away from vibration-sensitive sites as possible; and
- g) Construction hours, allowable workdays, the telephone number of the job superintendent must be clearly posted at all construction entrances to enable surrounding owners and residents to contact the job superintendent directly. If the job superintendent receives a complaint, the superintendent must notify the Planning and Environmental Review Director, or designee, and investigate, take appropriate corrective action and report the action taken to the reporting party and the Planning and Environmental Review Director, or designee.

All signs must be in place before the start of site preparation and grading activities and maintained through to occupancy clearance or Final Building Inspection Clearance. Requirements a-f must be incorporated as text into all plans sets and must be incorporated graphically into all plan submitted for approval of any Land Use Permit, Grading Permit or Building Permit.

The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Land Use Permit, and before commencement of construction activities, and during construction.

**Construction Timing.** Construction activity and equipment maintenance is limited to the hours between 8 AM and 5 PM, Monday through Friday. Exceptions to these restrictions may be made for onsite work for good cause at the sole discretion of the Planning and Environmental Review Director. Exceptions to these restrictions for work in the City Right-of-Way may be made for good cause at the sole discretion of the Public Works Director or designee. Any subsequent amendment to the General Plan noise standard upon which these construction hours are based shall supersede the hours stated herein. No construction can occur on State holidays (e.g., Thanksgiving, Labor Day). Non-noise generating construction activities such as interior plumbing, electrical, drywall and painting (depending on compressor noise levels), are not subject to these restrictions.

At least one sign near each Project site entrance stating these restrictions must be posted on the site. Signs must be a minimum size of 24" x 48." Signs must be in place before the beginning of and throughout grading and construction activities. Violations may result in suspension of permits.

The Planning and Environmental Review Director, or designee, must monitor compliance with restrictions on construction hours and must promptly investigate and respond to all complaints.

**Construction Noise Compliant Line.** The Applicant/Permittee must provide a non-automated telephone number for local residents and employees to call to submit complaints associated with construction noise.

The telephone number must be posted on the site and must be readily visible from adjacent public areas. At least one sign near each Project site entrance along Stow Canyon Road with the phone number must be posted at all times. The Applicant/Permittee must inform the Planning and Environmental Review Director, or designee, of any complaints within one week of receipt of the complaint. Signs must be in place before beginning of and throughout construction activities. Violations may result in suspension of permits.

Building Inspectors and/or Code Compliance staff may periodically inspect and respond to complaints.

**Distancing of Vehicles and Equipment.** Noise and ground-borne vibration construction activities whose specific location on the Project site may be flexible (e.g. operation of compressors and generators, cement mixing, general truck idling) must be conducted as far as possible from the nearest noise and vibration sensitive land use.

The location of vehicles and equipment must be designated on building and grading plans. Equipment and vehicles must remain in the designated location throughout construction activities.

The Planning and Environmental Review Director, or designee, must periodically inspect the site to ensure compliance.

**Construction Notice.** The Applicant/Permittee shall provide all adjacent property owners and residents within 300 feet of the construction site with a construction activity schedule and construction routes 30 days in advance of construction activities in both English and Spanish. Any alterations or additions shall require PER Director, or designee, approval and 30-days prior notification to affected property owners and residents.

The Applicant/Permittee shall submit a copy of the schedule and mailing list to PER Director, or designee. Schedule and mailing list shall be submitted 30 days prior to initiation of any earth movement. PER compliance monitoring staff shall perform periodic site inspections to verify compliance with activity schedules.

vi. **Residual Impact**

With implementation of these mitigation measures, the short-term construction noise impacts would be less than significant and no significant long-term noise impacts will occur with project build out.



**N. POPULATION AND HOUSING**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?			X		
b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?				X	

i. Existing Setting

As of January 2019, California Department of Finance (DOF) estimates that City has a population of 32,759 people, has approximately 12,381 housing units, and has an average household size of 2.76 people per household (DOF 2019). Upon build out of the GP/CLUP (anticipated to occur by the year 2030), the City's population is expected to reach 38,100 (City of Goleta, GP/CLUP Final EIR, 2006: 3.8-15).

ii. Thresholds of Significance

A significant impact on population and housing would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist.

iii. Project Specific Impacts

- a) **Less Than Significant Impact.** The project includes the development of a 7,293 S.F. synagogue and 841 S.F. storage building. Considering the small scale and the uses (religious services, storage) of the proposed buildings, the project would not substantially induce unplanned population growth as no new homes or businesses are proposed and the new buildings would be used for existing religious services. Additionally, the project would not indirectly induce population as there will be no extension of roads or other infrastructure. As such, project impacts related to population growth would be less than significant.
- b) **No Impact.** The project involves the construction of a 7,293 S.F. synagogue and a 841 S.F. storage building on property that is already being used for religious purposes that would not displace existing people or housing. As such, there would be no impact associated with displacing substantial numbers of existing people or necessitating the construction of replacing housing elsewhere.

iv. Cumulative Impacts

As the proposed project would not have any population and housing impacts, the proposed project combined with other similar projects would not result in any cumulatively considerable population and housing impacts.

v. Required/Recommended Mitigation Measures

No mitigation measures are warranted.

vi. Residual Impact

The project would not result in any residual impacts to population and housing.

**O. PUBLIC SERVICES**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of these public services:					
Fire protection?			X		
Police protection?			X		
Schools?				X	
Parks?				X	
Other public facilities?				X	

i. Existing Setting*Fire Protection*

The project site is located within the urban area, in a central portion of the City of Goleta. Fire services would be provided by Santa Barbara County Fire Department (SBCFD) under contract to the City. The closest fire station to the project site is Station #14, located at 320 N. Los Carneros Road (approximately 1.2 miles away).

The GP/CLUP identifies three standards under Public Facilities Element Policy PF 3.1 with respect to the provision of fire protection services, which are derived from guidelines by the National Fire Protection Association (NFPA) and the Santa Barbara County Fire Protection District. These standards include:

1. A firefighter-to-population ratio of one firefighter on duty 24 hours a day for every 2,000 persons is considered "ideal," although a countywide ratio of one firefighter per 4,000 persons is the absolute maximum standard;
2. A ratio of one engine company per 12,000 persons, assuming three firefighters per company (or 16,000 persons assuming four firefighters per company), represents the maximum population that should be served by a three-person crew; and
3. A five-minute response time in urban areas.

The mandated California Division of Occupational Safety and Health (Cal-OSHA) requirement for firefighter safety, known as the "two-in-two-out rule", is also applicable. This rule requires a minimum of two personnel to be available outside a structure prior to entry by firefighters to provide an immediate rescue for trapped or fallen firefighters, as well as immediate assistance in rescue operations.

The SBCFD has implemented a dynamic deployment system for its fire engines, in addition to the traditional static deployment system from fire stations, when the station's engine is "in house". Dynamic deployment allows for the dispatching of engines already on the road for emergency calls by employing a Global Positioning System (GPS) to monitor the exact location of each engine in real time, rather than dispatching by a station's "first in area", as has been the previous practice. Previously, when an engine was out on routine (non-emergency) activities, such as inspections or training, the engine company was considered "in-service" and its exact location at any given moment in time was not known to County Dispatch. However, with dynamic deployment using the County's GPS, County dispatch has real time information on the exact location of each engine at all times and can dispatch the closest, un-engaged engine to an emergency incident, regardless of which fire station's service area the call originates from. This precludes the need for an in-service engine to have extended run times when another fire engine would be closer. The Fire Department has also added a battalion chief as the fourth fire fighter on scene, in order to meet the "two-in-two-out rule."

Station 14 has an engine company with a staff of three personnel, consisting of an engine company captain, engineer, and firefighter. Fire Station 14 meets the NFPA and SBCFD guidelines, as follows (City of Goleta, GP/CLUP Final EIR, Table 3.12-1; 2006):

- 1) The ratio of firefighters to population at Fire Station 14 is 1: 1,987.
- 2) Fire Station 14 currently serves a population of 5,960 (2000 Census), which is below the ratio of one engine company (three-person solely Station 14's first-in district crew) per 12,000 population by approximately 6,040 people.
- 3) Response time from Fire Station 14 is typically within 5 minutes, although the western edge of the City and some northern neighborhoods may experience a longer response time. Fire Station 14 is approximately 1.8 miles by road to the southwest of the project site and well within a five-minute response time.

#### ***Police Protection***

Police services are provided by the Santa Barbara County Sheriff's Department under contract with the City of Goleta. The City is divided into 3 patrol units, with 1 police car assigned to each unit. Additional police services are available from Santa Barbara County to supplement City police in an emergency. City police operate from three locations: the City offices at 130 Cremona Drive, an office located in Old Town on Hollister Avenue, and a third location at the Camino Real Marketplace.

#### ***Schools***

Public education services are provided by the Goleta Union School District (GUSD) and the Santa Barbara Unified School District (SBUSD). In general, enrollments in the area school system have been not been increasing and area schools serving the project vicinity are operating below capacity (Goleta Union School District 2017). These schools include La Patera Elementary School at 555 N La Patera Lane, Kellogg Elementary School at 475 Cambridge Drive, Goleta Valley Junior High and Santa Barbara Charter School co-located at 6100 Stow Canyon Road, Dos Pueblos High School at 7266 Alameda Ave., and San Marcos High School at 4750 Hollister Avenue.

#### ***Parks***

A more detailed discussion of parks is provided below under Recreation. The City currently contains 19 parks (including 3 that are privately-owned but publicly accessible), 21 open spaces (including 3 that are privately-owned but publicly accessible), and 1 community center (the Goleta

Valley Community Center). City parks are considered in combination with open space to provide recreational opportunities and encompass approximately 554 acres for a ratio of approximately 17.8 acres per 1,000 residents.

#### *Libraries*

Services at the Goleta Public Library are provided by the City of Goleta at 500 North Fairview Avenue. The 2-acre library site includes a 15,437 square foot (SF) building and parking areas. The facility provides services to the City and nearby unincorporated areas including Isla Vista, Hope Ranch, and the Gaviota Coast with a population of approximately 95,202. In the FY 2017/2018, the library had approximately 264,242 visitors and circulated 688,581 items, not counting the items that were downloaded electronically. Services were provided by 6 full-time and 15 part-time employees (Personal Communication from Allison Gray, Goleta Valley Library Director, July 15, 2019).

#### ii. Thresholds of Significance

A significant impact on public services would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist. In addition, the City's Environmental Thresholds and Guidelines Manual include thresholds of significance for potential impacts on area schools. Specifically, under these thresholds, any project that would result in enough students to generate the need for an additional classroom using current State standards would be considered to result in a significant impact on area schools. The City's Environmental Thresholds and Guidelines Manual notes State standards were: Grades K – 2, 20 students per classroom; Grades 3 – 8, 29 students per classroom; and Grades 9 – 12, 28 students per classroom. However, today the State standards are based on average class sizes of 31 (not to exceed 33) for kindergarten, 30 (not to exceed 32) for Grades 1 – 3, and 29.9 (or the district's average number of students per teacher in 1964, whichever is greater) for Grades 4 – 8 (California Department of Education 2018).

#### iii. Project Specific Impacts

##### **a) *Less Than Significant Impact***

##### *i. Fire Protection*

The project would result in an increase of religious service, office, event hall/kitchen, and storage square footage on the property. Fire protection requirements for the project would include, but would not be limited to, structural fires, emergency medical services, public assistance, and other requests. Once on the scene following any emergency call, the Fire Department would need adequate onsite fire protection facilities. The Fire Department has reviewed the project and determined that the plans prepared by the applicant are acceptable (SBCFD Memorandum, June 14, 2019). Access for the project must be maintained with a minimum 20-foot wide all-weather travel way that is serviceable and maintained for the life of the project. The project would require compliance with Fire Department standard conditions such as fire sprinklers, proper addressing, gated access, and payment of Fire Department development impact fees. Compliance with these standards in addition to implementation of the dynamic deployment system discussed above would reduce impacts to fire protection services to less than significant.

On December 6, 2018, the City approved the addition of a new Fire Station #10 to serve the western portion of the City. The addition of Fire Station #10 will improve the City wide firefighter to population ratio to 1:3,681 and will bring the service ratio into compliance with the City's minimum service standard upon commencement of normal operations. The

proposed project will not increase the intensity of use at the project location that would increase the existing demand for fire services, exacerbate the current firefighter to population ratios deficiency, or change the existing Station #14 response times. As such, the project would have a less than significant impact to fire protection service.

***ii. Police Services***

As stated above, the Santa Barbara County Sheriff Department provides 24-hour police protection services to the area under contract to the City of Goleta. Demand for police services resulting from the project, would not change measurably from baseline levels in the foreseeable future. Additionally, the project includes adequate patrol car access. Therefore, project related impacts on police services in the City would be less than significant.

**a) No Impact**

***iii.-v. Schools, Parks, Other Facilities***

Given the non-residential nature of the project and that existing operations would remain largely unchanged, the proposed project would not result in additional enrollment of school aged children in either the Goleta Union or Santa Barbara High School Districts. Similarly, the project would not generate any extra demand for parks and other public facilities/services. Therefore, the project would have no impact to schools, parks, or other public facilities.

**iv. Cumulative Impacts**

There are no project-specific significant impacts to public services as described above, as there would be less than appreciable change in intensity to existing onsite use from this project. When the recently approved Fire Station 10 on the west side of the City is constructed, a reduction in the existing deficiency in Citywide firefighter to population ratio will be resolved. The project would not result in a cumulatively considerable increase in the demand for public services and would result in less than significant cumulative impacts.

**v. Required/Recommended Mitigation Measures**

Based on the above analysis, no mitigation measures are required.

**vi. Residual Impact**

Based on the above analysis, residual project-specific and cumulative impacts to Public Services would be considered less than significant.

**P. RECREATION**

	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?				X	
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?				X	

i. Existing Setting

The City of Goleta has 19 parks (including 3 that are privately-owned but publicly accessible), 21 open spaces (including 3 that are privately-owned but publicly accessible), and 1 community center (the Goleta Valley Community Center), comprising a total of approximately 554 acres. This is approximately 17.8 acres per one thousand residents. The City has adopted a goal of providing 4.7 acres of parkland (open space lands whose primary purpose is recreation) per thousand residents.

ii. Thresholds of Significance

A significant impact on recreation would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist.

iii. Project Specific Impacts

**a-b) No Impact.** Given the scope and nature of the proposed site improvements (a 7,380 S.F. synagogue and 841 S.F. storage building), the project would not create a demand for, nor increase the use of, existing park/recreational facilities within the community. Further, no recreational facilities are proposed with this project, nor given the nature of the proposal would the project require the construction of additional recreation space. Therefore, no impacts associated with the construction of recreational facilities would occur.

iv. Cumulative Impacts

Because project would have no impact to recreation, the project contributions to cumulative recreation impacts would be less than significant.

v. Required/Recommended Mitigation Measures

Based on the above analysis, no mitigation measures are required.

vi. Residual Impact

Based on the above analysis, residual project-specific and cumulative impacts to Recreation would be considered less than significant.



**Q. TRANSPORTATION**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Conflict with a program plan, ordinance, or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?			X		
b. Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?			X		
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?			X		
d. Result in inadequate emergency access?			X		

This section incorporates the analysis, findings, and recommendations in the *Phase I Traffic Analysis for the 6045 Stow Canyon Road Project – City of Goleta* (Associated Transportation Engineers 2018) as well as the *Updated Traffic Study for the 6045 Stow Canyon Road Synagogue Project – City of Goleta* (Associated Transportation Engineers 2019). These documents are respectively provided in Appendices G and H of this document.

i. Existing Setting

The project site is served by a network of City streets and U.S. Highway 101. Access to the project site is provided from Stow Canyon Road west of North Fairview Avenue. Stow Canyon Road is a two-lane, undivided, major collector roadway that parallels Cathedral Oaks Road and serves the surrounding neighborhood as well as the Goleta Valley Junior High School and the Santa Barbara Charter School. There are no existing or planned bikeways on Stow Canyon Road; the nearest bikeway is the existing Class II path on Fairview Avenue (GP/CLUP Figure 7-6, November 2009). Sidewalks exist along the project frontage on both sides of Stow Canyon Road. The closest MTD bus stops are approximately 390 feet east and 940 feet west of the project site, respectively located on Fairview Avenue (at Stow Canyon Road) and on Stow Canyon Road (at the Goleta Valley Junior High School).

As described in the General Plan and the associated General Plan Final EIR (GP FEIR), the intersection of Stow Canyon Road and Fairview Avenue had a Level of Service (LOS) rated as “F” as of 2005 (see General Plan Table 7-1). This was anticipated to remain the same under full build-out by 2030. Planned transportation improvements (i.e., installing a signal at the intersection or other traffic flow regulating improvement), however, would improve the LOS rating at the intersection to a “B.” The time schedule for this improvement is presently unknown. Since 2005,

a new signal has been installed at the intersection of Berkeley Road and Fairview Avenue; Fairview Avenue, north of Stow Canyon Road, has been expanded from one lane to two lanes; and sidewalk and bike lane improvements have been made as well in this area. While the intersection of Stow Canyon Road remains unsignalized, the aforementioned improvements on Fairview Avenue have improved the LOS at the intersection of Stow Canyon Road and Fairview Avenue to a rating of "C". The primary cause of the existing, poor level of service is traffic associated with the start and end of the school day at the Goleta Valley Junior High School and the co-located Santa Barbara Charter School (Table TRA-1).

<b>Table TRA-1 School Statistics</b>		
<b>Statistic</b>	<b>Goleta Valley Junior High School</b>	<b>Santa Barbara Charter School</b>
First Day of School (2019-2020)	August 20	August 22
Last Day of School (2019-2020)	June 3	June 3
Grades Served	7 – 8	K – 6 <sup>3</sup>
Typical Class Start	0 Period: 7:30AM <sup>1</sup> 1 <sup>st</sup> Period: 8:30AM <sup>2</sup>	8:15 AM (all grades)
Typical Class End	6 <sup>th</sup> Period: 2:34 PM <sup>1</sup> 7 <sup>th</sup> Period: 3:24 PM	K: 1:30PM <sup>4</sup> 1 – 6: 2:50 PM <sup>4</sup>
Enrolled Students (Approximate)	780	Kindergarten: 35 <sup>3</sup> 1 – 6: 200 <sup>3</sup>
<b>Notes:</b> 1. Approximately 50 students at the Goleta Valley Junior High School attend 0 period; these students are released at the end of 6 <sup>th</sup> period. 2. 1 <sup>st</sup> Period at the Goleta Valley Junior High School begins at 9:15AM on Thursdays. 3. Additional students are enrolled in the Santa Barbara Charter School K-8 <sup>th</sup> grade "HomeBased Partnership" program located offsite, on Cathedral Oaks Road. 4. Santa Barbara Charter School ends classes at 1:00 PM on Fridays (all grades).		

U.S. Highway 101 is a four-lane, north-south interstate highway that connects the City of Goleta to the Cities of Santa Barbara, Carpinteria, and Ventura to the south and Cities of Buellton, Lompoc, and Santa Maria to the north. Calle Real and Cathedral Oaks Road are the primary east/west arterials on the north side of U.S. Highway 101 and vary from two to four lanes between Los Carneros Road and Fairview Avenue.

On major collector roadways such as Stow Canyon Road, City of Goleta Engineering Standard 4-050 requires a separation of 150 feet between driveway access points. Additionally, the Engineering Design Standards adopted by the City of Goleta require the fewest number of access points onto a City road (i.e., one access point or driveway) (Santa Barbara County Department of Public Works Transportation Division's Engineering Design Standards, Section 5.1[G], September 2011).

The project property has two existing driveway access points on Stow Canyon Road. The western driveway access point is for the synagogue's parking lot; its centerline is located approximately 91 feet east of the centerline for the nearest driveway on the adjacent property to the west, and was constructed as permitted under an SCD to the original CUP (92-CP-018) that was approved by the Santa Barbara County's Development Review Division on July 1, 1999. The eastern driveway access point is for the residence's former garage (converted to habitable residential space under 93-LUS-028); its centerline is located approximately 50 feet east of the centerline for the synagogue's parking lot access, and approximately 175 feet west of the centerline for the

nearest driveway on the adjacent property to the east. In addition to allowing the conversion of the former garage to habitable residential space, 93-LUS-028 also involved the removal of the existing former-garage driveway access point and the construction of a new driveway access point; its centerline was to be located near the eastern property boundary, approximately 103 feet east of the centerline for the existing driveway to the former garage.

The Santa Barbara County Association of Governments (SBCAG) coordinates with regional agencies, including the City of Goleta, to prepare and implement the SBCAG 2016 Congestion Management Program (CMP). The CMP coordinates regional and multi-jurisdictional issues related to congestion, land development, and air quality, and efficient use of limited transportation funds. The CMP defines the roadway facilities (intersections and road segments), appropriate roadways level of service (LOS) standards, performance measures including vehicle miles travelled (VMT), alternative transportation methods, land use impacts, and a capital improvement program. The City of Goleta General Plan Transportation Element establishes the Citywide LOS in coordination with SBCAG and is consistent with the CMP.

ii. Thresholds of Significance

A significant traffic impact would be expected to occur if the project resulted in any of the impacts noted in the above checklist. Additional thresholds of significance are set forth in the City's Thresholds Manual and include the following:

**Threshold TRA-1.** The addition of project traffic to an intersection increases the volume to capacity (V/C) ratio by the value provided below or sends at least 5, 10, or 15 trips to intersections operating at LOS F, E or D, respectively.

<u>LEVEL OF SERVICE</u> <u>(Including the project)</u>	<u>INCREASE IN V/C</u> <u>(Greater than)</u>
A	.20
B	.15
C	.10
<u>OR THE ADDITION OF</u>	
D	15 trips
E	10 trips
F	5 trips

**Threshold TRA-2.** Project access to a major road or arterial road would require a driveway that would create an unsafe situation, a new traffic signal, or major revisions to an existing traffic signal.

**Threshold TRA-3.** Project adds traffic to a roadway that has design features (e.g. narrow width, road side ditches, sharp curves, poor sight distance, inadequate pavement structure) or receives use which would be incompatible with a substantial increase in traffic (e.g. rural roads with use by farm equipment, livestock, horseback riding, or residential roads with heavy pedestrian or recreational use, etc.) that would become potential safety problems with the addition of project or cumulative traffic.

**Threshold TRA-4.** Project traffic would utilize a substantial portion of at least one intersection's capacity where the intersection is currently operating at acceptable levels of service (A-C) but with cumulative traffic would degrade to or approach LOS D (V/C 0.81)

or lower. Substantial is defined as a minimum change of 0.03 for intersections which would operate from 0.80 to 0.85 and a change of 0.02 for intersections which would operate from 0.86 to 0.90, and 0.01 for intersections operating [at a lower level of service (i.e., a V/C ratio exceeding 0.90)].

iii. Project Specific Impacts

***a,b, TRA-1, TRA-2, TRA-4) Less Than Significant Impact.*** Many of the uses planned within the new, 7,293-SF synagogue building are already occurring within the existing 2,445-sf synagogue/pre-school building. Further, many attendees at services and holiday events often do not drive but will walk to the site in keeping with religious practices and observances (Associated Transportation Engineers 2018). The project's location does not conflict with a transit plan or transit activities.

The project is not located within 1/4 mile of any of the intersections identified in the SBCAG 2016 CMP Table 4.3 *CMP Intersections Located Within Transit Priority Area*. As described above, the closest MTD bus stops are approximately 390 feet east and 940 feet west of the project site, respectively at the intersection of Fairview Avenue and Stow Canyon Road and on Stow Canyon Road at the Goleta Valley Junior High School.

The City Bicycle and Pedestrian Master Plan identifies implementation priorities for the City's vision for an integrated bicycle and pedestrian network; the nearest planned improvements are located on Fairview Avenue, approximately 390 feet east of the project site (City of Goleta 2018: Figure 4-3). The project's continued use of the property for residential, religious services, and educational services at incrementally increased intensities does not change the intensity of use of the existing transit, roadway, bicycle and pedestrian facilities that would result in a significant impact to a City plan, concept plan or implementing ordinance for the circulation system or with transit activities.

Project-related construction traffic would result primarily from construction personnel commuting to and from the project site. Given the small scale of the construction activities, it is expected that project construction would not require a substantial number of construction workers and would not generate a significant amount of construction-related traffic. While the applicant has not chosen a construction contractor, the CalEEMod v. 2016.3.2 estimation of the number of worker trips and vendor trips is minimal (see Summary Report Table *Trips and VMT* in Appendix C). Given this, and (1) the generally good existing traffic conditions on Stow Canyon Road and at the intersection of Stow Canyon Road and Fairview Avenue (outside of school start and end times); (2) existing school start and end times (see Table TRA-1, above); (3) the *Construction Timing* Condition of Approval (see Section 15.M, *Noise*) that limits construction activity and equipment maintenance to the hours between 8 AM and 5 PM; and (4) the *Construction Deliveries* Condition of Approval (discussed below in Section "v," *Required/Recommended Mitigation Measures*) that limits the delivery of construction materials to non-peak traffic periods when schools are in session; short-term traffic generation impacts would be less than significant as the arrival and departure of construction workers and materials deliveries would have little to no overlap with traffic associated with school start and end times.

Adequate area exists on the project site to provide parking for construction workers and to accommodate material and equipment staging outside of the 100-foot Stream

Protection Area, consistent with the *Stream Protection Area* Condition of Approval (see Section 15.D, *Biological Resources*).

In regard to long-term use of the site, the Updated Traffic Study (see Appendix H) applied composite rates developed from Institute of Transportation Engineers (ITE) and San Diego Association of Governments (SANDAG) sources, as each source has limited data available. The composite rate is applied to the net increase in synagogue size (7,293 SF) as shown in Table TRA-2 below. The Traffic and Parking analysis concludes that the project would result in a net increase of 72 average daily trips (ADT), 7 AM peak hour trips, and 13 PM peak hour trips (Table TRA-2). Tables 5 and 8 of the Updated Traffic Study (see Appendix H) show that these additional trips would have a minimal impact on Fairview Avenue's roadway operations and that the LOS "A" rating would remain as-is under both the current (Table 5) and cumulative (Table 8) scenarios. Impacts to nearby intersections are discussed below.

<b>Table TRA-2</b>							
<b>Project Trip Generation Estimates (Net Increase)</b>							
<b>Land Use</b>	<b>Size</b>	<b>ADT</b>		<b>AM Peak Hour</b>		<b>PM Peak Hour</b>	
		<b>Rate</b>	<b>Trips</b>	<b>Rate</b>	<b>Trips (In/Out)</b>	<b>Rate</b>	<b>Trips (In/Out)</b>
Synagogue <sup>1</sup>	7,293 SF	9.82	72	0.98	7 (3/4)	1.78	13 (6/7)
Notes:							
1. Trip generation rates per 1,000 SF of building area using average of ITE and SANDAG rates.							

Existing levels of service for nearby key intersections, and the number of trips that the proposed project would add to each intersection, are provided in Table TRA-3 (AM peak) and Table TRA-4 (PM peak). These tables also identify whether the impact would be significant based on the City's Thresholds Manual. The analysis considered the improvements on Fairview Avenue that have been made since 2005. As shown, the key intersections studied currently operate in the LOS A-C range under existing conditions and the project would add between 3 and 13 peak hour trips at these locations. This analysis is considered to be conservatively high, as many attendees at services and holiday events often do not drive but will walk to the site in keeping with religious practices and observances (Associated Transportation Engineers 2018) and because proposed changes to ongoing activities are minor. It should also be noted that the Fairview Avenue/Stow Canyon Road intersection operates at LOS C with the improvements that have been completed since 2005.

<b>Table TRA-3</b>				
<b>Existing LOS and Project-added Traffic (AM Peak Hour)</b>				
<b>Intersection</b>	<b>Existing</b>		<b>Project-Added</b>	
	<b>V/C</b>	<b>LOS</b>	<b>Trips</b>	<b>Significant Impact?</b>
Fairview Avenue/Stow Canyon Road	19.1 Sec <sup>1</sup>	C	7	No
Fairview Ave/Calle Real	0.61	B	5	No
Fairview Ave/US101 Northbound	0.65	B	5	No
Fairview Ave/US 101 Southbound	0.53	A	3	No
Notes:				
1. This intersection is not signalized; LOS is based on average weighted delay per vehicle, in seconds.				

Table TRA-4 Existing LOS and Project-added Traffic (PM Peak Hour)				
Intersection	Existing		Project-Added	
	V/C	LOS	Trips	Significant Impact?
Fairview Avenue/Stow Canyon Road	17.1 Sec <sup>1</sup>	C	13	No
Fairview Ave/Calle Real	0.71	C	8	No
Fairview Ave/US101 Northbound	0.60	A	8	No
Fairview Ave/US 101 Southbound	0.59	A	5	No
Notes:				
1. This intersection is not signalized; LOS is based on average weighted delay per vehicle, in seconds.				

Given the above analysis, the additional trips resulting from the project would not significantly impact traffic conditions at these intersections. Moreover, the LOS at the intersection of Fairview Avenue and Stow Canyon Road will further improve when the future planned improvements at the intersection (i.e., signalization) and Condition of Approval are implemented.

Lastly, the proposed project was estimated by the CalEEMod v. 2016.3.2 to generate 171,830 vehicle miles travelled (VMT) annually (see Summary Report Table 4.2, *Trip Summary Information*, in Appendix C). This analysis is considered to be conservatively high, however, as many attendees at services and holiday events often do not drive but will walk to the site in keeping with religious practices and observances (Associated Transportation Engineers 2018) and because proposed changes to ongoing activities are minor. As such, these project changes would not lead to a significant change in the VMT. The project is also located 2.1 miles from the Goleta Amtrak station. As noted above, the closest MTD bus stops are approximately 390 feet east and 940 feet west of the project site, respectively located on Fairview Avenue (at Stow Canyon Road) and on Stow Canyon Road (at the Goleta Valley Junior High School). Therefore, based on the nature of the project and its location, less than significant impacts are expected to occur.

Based on the above analysis, the proposed project will not have a significant effect to CEQA Checklist items a and b or City Threshold TRA-1, TRA-2, and TRA-4; additional discussion of City Threshold TRA-2 is also provided below.

**c, TRA-2, TRA-3) Less Than Significant Impact.** The proposed project, per the City's standards, would reduce the number of driveways on the property from two to one. The proposed driveway is located approximately 35 feet to the west of the existing synagogue (western) driveway. This would reduce the separation distance between the new, single driveway's centerline and that of the nearest access point to the neighboring property to the west from approximately 91 feet to approximately 56 feet. While the proposed separation distance is less than the 150 feet required by City of Goleta Engineering Standard 4-050, the line of sight on Stow Canyon Road is sufficient to minimize any potential safety concerns. Additionally, the single driveway proposed would reduce the number of points of conflict between pedestrians and vehicles and would provide the fewest number of access points onto a City road possible (i.e., one access point or driveway), consistent with the Santa Barbara County Department of Public Works Transportation Division's Engineering Design Standards, Section 5.1[G] (September 2011). Further, there is good line-of-sight available on Stow Canyon Road at the proposed location and there are no other design features (e.g. narrow width, road side ditches, sharp

curves, inadequate pavement structure) that would create an additional safety risk. The requirement to reduce the number of driveways onto Stow Canyon Road will reduce existing potential safety impacts and result in a less than significant impact.

- d) **Less Than Significant Impact.** The continued use of the site is compatible with the uses along Stow Canyon Road. There are no roadway geometric concerns (i.e. sharp curves, blind curves etc.) associated with the design of Stow Canyon Road that would impede emergency access to the site. The project would not generate traffic volumes as noted above that would impede Santa Barbara County Fire Department (SBCFD) access to the site. Further, SBCFD has reviewed the project's access and circulation plan and has found it acceptable subject to conditions which compliance will be required prior to issuance of occupancy permits (County of Santa Barbara Fire Department 2019). Based on nature and intensity of the use and the design of Stow Canyon Road, the project would not have the potential to significantly impact emergency access.

iv. Cumulative Impacts

Tables TRA-5 and TRA-6 provide the cumulative LOS and the number of trips that would be added to each intersection by the project for the AM and PM peak hour, respectively, for the key intersections analyzed above. As shown, the Fairview Avenue/Stow Canyon Road intersection is forecast to operate at LOS F during both the AM and PM peak hour periods with cumulative traffic volumes. The project would add 7 AM and 13 PM peak hour trips to the intersection, which represent an increase in volumes of 0.7%. This level of traffic would not generate significant cumulative impacts based on the City's Thresholds Manual, which states the significance threshold at intersections operating at LOS F is a 1.0% increase. As shown in the tables, other intersections studied operate at a LOS of B-D, and the increase in traffic volumes resulting from the project would be less than significant. It should also be noted that the future planned improvements at the Fairview Avenue/Stow Canyon Road intersection (i.e., signalization) would improve the LOS at this intersection to an "A" rating with cumulative volumes.

<b>Table TRA-5</b>				
<b>Existing LOS and Project-added Traffic (AM Peak Hour)</b>				
<b>Intersection</b>	<b>Existing</b>		<b>Project-Added</b>	
	<b>V/C</b>	<b>LOS</b>	<b>Trips</b>	<b>Significant Impact?</b>
Fairview Avenue/Stow Canyon Road	>50 Sec <sup>1</sup>	F	7	No
Fairview Ave/Calle Real	0.836	D	5	No
Fairview Ave/US101 Northbound	0.748	C	5	No
Fairview Ave/US 101 Southbound	0.685	B	3	No
Notes:				
1. This intersection is not signalized; LOS is based on average weighted delay per vehicle, in seconds.				

Table TRA-6 Existing LOS and Project-added Traffic (PM Peak Hour)				
Intersection	Existing		Project-Added	
	V/C	LOS	Trips	Significant Impact?
Fairview Avenue/Stow Canyon Road	>50 Sec <sup>1</sup>	F	13	No
Fairview Ave/Calle Real	0.791	C	8	No
Fairview Ave/US101 Northbound	0.885	D	8	No
Fairview Ave/US 101 Southbound	0.785	C	5	No
Notes:				
1. This intersection is not signalized; LOS is based on average weighted delay per vehicle, in seconds.				

Based on the above analysis and the Updated Traffic Study (Appendix H), the project will not increase the employment intensity or change land uses at the site that would contribute incrementally to cumulative traffic that would exceed the LOS or V/C ratio that would result in a conflict with the General Plan or SBCAG 2016 CMP that would result in a cumulative impact to the regional road system. Moreover, under CEQA Guidelines Section 15064.3(b), generally, projects within one-half mile of either an existing major transit stop or a stop along an existing high-quality transit corridor should be presumed to cause a less than significant transportation impact. Since the project would not result in a significant increase in VMT and is located near public bus service and in proximity to train service it is expected to not have a significant impact due to a conflict or inconsistency with CEQA Guidelines section 15064.3(b).

v. Required/Recommended Mitigation Measures

No mitigation measures are proposed or needed. However, the following conditions of approval will be imposed. In addition, condition of approval *Construction Timing* (see Section 15.M, *Noise*) limits construction activity and equipment maintenance to the hours between 8 AM and 5 PM.

**Construction Materials and Equipment Delivery.** Delivery of materials and equipment for construction must be limited to non-school peak traffic periods when either the Goleta Valley Junior High or the Santa Barbara Charter School is in session unless a specific need arises and appropriate measures are developed to the satisfaction of the Planning and Environmental Review Director, or designee.

**Unimpeded Right Turn for Eastbound Stow Canyon Road.** Improvements shall be installed prior to the request for Certificate of Occupancy Building Permit issuance to allow for an unimpeded right-turn at all times for eastbound traffic on Stow Canyon Road at the Fairview Avenue intersection. Improvements may include, but are not limited to, a no parking zone, red curb painting, and restriping. Improvements shall be constructed to the satisfaction of the Public Works Director, and at the Applicant's cost.

vi. Residual Impact

Based on the above analysis, residual impacts to traffic and transportation systems would be less than significant.



**R. TRIBAL CULTURAL RESOURCES**

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:					
i. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or		X			
ii. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resource Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.		X			

This section incorporates the analysis, findings, and recommendations in the *Phase I Archaeological Investigation for 6045 Stow Canyon Road, APN 077-170-044, Goleta, California* (Stone 2018) as well as a supplemental memorandum (Stone 2019) in response to the City of Goleta's request for additional information. These documents are referenced herein as Appendix D and contain confidential information that is kept on file with the City of Goleta and may be reviewed with prior authorization by the City of Goleta Planning and Environmental Review Department in accordance with applicable law. Cultural Resources are also addressed in Section E, above.

i. Existing Setting*Prehistoric Setting*

Evidence exists for the presence of humans in the Santa Barbara coastal area for thousands of years. While some researchers have proposed that the Santa Barbara Channel area may have been settled as early as 40,000 years ago, only limited evidence for occupation much earlier than

9,500 years has been discovered. Even so, human prehistory along the Santa Barbara channel area coast may extend back as much as 12,000 years. Beginning approximately 7,500 years ago, prehistoric human settlement in the local area apparently increased rapidly with a number of sites dating to approximately this time, and many more dating subsequent to it (General Plan Final EIR [GP FEIR]).

### *Ethnographic and Historic Setting*

Historically, settlement in the vicinity of the project site was defined by three periods: the Mission Period (AD 1769 to 1830), the Rancho Period (AD 1830 to 1865), and the American Period (AD 1865 to 1915). The first European contact to the Santa Barbara coastal region was by Portuguese explorers in 1542, followed by the Spanish in 1602. At the time of this first European contact in 1542, the Goleta area was occupied by a Native American group speaking a distinct dialect of the Chumash Language (General Plan Final EIR [GP FEIR]). This group later became known as the Barbareno Chumash. The Chumash were hunters and gatherers who lived in areas surrounding the much larger prehistoric Goleta Slough. At the time of Spanish contact, the prevalent Chumash population had at least 10 Chumash villages in the Goleta Area and immediate vicinity (GP FEIR).

As provided in the City's General Plan Final EIR (Section 3.5, Cultural Resources), the City contains prehistoric, ethnographic, historical, and paleontological resources. GP/CLUP Figure 6-2 (November 2009) shows areas that contain sensitive historic/cultural resources and identifies 46 historic resource locations.

A Phase 1 Archaeological Investigation was completed in June 2018 for the proposed project that included an archaeological site records and literature search as well as an intensive surface survey of the property (Stone 2018). The literature and records search identified (1) two previously recorded archaeological sites are within a 0.25-mile radius, (2) two previous investigations conducted within the project area (a ground surface survey and subsequent Extended Phase 1 backhoe testing program), and (3) thirteen other investigations conducted within a 0.25-mile radius of the project site. Past and present on-site surveys identified low densities of estuarine shellfish fragments and fire altered rock within the southeastern portions of the project site and estuarine three shellfish fragments within the western portion of the project site. The report concluded that:

"The presence of surface shellfish fragments is a function of placement of fill when Las Vegas Creek was realigned from within the site to outside its western boundary between 1929 and 1938, and/or slough fill soils that were imported to the property when existing avocado trees were planted. The shell fragments on the ground surface are therefore not associated with a prehistoric or historic archaeological site."

Additionally, a supplemental memorandum (Stone 2019) concluded that "soils below 2 feet from the surface were formed prior to Native American occupation in this area" and that disturbance of soils below 2 feet from the surface "would not have the potential to disturb unknown buried archaeological resources."

California Native American tribes traditionally and culturally affiliated with this area have been notified of the project pursuant to Public Resources Code Section 21080.3.1. Eight tribes were sent consultation requests in accordance with Assembly Bill 52; one tribe, the Santa Ynez Band of Chumash Indians responded. On February 8, 2019, Santa Ynez Band of Chumash Indians

indicated that they did not want to formally consult and that they would accept the standard Conditions of Approval requiring disclosure in the advent of an inadvertent discovery. No other responses were received.

ii. Thresholds of Significance

The project would be considered to have a significant impact on tribal cultural resources if it were to cause a substantial adverse change in the significance of a tribal cultural resource as defined in the checklist above.

iii. Project Specific Impacts

**a.i, ii) Less Than Significant.** An archaeological site records and literature search of the California Historical Resources Information System (CHRIS) Central Coast Information Center (CCIC), University of California, Santa Barbara, was conducted on December 7, 2017 (Stone 2018, 2019). The records search identified all recorded archaeological sites and investigations within the proposed Project area and a 0.25-mile buffer distance. The records search determined that (a) no previously recorded archaeological sites are within the project site; (b) two previously recorded archaeological sites are within a 0.25-mile radius; (c) two investigations, a ground surface survey and subsequent Extended Phase 1 backhoe testing program, were conducted within the project area; and (d) 13 other investigations have been conducted within a 0.25-mile radius.

The project site is not listed or described in a Sacred Lands File, is not listed among the sites identified on General Plan Figure 3.5-1 (Historic Resources), and is not officially designated or recognized as a historically significant site in the CHRIS system by a local government pursuant to a local ordinance or resolution.

As described above, the Phase I Archaeological report and supplemental memorandum concluded that shell fragments on the ground surface are not associated with a prehistoric or historic archaeological site and that soils below 2 feet from the surface were formed prior to Native American occupation in this area and that disturbing these soils would not have the potential to disturb unknown buried archaeological resources. Based on these factors, and the input received through consultation, the project would not result in a substantial adverse change in the significance of a tribal cultural resource as defined in Public Resources Code section 21074 and impacts would be less than significant with implementation of MM-CUL-3.

iv. Cumulative Impacts

In general, cumulative impacts to tribal cultural resources would occur when a series of actions leads to the loss of a substantial type of site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe. Consistent with the City General Plan, potential project related contributions to cumulative impacts to currently undiscovered tribal cultural resources in the incorporated City of Goleta are reduced to less than significant by implementation of Mitigation Measure CUL-3.

v. Required/Recommended Mitigation Measures

Mitigation Measure CUL-3 (See Section E, *Cultural Resources*, above) will ensure that if, in the unlikely event of discovering a Native American/cultural resource, the work would stop and the appropriate measures would be taken. This mitigation measure is proposed by the City and agreed to by the applicant. Implementation of Mitigation Measure CUL-3 will be verified by the City through a Mitigation Monitoring and Reporting Program (MMRP). Therefore, with the implementation of Mitigation Measure CUL-3 and the MMRP, the City has a mechanism to verify that any potentially significant impacts to the historic barn would be reduced to less than significant.

vi. Residual Impact

With Mitigation Measure CUL-3 implemented, less than significant CEQA-defined residual impacts to Tribal Cultural Resources would occur due to the project.

**S. UTILITIES AND SERVICE SYSTEMS**

<b>Would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?			X		
b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?			X		
c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?			X		
d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?			X		
e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?			X		

i. Existing Setting*Wastewater Treatment*

Wastewater in the project area is collected and treated by the Goleta Sanitary District (GSD) at the Goleta Wastewater Treatment Plant (GWWTP). The GWWTP has a design capacity of 9.7 million gallons per day (mgd), based on an average daily flow rate. However, the discharge is restricted under the facility's National Pollution Discharge Elimination System (NPDES) permit (Permit No. CA0048160) (a Clean Water Act Requirement by the U.S. EPA), to a daily dry weather discharge of 7.64 mgd (RWQCB, 2010). GSD owns 59.22 percent of the capacity rights at the GWWTP, which gives GSD an allotment of 4.52 mgd of treatment capacity. GSD currently contributes 2.54 mgd in flow to the GWWTP, leaving GSD 1.98 mgd of remaining capacity.

The plant's treatment system consists of primary settling, biofiltration, aeration, secondary clarification, chlorine disinfection, and de-chlorination. Wastewater flows greater than 4.38 mgd receive primary treatment only and are blended with treated secondary wastewater prior to disinfection and discharge to the ocean. Treated wastewater is discharged to the Pacific Ocean

through a diffuser 5,912 feet offshore at a depth of approximately 87 feet. In May 2013, the GSD treatment facilities were upgraded from the partial secondary blended process. With the plant upgrades completed, the plant is able to discharge effluent that has been treated to full secondary standards as well treat some wastewater to the tertiary standards required for recycled water use (Goleta Sanitary District 2018: 3).

#### *Water Sources, Supply, and Demand*

The Goleta Water District (GWD) is the water purveyor for the City of Goleta and surrounding areas. The GWD service area is located in the southern portion of Santa Barbara County with its western border adjacent to the El Capitan State Park, its northern border along the foothills of the Santa Ynez Mountains and the Los Padres National Forest, its eastern border along the City of Santa Barbara, and its southern border along the Pacific Ocean. The service area encompasses approximately 29,000 acres and includes the City of Goleta, University of California, and Santa Barbara Airport (City of Santa Barbara property); the remainder of the service area is located in the unincorporated County of Santa Barbara. GWD provides water service to approximately 87,000 people through a distribution system that includes over 270 miles of pipeline, as well as nine ground water wells and nine reservoirs (Goleta Water District 2019).

#### *Drainage Facilities*

All stormwater runoff, as well as tailwater from landscape irrigation onsite, flows southwest to Las Vegas Creek and ultimately to the confluence of San Jose Creek and Atascadero Creek located east of the Goleta Slough.

#### *Electric power, Natural Gas, and Telecommunications Facilities*

Electric power, natural gas, and telecommunications services are respectively provided by Southern California Edison, Southern California Gas and Cox Communications and a variety of cellular providers.

#### *Landfill Capacity and Solid Waste*

The County of Santa Barbara owns and operates (through its Public Works Department) the Tajiguas Landfill as well as the South Coast Recycling and Transfer Station. The management of solid waste by the County's Public Works Department includes collection, recycling, disposal, and mitigation for illegal dumping. Within the City of Goleta, collection services are provided by Marborg Industries. Waste generated in the City of Goleta is handled at the South Coast Recycling and Transfer Station where recyclable and organic materials are sorted. The remaining solid waste is disposed of at the Tajiguas Landfill.

The Tajiguas Landfill is the only active landfill in Santa Barbara County (County of Santa Barbara 2018a). The landfill encompasses 497 acres, with a permitted operational area of 357 acres. Of this, the total permitted waste footprint is 118 acres for a capacity of 23.3 million cubic yards (County of Santa Barbara 2018b). The permitted waste area is comprised of both lined and unlined (pre-Subtitle D) areas. The Tajiguas landfill is permitted to accept up to 1,500 tons of municipal solid waste and yard waste per day (Santa Barbara County 2015:1-7, 1-8).

Based on current waste disposal rates, the landfill will reach permitted capacity in approximately 2036, based on current projections of materials delivery to the landfill and assuming timely completion and expected performance of the Tajiguas Resource Recovery Project (TRRP) that

would increase waste diversion (e.g., compost and recycling) rates. The landfill is classified as a Class III (non-hazardous solid waste) disposal facility (County of Santa Barbara 2018:20,30).

ii. Thresholds of Significance

A significant impact would be expected to occur if the proposed project resulted in any of the impacts noted in the above checklist. In addition, the City of Goleta's *Environmental Thresholds and Guidelines Manual* (Section 17, *Solid Waste Thresholds*) provides the following applicable thresholds to determine whether significant noise impacts would occur:

**Threshold USM-1.** A project would result in a significant impact on the City's landfill capacity if it would generate more than 196 tons of solid waste per year, after a 50% reduction credit is given due to recycling efforts.

**Threshold USM-2.** Projects with a project-specific impact as identified above (196 tons/year or more) are also considered to have a cumulatively significant impact. Additionally, projects that would generate more than 40 tons or more tons per year (but less than 196 tons per year) of solid waste are considered to have a less than significant but adverse (i.e., a Class III) impact to regional solid waste and mitigation should be recommended.

iii. Project Specific Impacts

a) ***Less Than Significant.*** All utilities exist on site to serve the development and have the capacity to support the development. The relocation, addition, and/or extension of onsite utilities to serve proposed structures will occur within the footprint of existing onsite development and is minor in nature. The onsite stormwater drainage will be required to be designed and constructed in compliance with Regional Water Quality Control Board regulations and City of Goleta development standards prior to issuance of a building permit. Therefore, the project would result in less than significant environmental effects from addition of onsite utility distribution lines and stormwater drainage. The project would not require the development or relocation of electric power, natural gas, or telecommunications facilities.

b,c) ***Less Than Significant.*** Water for the project would be provided by the Goleta Water District (GWD). Based on demand study and project use description prepared by MAC Design Associates, dated December 2, 2016, and landscape demand prepared by Earthform Design, dated February 17, 2017, and accepted by GWD per the Preliminary Water Service Determination Letter dated October 12, 2018, the new synagogue building with proposed property development, activities (e.g., religious and educational services) of existing structures, and retrofits would be 0.95 acre-feet of annual demand, which is less than the property's historic water supply credit of 0.97 acre-feet per year (Goleta Water District: July 11, 2019 email from Jim Heaton). As such, the project will not substantially change water use onsite and development of the project would pose a less than significant impact on the area's water supply.

The project would not change existing uses onsite but would result in an overall increase of 6,641 S.F. in building area. While the project would result in a net increase in wastewater produced onsite, average discharge to District facilities has been less than 150,000 gallons annually and the Goleta Sanitary District anticipates that future additional flows resulting from the project would not be a significant factor to either the District

collection or treatment facilities (Goleta Sanitary District 2019: July 12 email from Luis Astorga). Additionally, the project already has a Sewer Service Connection Permit from the GSD to guarantee sewer service and would be required to obtain a service extension to the new synagogue. Therefore, the project's contribution to waste water discharge would be less than significant.

***d,e, USM-1) Less Than Significant.***

***Construction/ Demolition Debris***

The California Green Building Code requires demolition of any structure requiring a permit to divert 65% of the construction materials generated during construction. Therefore, the City has implemented a mandatory Construction and Demolition (C&D) Debris Recycling Program to divert at least 65% of these highly recoverable materials from the landfill in accordance with state law. To address the waste, diversion reporting is required after construction in accordance with the City of Goleta's Construction and Demolition Debris Recycling Program Waste Reduction and Recycling Guidance Document. The applicant will be required to substantiate how a 65% diversion factor was achieved during construction. Compliance with adopted Green Building Code requirements will reduce the project's short-term waste generation impacts to a less than significant level.

***Long Term Operations***

The City's Thresholds Manual provides a project-specific threshold of 196 tons of solid waste generated per year, which is equivalent to the annual waste generated by 70 single-family dwellings. The project would not change existing uses onsite but would result in an overall increase of 6,641 S.F. in building area. The City of Goleta's *Environmental Thresholds and Guidelines Manual* provides specifies methods assess the impact associated with residential, commercial, industrial, and institutional projects, although none are specific to religious institutions. Conservatively applying the "Educational Institutions" annual generation rate (of 0.0010 tons/year/square foot of new development), the proposed project would generate an additional 6.75 tons/year of solid waste, and at least half of this would be diverted (e.g., recycled or composted). Given this, and the fact that the proposed changes to ongoing activities are minor, the project would not have the potential to generate 196 tons of solid waste annually. Therefore, the project's operational specific impact on solid waste disposal capacity at the Tajiguas Landfill would be less than significant.

iv. Cumulative Impacts

Based on the above analysis, the proposed project would not result in a substantial increase in use of available utility capacity, including the GWD's water supply, GSD's sewage treatment capacity, or the City storm drain system. Regarding disposal capacity at the Tajiguas Landfill, for the reasons described above, the project would not have the potential to generate 40 tons/year of additional solid waste and therefore would not exceed Threshold USM-2. Therefore, the project's public utility impacts would not be cumulatively considerable or significant.

v. Required/Recommended Mitigation Measures

No mitigation measures are proposed or needed.



vi. Residual Impact

The project would result in no residual impacts to utilities and service systems.

**T. WILDFIRE**

<b>If located in or near a state responsibility areas or lands classified as very high fire hazard severity zones, would the project:</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant With Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>	<b>See Prior Document</b>
a. Substantially impair an adopted emergency response plan or emergency evacuation plan?			X		
b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of wildfire?			X		
c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?			X		
d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?			X		

i. Existing Setting

The project site is not located in a very high fire hazard severity zone or in or near areas of state responsibility. The site is located in a designated Local Responsibility Area (LRA) as designated on the California Department of Forestry and Fire Protection Fire Hazards Severity Zone in State Responsibility Areas Map (California Department of Forestry and Fire Protection 2007).

ii. Thresholds of Significance

The project would have a significant impact if it is near a state responsibility areas or lands classified as very high fire hazard severity zones, if the project were found to cause an impact defined in the above checklist.

iii. Project Specific Impacts

- a) **Less Than Significant Impact.** The project is located approximately 1,880 feet south of the nearest designated High Fire Hazard Area in a state responsibility area. The project will consist of residential, office, religious service, educational service, storage, parking, and landscaping uses, all of which exist on-site and are located in an urban area that receives fire protection from the County of Santa Barbara Fire Department. The project does not propose a use that has been determined to be inconsistent with adopted plans including emergency response plans or an evacuation plan, and therefore would have a less than significant impact to such plans.

**b,c) Less Than Significant Impact.** The project is not located on moderate or steep slopes, in an area with potential wildfire fuels, or an area with difficult or constrained access. Therefore, the project would not substantially increase existing wildfire risks. The project would not introduce new structures or people into areas with an existing high wildfire risk, nor does the project propose infrastructure or utility construction requiring fire breaks. The project is in an urbanized area wherein prevailing winds could carry wildfire smoke and ash to the project site. This is an existing situation that affects the entire City and is not unique to the project site. Since the project is not proposing new uses within or adjacent to a designated wildfire hazard area, and would not substantially increase existing wildfire risk, the project would not result in a significant increase in potential wildfire-related impacts.

**d) Less Than Significant Impact.** The project is located on a developed urban site. Although small portions of the project site (i.e., the entrance to the existing and proposed synagogue parking lot, as well as the westernmost corners of the property [north and south]) are located within the 100-year flood zone (FEMA 2012), all proposed structures would be located outside the 100-year flood zone. While there is a slightly elevated risk of post-fire flooding and mud flows given the proximity to Las Vegas Creek, the exposure is minimal given the distance of the building away from the creek, the elevation of the site, and relatively small increase in building area, and distance from mountainous areas. As such, neither people nor structures would be exposed to significant risks such as downslope or downstream flooding or landslides because of runoff, post-fire slope instability, or drainage changes. Additionally, the project site is well removed (i.e., is located approximately 1,880 feet) from a designated High Fire Hazard zone located in a state responsibility area. Therefore, the project would not be positioned in a manner that would directly or indirectly exacerbate the risk of a natural disaster by bringing new development to vulnerable areas and would have a less than significant impact.

iv. Cumulative Impacts

The project is located approximately 1,880 feet (0.36 mile) south of the nearest designated High Fire Hazard Area in a state responsibility area. The project will consist of residential, religious services, and educational services uses and located in an urban area that receives fire protection from the County of Santa Barbara Fire Department. The project would not substantially increase existing wildfire-related impact risk on the project site or other existing development in the City. Therefore, the project's cumulative wildfire-related impacts would not be cumulatively considerable and would not be significant.

v. Required/Recommended Mitigation Measures

No mitigation is required as the project would have a less than significant impact to state responsibility areas.

vi. Residual Impact

Based on the above analysis, residual project-specific and cumulative impacts to Wildfire would be considered less than significant.

**U. MANDATORY FINDINGS OF SIGNIFICANCE**

	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact	See Prior Document
a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?			X		
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?			X		
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?			X		

- a)** The project is located within an urbanized area on a site developed with residential, office, religious service, educational service, storage, parking, and landscaping uses. Section 15.D, *Biological Resources*, fully analyzes the potential direct as well as indirect impacts (e.g., new lighting) on biological resources, including potential impacts to Las Vegas Creek. The analysis concluded there may be project effects on roosting and nesting birds during construction activities. As such, the *Nesting Birds* Condition of Approval was proposed by the City and agreed to by the applicant. Therefore, with the implementation of the *Nesting Birds* Condition of Approval and the MMRP, the City has a mechanism to verify that any impacts to migrating and nesting birds would be reduced.

The Cultural and Tribal Resources sections of this study indicates possible project effects on cultural and tribal resources. The Cultural and Tribal Cultural Resources sections detail mitigation for reducing impacts to these important Cultural and Tribal Cultural Resources to less than significant.

- b)** This project is consistent with the designated land uses in the City of Goleta General Plan. This initial study has identified potential impacts in the areas of biological resources and

cultural/tribal cultural resources that individually are limited and require mitigation to ensure that the impacts would be reduced to a less than significant level both incrementally and cumulatively. The project approval is conditioned upon implementation of these mitigation measures that avoid incremental effects that would emerge with implementation of cumulative projects.

- c) Project effects on human beings related to cultural resources, noise, hydrology, and transportation/traffic have been analyzed in this study. Impacts on human beings would be less than significant with the incorporation of Mitigation Measures, where required.

## **16. PREPARERS OF THE INITIAL STUDY, CONTACTS, AND REFERENCES**

This document was prepared by City of Goleta Planning and Environmental Review Department staff.

### **Contributors and Contacts:**

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Jim Heaton, Goleta Water District  
Desmond Ho, Santa Barbara County Air Pollution Control District

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**18. APPENDICES:**

- A. Mitigation Monitoring and Reporting Program
- B. Project Plans (11" x 17" reductions)
- C. CalEEMod Version 2016.3.2 Modeling Results
- D. *Phase I Archaeological Investigation for 6045 Stow Canyon Road, APN 077-170-044, Goleta, California* (Stone, June 2018)  
*Memorandum regarding 6045 Stow Ranch Road Archaeological Resources Investigation; 18-031-CUPAM-DP* (Stone, March 11, 2019)  
*--Confidential file access by appointment and demonstrated need only. Contact Planning and Development Department--*
- E. *Historic Resources Evaluation for Out-Building at 6045 Stow Canyon Road, City of Goleta, California, Assessor's Parcel Number 007-717-044* (Shelley Bookspan, January 26, 2007)
- F. *Preliminary Stormwater Control Plan for Chabad Synagogue, 6045 Stow Canyon Rd.* (MAC Design Associates, March 29, 2019)
- G. *Phase I Traffic Analysis for the 6045 Stow Canyon Road Project – City of Goleta* (Associated Transportation Engineers 2018)
- H. *Updated Traffic Study for the 6045 Stow Canyon Road Synagogue Project – City of Goleta* (Associated Transportation Engineers 2019)
- I. Comments Received During Public Review Period Ending November 20, 2019
  - i. CA Department of Conservation, Division of Oil, Gas, and Geothermal Resources (November 6, 2019)
  - ii. Applicant Comments (November 19, 2019)

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## **APPENDICES**

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## **APPENDIX A**

### **Mitigation Monitoring and Reporting Program**

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**APPENDIX A**  
**NEW SYNAGOGUE BUILDINGS AND SITE IMPROVEMENTS FOR CHABAD OF SANTA BARBARA**  
**MITIGATION MONITORING AND REPORTING PROGRAM**  
**Case No. 18-031-DP-CUPRV-DRB**

Environmental Conditions of Approval/Mitigation Measures	Responsible Party Obligation	Time Frame	Monitoring Party
<b>Cultural Resources and Tribal Cultural Resources</b>			
<p><b><u>MM-CUL-1: Historic Building Relocation.</u></b> The Applicant/Permittee, at its sole expense, shall make a good faith effort to relocate the barn/storage building. Prior to the issuance of a demolition permit, the Applicant shall offer the building to be moved, at the expense of the recipient, to another site including a good-faith effort (as determined by the Planning and Environmental Review Director, or designee) to work with a Realtor to advertise the building. The order of preference for relocation is 1) in Goleta; 2) in the South Coast area of Santa Barbara County; 3) in Santa Barbara County; and 4) outside of the County. This offer will be made through an advertisement placed in a paper general circulation twice within a 60-day time period, and a direct offer to local preservation groups including the Goleta Valley Historical Society.</p>	Chabad of Santa Barbara	<p>Prior to publishing the notice, the notice shall be submitted to the City for review and approval.</p> <p>Prior to the issuance of a demolition permit, the applicant must provide evidence of the good faith outreach effort to relocate the building including seeking the services of a Realtor (for a period of no less than 90 days) to assist in finding a receiver for the building to the satisfaction of the Planning and Environmental Review Director, or designee.</p>	Planning Director or designee shall verify compliance before issuance of the demolition permit
<p><b><u>MM-CUL 2: Historic Building Documentation.</u></b> The Permittee/Applicant shall have a City- approved architectural historian provide written and photographic documentation of the barn/storage garage's historical features and former setting using historic photographs, measured drawings, and archival quality photography. The City shall review and approve the documentation prior to</p>	Chabad of Santa Barbara	The applicant shall secure approval of the architectural historian by the City of Goleta prior to the documentation phase. Further, the review of the written and photographic history of the site must be	Planning Director or designee shall verify compliance before issuance of the demolition permit ; qualified

Environmental Conditions of Approval/Mitigation Measures	Responsible Party Obligation	Time Frame	Monitoring Party
<p>issuance of demolition permit. One copy of the historical documentation shall be kept with the City of Goleta and copies shall be deposited with the Goleta Valley Historical Society and in the Special Collections Department of the Davison Library of the University of California, Santa Barbara.</p> <p><b>MM-CUL-3: Discovery of Cultural Resources.</b> If archaeological resources are encountered during grading, work must be stopped immediately or redirected until a City-approved archaeologist and local Chumash Native American consultant can evaluate the significance of the find pursuant to the Phase 2 investigation standards set forth in the City Archaeological Guidelines. The Phase 2 study must be funded by the Applicant/Permittee at his sole expense. If resources are found to be significant, they must be subject to a Phase 3 mitigation program consistent with City Archaeological Guidelines. The Phase 3 mitigation program must be funded by the Applicant/Permittee.</p>	Chabad of Santa Barbara	<p>completed, approved, and submitted to the designated repositories prior to the issuance of a demolition permit.</p> <p>If archaeological resources are encountered during grading, the identification of the City-qualified archaeologist and Chumash Native American consultant shall be approved by the City prior to additional grading in the vicinity of the find. The monitors must be onsite during all project excavation, grading or other soil disturbance required to conduct the Phase 2, and if necessary, Phase 3 investigations.</p>	<p>architectural historian</p> <p>Planning Director or designee must verify compliance before grading/construction in the vicinity of the find may be resumed; qualified archaeologist; Native American observer</p>
<b>Tribal Cultural Resources</b>			
Refer to Mitigation Measure CUL-3.			

**I have read and agree to the conditions and mitigation measures in this document:**

Applicant's Name

Signature

Date



## ATTACHMENT 1

### EXHIBIT 2

#### Conditions of Approval

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## **EXHIBIT 2**

### **CONDITIONS OF APPROVAL NEW SYNAGOGUE BUILDINGS FOR CHABAD OF SANTA BARBARA DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT REVISION 18-031-DP-CUPRV**

In addition to all applicable provisions of the Goleta Municipal Code (“GMC”), Chabad of Santa Barbara (“Applicant(s)”, “Developer(s)”, or “Permittee(s)”) agrees to the following conditions for the City’s approval of Case No. 18-031-DP-CUPRV (“Project Conditions”).

Unless the contrary is stated or clearly appears from the context, the construction of words and phrases used in these Project Conditions use the definitions set forth in the GMC. For purposes of these Project Conditions, the term “Director” refers to the Planning and Environmental Review Director, or designee.

#### **AUTHORIZATION**

1. This Development Plan and Conditional Use Permit Revision, Case No. 18-031-DP-CUPRV, authorizes implementation of plans dated November 7, 2019, and attached hereto, subject to the Conditions of Approval set forth below, including mitigation measures and specified plan sheets and agreements included by reference, as well as all applicable City rules and regulations. The project is approved as stated below.
  - a. A Development Plan (DP) with a parking modification in regard to the manner in which the residential parking is provided to allow for the following:
    - i. Construction of a new, approximately 7,293-square foot (SF), one-story synagogue with 815 SF roof terrace (8,108 SF total). The building will include a sanctuary, event hall, kitchen, offices, conference rooms, bathrooms, storage, and mechanical and electrical space. The tallest proposed roof line is 24 feet, 6.5 inches, although the building also includes an architectural projection that would have a maximum height of 34 feet, 7.25 inches. Solar panels will also be included on the roof.
    - ii. Construction of a new, 841-SF, one-story storage building. The maximum height would be approximately 16 feet.
    - iii. Removal of the existing, 1,440-SF, approximately 80-year-old barn/storage garage.
    - iv. Approval of the existing, 176-SF, open-sided, lean-to storage container.
    - v. Removal/demolition of an existing, 120-SF trailer and a 53-SF shed.
    - vi. Consolidation of access to the site into a single driveway. This entails the removal of the existing residential driveway and the removal and relocation

of the existing synagogue driveway to approximately 35 feet west of its current location.

- vii. Replacement, reorientation, and expansion of the existing, 24-space parking area (including 1 van-accessible space) with a 31-space parking area. The 31-space parking area would include 3 van-accessible spaces as well as 3 spaces designated for the residence. The three parking spaces for the residence shall be provided within a carport structure and be connected to the residence via a covered walkway.
  - viii. Addition of landscape improvements to integrate the proposed synagogue and storage building with existing buildings and provide landscape buffers for neighboring properties and the Las Vegas Creek. Plants are intended to be generally native and drought tolerant.
  - ix. Addition of two, new bioretention basins.
  - x. Additional improvements on the site to accommodate the above proposed changes, including hardscape, new site lighting, and a trash enclosure.
  - xi. No development is allowed within the General Plan-required, 100-foot Stream Protection Area setback from Las Vegas Creek.
  - xii. The existing well may be used to supplement irrigation water; any use shall not exceed 2.4 acre-feet per year.
- b. A Revision to the existing Conditional Use Permit (92-CP-018) to permit the expanded synagogue and pre-school/daycare activities as described below:
- i. Synagogue:
    - 1. Services and programs associated with the synagogue, including activities typically associated with a religious institution, including but not limited to worship, fellowship events, summer camp, and educational activities, etc.
    - 2. Activities would occur throughout the year on varying days of the week and times of the day.
    - 3. The number of people would fluctuate depending on the specific activity. The maximum number of attendees anticipated is 113 persons, including members, guests, and staff; such events are anticipated to occur on average once per week.
    - 4. Services and programs described above would occur within the new 7,293-SF synagogue and support and ancillary activities may take place within the existing 2,445-SF synagogue/pre-school building. Outdoor facilities (refer to the Development Plan description above) may also be utilized.

5. Any future special events such as interfaith-gatherings, open houses, carnivals, and similar events would be regulated separately as needed and provided for in City regulations.
- ii. Pre-School/Daycare:
  1. The pre-school/daycare would operate year-round, Monday through Friday, from 9AM through 3PM.
  2. Attendees would be limited to no more than 20 students and 4 teachers.
  3. Events may utilize the existing 2,445-SF synagogue/pre-school building as well as outdoor facilities
2. These Conditions of Approval and the associated Development Plan and Conditional Use Permit Revision rescind and replace all previous approvals, including 92-CP-018.
3. All construction, improvements, implementation, and/or any other actions taken pursuant to this permit shall be in substantial conformance with the project. Any deviations from the project must be reviewed and approved by the City of Goleta (City). The City shall determine whether any deviation substantially conforms to the project. Any deviation determined to not be in substantial conformance with the project requires the Applicant/Permittee to seek additional approval, permits, or other action by the City. Any deviation from the project made without the above-described review and approval of the City is a violation of this permit.
4. Approval of the Development Plan and Conditional Use Permit Revision will expire five (5) years after approval, unless before the expiration, substantial physical construction has been completed on the Development Plan and Conditional Use Permit Revision or a time extension has been applied for by the Permittee. The decision-maker with jurisdiction over the project may, upon good cause shown, grant a time extension as specified by City regulations. If the Applicant/Permittee requests a Time Extension, the project may be revised to include updated language to standard conditions and/or may include revised/additional conditions which reflect changed circumstances or additional identified project impacts. Any new fees imposed, and existing fees will be those in effect at the time of the extension request.
5. This Development Plan and Conditional Use Permit Revision shall become effective upon the date of the expiration of the applicable appeal period provided an appeal has not been filed. If an appeal has been filed, the planning permit shall not be deemed effective until final action by the final review authority on the appeal. No entitlement for the use or development shall be granted before the effective date of the planning permit.
6. Pursuant to GMC § 35-315.9 the Applicant/Permittee shall obtain a Land Use Permit to effectuate the Development Plan and Conditional Use Permit Revision five (5) years from the effective date of the Development Plan and Conditional Use Permit Revision. If the required Land Use Permit is not issued within the five year period following the effective date of this Development Plan and Conditional Use Permit Revision, or within such extended period of time as may be authorized in compliance with the City's zoning code,

and an application for an extension has not been submitted to the City PER Department, then the Development Plan and Conditional Use Permit Revision shall be considered void and of no further effect.

7. On the date that a subsequent Development Plan is approved for this site, any previously approved but unbuilt plans must become null and void.
8. This Conditional Use Permit shall become void and be automatically revoked if the development and/or authorized use allowed by this Conditional Use Permit is discontinued for a period of more than 12 months, or within such extended period of time as may be authorized in compliance with the City's zoning code. Any use authorized by this Conditional Use Permit shall immediately cease upon expiration or revocation of this Conditional Use Permit. Any permits approved or issued pursuant to this Conditional Use Permit shall expire upon expiration or revocation of the Conditional Use Permit. Conditional Use Permit renewals must be applied for prior to expiration of the Conditional Use Permit.
9. This approval runs with the land. All rights and obligations of this approval, including the responsibility to comply with these Conditions of Approval, are binding upon applicant's successors in interest unless revoked by the City for cause at a noticed public hearing. These Conditions of Approval may be modified, terminated, or abandoned in accordance with applicable law including, without limitation, the GMC.
10. This permit is granted for the property/parcel(s) of record on which the project is located and shall not be transferred.
11. The Applicant/Permittee are responsible for complying with all conditions of approval contained in this Conditional Use Permit. Any zoning violations concerning the installation, operation, and/or abandonment of the facility are the responsibility of the Owner and the Operator.
12. This approval does not confer legal status on any existing structures or uses on the property unless specifically reviewed and authorized within the project description of this Development Plan and Conditional Use Permit Revision.
13. The City will only issue permits for development, including grading, when the construction documents (e.g., grading plans and building plans) substantially comply with the approved plans. The size, shape, arrangement, use and location of buildings, walkways, parking areas, drainage facilities, and landscaped areas must be developed in substantial conformity with the approved plans. Substantial conformity may be determined by the Director.
14. Any proposed deviations from the exhibits, project description or Project Conditions must be submitted to the Director for review and approval. Any unapproved deviations from the project approval will constitute a violation of the permit approval. The exhibits associated with this permit include the plans dated November 7, 2019 which are all incorporated by reference as if fully set forth.

15. Excluding final DRB approved plans, when exhibits and/or written Project Conditions are in conflict, the written Project Conditions shall prevail. If/when the Project Conditions and Mitigation Measures are in conflict, the written Mitigation Measures must prevail.
16. The project site and any portions thereof shall be sold, leased or financed in compliance with the exhibit(s), project description and the conditions of approval including all related covenants and agreements.
17. No signs of any type are approved with this action unless otherwise specified. All future signage must be reviewed and permitted in compliance with the City's zoning code.
18. Applicant/Permittee agrees to indemnify and hold the City harmless from and against any claim, action, damages, costs (including, without limitation, attorney's fees), injuries, or liability, arising from the City's approval of the Development Plan and Conditional Use Permit Revision as described under Condition #1 above, adoption of the Mitigated Negative Declaration, adoption of the Mitigation Monitoring and Reporting Program, except for such loss or damage arising from the City's sole negligence or willful misconduct. Should the City be named in any suit, or should any claim be brought against it by suit or otherwise, whether the same be groundless or not arising out of the City's approval of the Project, Applicant/Permittee agrees to defend the City (at the City's request and with counsel satisfactory to the City) and will indemnify the City for any judgment rendered against it or any sums paid out in settlement or otherwise. For purposes of this section "the City" includes the City of Goleta's elected officials, appointed officials, officers, and employees.
19. The effectiveness of this Development Plan and Conditional Use Permit Revision will be suspended for the time period that any Project Condition is appealed, whether administratively or as part of a legal action filed in a court of competent jurisdiction. If any Project Condition is invalidated by a court of law, the Project must be reviewed by the City and substitute conditions may be imposed to validate the Development Plan and Conditional Use Permit Revision.
20. In the event than any conditions imposing a fee, exaction, dedication or other mitigation measure is challenged by the Applicant/Permittee in action filed in a court of competent jurisdiction or threatened to be filed, this approval must be suspended pending dismissal of such action, the expiration of the limitation period applicable to such action, or final resolution of such action. If any Project Condition is invalidated by a court of competent jurisdiction, the Project must be reviewed by the City and substitute conditions may be imposed to validate the Development Plan.

## **GENERAL CONDITIONS**

21. Violation of any of these Conditions of Approval is unlawful, prohibited and a violation of the Goleta Municipal Code. The City reserves the right to initiate civil, criminal and/or administrative enforcement, or after notice and a public hearing, to revoke this permit or

modify these Conditions of Approval if it is found that there is a violation of these Conditions of Approval or the Goleta Municipal Code or that the project operates as or causes a public nuisance. This Condition of Approval is not intended to, nor does it limit in any manner whatsoever the ability of the City to take appropriate enforcement actions.

22. The Applicant/Permittee shall be responsible for the completeness and accuracy of all plans, forms and supporting materials submitted in connection with the project. Any error or discrepancies found therein are a violation of this permit.
23. Any new, expanded, or changed use on the project site shall be subject to City review and approval. The City shall determine whether the new, expanded, or changed use on the project site requires the Applicant/Permittee to seek additional approval, permits, or other action by the City. Failure of the Applicant/Permittee to obtain the above-described review and approval of the City is a violation of this permit.
24. During construction, the Applicant/Permittee must promptly remove any graffiti at the Project site.

#### **ENVIRONMENTAL MITIGATIONS/CONDITIONS<sup>1</sup>**

25. The Permittee must comply with all mitigation measures in the Mitigation Monitoring and Reporting Program (MMRP) adopted in the Final Mitigated Negative Declaration for the Project, which are incorporated into these Project conditions by reference.
26. **Lighting Specifications.** The applicant must secure Design Review Board approval of all exterior lighting fixtures to be installed on the project site. The site lighting must be:
  - a. controlled and directed away from the Stream Protection Area and its associated 100-foot buffer;
  - b. low intensity;
  - c. low glare design;
  - d. hooded to direct light downward onto the subject parcel and prevent spill-over onto adjacent parcels; and
  - e. otherwise meet dark sky requirements.

Exterior lighting fixtures must be kept to the minimum lighting level and intensity needed to ensure public safety. These lights must be dimmed after 11 PM to the maximum extent practical without compromising public safety as determined by the Planning and Environmental Review Director, or designee. Lighting fixtures must be appropriate for the architectural style of the structure and surrounding area. The final lighting plan must include identification of all types, sizes, and intensities of wall mounted building lights and landscape accent lighting and a photometric map must be provided. "Moonlighting" type fixtures that illuminate entire tree canopies should also be avoided.

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<sup>1</sup> Some conditions referenced in Final Mitigated Negative Declaration are included under the Public Works Department Heading and as such are not listed under this heading.



The Applicant/Permittee shall secure DRB approval of the lighting plan prior to issuance of the Land Use Permit for the project.

The Planning and Environmental Review Director, or designee, must verify plan compliance before issuance of the Land Use Permit and site installation at time of Final Building Inspection Clearance.

**27. Vegetation Screening.** Vegetation and/or fencing/walls shall be installed such that on-site parking areas and structures are partially screened and in substantial conformity with the approved plans. Vegetation shall be maintained in substantial conformity with the approved Landscape Plans. Substantial conformity must be determined by the Planning and Environmental Review Director, or designee.

**28. Nesting Birds.** At the Permittee's expense, the Applicant/Permittee must retain a City-approved biologist to conduct a survey to determine if special status breeding/nesting birds, breeding/nesting birds protected by the Migratory Bird Treaty Act, and/or raptor nests or roosts exist on or adjacent to the project site within 300 feet. The survey must be conducted prior to commencement of any demolition, grading, and/or construction activities. The survey must establish the breeding and roosting status of any protected birds found on the site or within 300' of the site and designate a 300-foot buffer from any nest/roost if found. The survey must include recommendations to minimize impacts to protected birds during construction, including but not limited to, imposing setbacks, installing fence protection, and restricting the construction schedule. The survey must take into account expected increases and decreases in protected birds over the construction period and must include a map showing known roosting and nesting sites of protected bird species.

Construction within the 300-foot buffer must be avoided during the nesting season (e.g. February 1st through July 31st, but is variable based on seasonal climatic conditions). In addition, construction must not occur until the City-approved biologist has notified the City that all young birds have successfully fledged, and the nests/roosts are no longer active.

The 300-foot buffer(s) must be shown on all grading and construction plans where applicable. The survey must be conducted no more than 14 days and no less than 7 days prior to commencement of any demolition, grading and/or construction activities. Survey conclusions must be reviewed and approved by the Planning and Environmental Review Director, or designee, prior to the issuance of Grading/Building permits.

The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Grading/ Building Permit.

**29. Stream Protection Area.** Grading, construction activities, and structural development shall occur outside of a 100-foot SPA/riparian buffer measured from the top-of-bank or edge of riparian vegetation, whichever is greater, of Las Vegas Creek. Under no circumstances shall

any construction equipment or construction worker vehicles be allowed inside the buffer; mechanized equipment for the purpose of mowing the existing grass field is allowed.

The 100-foot SPA buffer, the location of construction fencing, and fencing materials shall be shown on the project plans submitted for approval of any LUP or the issuance of any building or grading permit for the project and approved by the PER Director. The fence and fencing material must be installed in accordance with the approved plans prior to commencement of grading/ground disturbing activities.

City staff shall review plans, confirm fence installation prior to the commencement of construction activities (including grading), and perform site inspections throughout the construction phase.

30. **Invasive Species.** Non-native, invasive plant species cannot be included in any erosion control seed mixes and/or landscaping plant palette. The California Invasive Plant Council maintains an Inventory database of non-native, invasive plants.

The Applicant/Permittee shall secure DRB approval of the planting plan prior to issuance of the Land Use Permit for the project. The prohibition of the use of non-native invasive plant species must be printed on all Landscape plans.

The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Land Use Permit and that the requirement has been satisfied at the time of Final Building Inspection Clearance.

31. **Geotechnical and Soils Engineering Report.** The owner/applicant shall submit a Geotechnical and Soils Engineering Report related to soil engineering associated with the demolition, grading, and construction of the new synagogue and storage buildings. The recommendation of the Geotechnical and Soils Engineering Report must be incorporated into the Project's grading and building plans. The Geotechnical and Soils Engineering Report must meet the City of Goleta standards for engineering documents and address potential for liquefaction and/or seismic-related settlement and identify appropriate structural-design parameters and soils compaction ratios to address potential hazards.

Grading and building plans must be submitted for review and approval by the Planning and Environmental Review Director, or designee, the Building Official, or designee, and the Public Works Director, or designee, before the City issues grading and building permits.

The Project soils engineer must observe all excavations before soil modification (including placement of compacted soil), gravel backfill, or rebar and concrete and report observations to the City. Building Inspectors and/or Public Works Inspectors will conduct field inspections as needed.

32. **Asbestos Abatement.** Before the City issues a demolition permit for the existing barn/storage garage (if the barn is not relocated to another site), the Applicant/Permittee must notify the Santa Barbara Air Pollution Control District (APCD) and test for asbestos. If asbestos is found, then the Applicant/Permittee must abate and dispose of the materials in a manner consistent with the California Building Code, Santa Barbara County APCD requirements, and any other regulatory requirements.

Prior the issuance of the demolition permit, the Building Official or designee must receive the appropriate paperwork confirming the abatement. The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Land Use Permit.

33. **Trailer Disposal/Removal.** Before the City issues a demolition permit for the existing parking lot, the Applicant/Permittee must provide proof that the trailer has been either (1) properly recycled or disposed at a licensed or certified wrecking yard or disposal site (2) legally transferred to a third party and removed from the property.

Prior the issuance of the demolition permit, the Building Official or designee must receive the appropriate paperwork confirming the proper disposal or legal transfer and removal of the trailer. The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Land Use Permit.

34. **Washing and Fueling of Construction Equipment and Materials.** During construction, washing and fueling of construction equipment and materials (including concrete and paint) can occur only in areas where polluted water and materials can be contained for subsequent removal from the site on a regular basis. The washing and fueling areas shall be located at least 100 feet from any storm drain, waterbody or sensitive biological resources unless permitted by PER Director, or designee, due to site constraints. Areas designated for washing and/or fueling functions must be identified on all plans submitted for issuance of any grading and/or building permit(s).

Prior to the issuance of any grading or building permits, designated fueling and wash off area(s) must be specified on the all grading and building plans. The fueling and wash-off area(s) must be in place throughout all applicable phases of construction.

The Public Works Director, or designee, and the Building Official, or designee, must verify compliance before issuance of the Grading and Building Permits. Subsequently, the Public Works Director, or designee, and the Building Official, or designee, must conduct site inspections during construction to verify compliance.

35. **Noise Attenuation – Construction Noise.** The following measures must be incorporated into grading and building plan specifications to reduce the impact of construction noise:
- a. All construction equipment, fixed or mobile, must be equipped with properly operating and maintained mufflers. Noise attenuation barriers and mufflers on

- grading equipment must be required for construction equipment generating noise levels above 95dB at 50 feet from the source;
- b. Construction noise reduction methods such as but not limited to shutting off idling equipment, installing acoustic barriers around significant sources of stationary construction noise sources, maximizing the distance between equipment and staging areas occupied residential areas, and use of electric air compressors and similar power tools (rather than diesel equipment) must be used when feasible;
  - c. Whenever possible, construction activities must be scheduled to avoid operating several pieces of equipment simultaneously, which causes high noise levels.
  - d. During construction, stationary construction equipment must be placed such that emitted noise is directed away from sensitive noise receivers;
  - e. During construction, stockpiling and vehicle staging areas must be located as far as practicable from noise sensitive receptors;
  - f. Earthmoving equipment operating on the construction site must be as far away from vibration-sensitive sites as possible; and
  - g. Construction hours, allowable workdays, the telephone number of the job superintendent must be clearly posted at all construction entrances to enable surrounding owners and residents to contact the job superintendent directly. If the job superintendent receives a complaint, the superintendent must notify the Planning and Environmental Review Director, or designee, and investigate, take appropriate corrective action and report the action taken to the reporting party and the Planning and Environmental Review Director, or designee.

All signs must be in place before the start of site preparation and grading activities and maintained through to occupancy clearance or Final Building Inspection Clearance. Requirements a-f must be incorporated as text into all plans sets and must be incorporated graphically into all plan submitted for approval of any Land Use Permit, Grading Permit or Building Permit.

The Planning and Environmental Review Director, or designee, must verify compliance before issuance of the Land Use Permit, and before commencement of construction activities, and during construction.

36. **Construction Timing.** Construction activity and equipment maintenance is limited to the hours between 8 AM and 5 PM, Monday through Friday. Exceptions to these restrictions may be made for onsite work for good cause at the sole discretion of the Planning and Environmental Review Director. Exceptions to these restrictions for work in the City Right-of-Way may be made for good cause at the sole discretion of the Public Works Director or designee. Any subsequent amendment to the General Plan noise standard upon which these construction hours are based shall supersede the hours stated herein. No construction can occur on State holidays (e.g., Thanksgiving, Labor Day). Non-noise generating construction activities such as interior plumbing, electrical, drywall and painting (depending on compressor noise levels), are not subject to these restrictions.

At least one sign near each Project site entrance stating these restrictions must be posted on the site. Signs must be a minimum size of 24" x 48." Signs must be in place before the beginning of and throughout grading and construction activities. Violations may result in suspension of permits.

The Planning and Environmental Review Director, or designee, must monitor compliance with restrictions on construction hours and must promptly investigate and respond to all complaints.

- 37. Construction Noise Complaint Line.** The Applicant/Permittee must provide a non-automated telephone number for local residents and employees to call to submit complaints associated with construction noise.

The telephone number must be posted on the site and must be readily visible from adjacent public areas. At least one sign near each Project site entrance along Stow Canyon Road with the phone number must be posted at all times. The Applicant/Permittee must inform the Planning and Environmental Review Director, or designee, of any complaints within one week of receipt of the complaint. Signs must be in place before beginning of and throughout construction activities. Violations may result in suspension of permits.

Building Inspectors and/or Code Compliance staff may periodically inspect and respond to complaints.

- 38. Distancing of Vehicles and Equipment.** Noise and ground-borne vibration construction activities whose specific location on the Project site may be flexible (e.g. operation of compressors and generators, cement mixing, general truck idling) must be conducted as far as possible from the nearest noise and vibration sensitive land use.

The location of vehicles and equipment must be designated on building and grading plans. Equipment and vehicles must remain in the designated location throughout construction activities.

The Planning and Environmental Review Director, or designee, must periodically inspect the site to ensure compliance.

- 39. Construction Notice.** The Applicant/Permittee shall provide all adjacent property owners and residents within 300 feet of the construction site with a construction activity schedule and construction routes 30 days in advance of construction activities in both English and Spanish. Any alterations or additions shall require PER Director, or designee, approval and 30-days prior notification to affected property owners and residents.

The Applicant/Permittee shall submit a copy of the schedule and mailing list to PER Director, or designee. Schedule and mailing list shall be submitted 30 days prior to initiation of any

earth movement. PER compliance monitoring staff shall perform periodic site inspections to verify compliance with activity schedules.

40. **Construction Materials and Equipment Delivery.** Delivery of materials and equipment for construction must be limited to non-school peak traffic periods when either the Goleta Valley Junior High or the Santa Barbara Charter School is in session unless a specific need arises and appropriate measures are developed to the satisfaction of the Planning and Environmental Review Director, or designee.

#### **AGENCY REQUIREMENTS**

41. The Permittee must comply with all of the requirements of the Santa Barbara County Fire Department letter dated June 14, 2019 (attached).
42. The Permittee must comply with the requirements of the Goleta Sanitary District letter dated February 22, 2018 (attached).
43. The Permittee must comply with all the requirements of the Santa Barbara County Air Pollution Control District (APCD) letter dated April 17, 2018 (attached).
44. The Permittee must comply with all of the requirements of Goleta Water District (GWD) to the satisfaction of the GWD, including securing water service connections before the City issues certificate of occupancy. This may also require the provision of Building Permits to demonstrate required retrofits (e.g., within the existing residence and/or the spa/bath house) have been implemented.
45. The Permittee must comply with all of the requirements of Santa Barbara County Flood Control & Water Conservation District (Flood Control), to the satisfaction of Flood Control, regarding development and landscaping within Flood Control easements before the City issues a Land Use Permit.
46. The Permittee must comply with all the requirements of the Marborg Industries letter dated June 13, 2019 (attached).
47. The Permittee must comply with all of the requirements of the County of Santa Barbara's Environmental Health Services Department prior to the issuance of the Land Use Permit (and thereafter, as applicable).

#### **CITY DEPARTMENT CONDITIONS – Public Works Department**

48. All work within the public right-of-way including, without limitation, utilities and grading, must be explicitly identified on the building plans. The Permittee must obtain all necessary encroachment permits from the Public Works Director, or designee, before commencing work

within or over the public right-of-way including without limitation, water meters, backflow devices, signs, and curb/gutter/sidewalk improvements.

49. Prior to the Issuance of the Land Use Permit, the Applicant/Permittee must:

- a. Submit and secure approval of a Storm Water Pollution Prevention Plan (SWPPP) to control off-site discharge of sediments and non-stormwater pollutants during the construction phase, by the Public Works Director or designee if more than one acre of ground disturbance will occur. The SWPPP shall be prepared in compliance with California's Construction General Permit using the CASQA SWPPP template. The SWPPP shall be developed, amended, or revised by a Qualified SWPPP Developer (QSD). If less than one acre of disturbance, applicant shall prepare an Erosion and Sediment Control Plan (ESCP) in compliance with City of Goleta Municipal Code and Standards. The SWPPP or ESCP shall be reviewed and approved by Public Works.
- b. Submit and secure approval of a Final Drainage Study by the Public Works Director or designee. The Final Drainage Study shall incorporate appropriate Best Management Practices (BMPs) to minimize storm water impacts in accordance with the City's Storm Water Management Plan and the City's General Plan. The Study shall include but not be limited to:
  - i. Existing watershed map. Clearly identify all on-site drainage features and full limits of drainage area draining to Las Vegas Creek. Include any offsite drainage and potential run-on from properties to the north and east and clearly indicate all associated drainage patterns.
  - ii. Using the Santa Barbara Unit Hydrograph or approved equivalent, provide Hydrologic calculations for the 2, 5, 10, 25, 50, and 100-year storm events for both pre- and post-construction.
  - iii. Mitigate any increase in peak flow for the 2, 5, 10, 25, 50, and 100-year storm events over existing conditions.
  - iv. Location of existing drainage infrastructure.
  - v. Location of existing Zone AE Special Flood Hazard Areas and associated elevations per the latest FIRM map.
  - vi. Location of all proposed drainage infrastructure and full limits of proposed drainage area draining to Las Vegas Creek. Include any drainage infrastructure and permanent erosion and sediment control features to eliminate the potential for negative impacts to Las Vegas Creek. Clearly identify non-erosive emergency overland escape path of travel from each bio-filtration basin feature.
  - vii. Documentation and information that provides for clear correlation and continuity with the Project's Storm Water Control Plan. The Stormwater Control Plan may be inclusive within the Final Project Drainage Study/Report.

- c. Submit and secure approval of a Storm Water Control Plan (SWCP) to treat and control off-site discharge of stormwater following construction of the project, by the Public Works Director or designee. The SWCP shall be prepared in compliance with the Central Coast Regional Water Board's Post-Construction Stormwater Management Requirements for Development Projects in the Central Coast Region, Resolution No. R3-2013-0032, and shall use the Stormwater Technical Guide for Low Impact Development: Compliance with Stormwater Post-Construction Requirements in Santa Barbara County. The SWCP shall be prepared in clear correlation and continuity with the Project's Final Drainage Study/Report.
- d. Meet the following solid waste design requirements.
  - i. Design trash/recycle area(s) to incorporate Best Management Practices to ensure that organics and other materials are appropriately filtered prior to entering a public storm drain system or natural waterway.
  - ii. Provide trash, recycling, and organics containers so that 50% of the total volume of material generated is to be recycled/mulched/composted and thus diverted from landfill disposal. Trash/recycling/organic areas shall be easily accessed by the consumer/tenant /resident and the trash hauler. Trash hauler shall review the plans and provide concurrence to the City approving the location and accessibility of proposed trash enclosure/s.
  - iii. For commercial projects, "organics" includes both food waste and green waste and must be included in the recycled amount calculation. Green waste (i.e.: landscaping debris) is also a part of the 50% recycling calculation.
  - iv. Provide adequate area for green waste within trash/recycle/organic area(s) or provide a statement if the intent is to have a maintenance company haul off green waste to a certified composting/mulching facility.

50. Prior to Building or Grading Permit(s) Issuance, the Applicant/Permittee must:

- a. Provide a copy of a signed Certificate of Implementation: Construction and Demolition (C&D) Debris Recycling Program to ensure solid waste management compliance with the Green Building Code. This form indicates who will haul all material and an account number for the C&D project from the hauler (typically the site address). This will facilitate the recycling of all construction recoverable/recyclable materials. The project will be required to meet the CalGreen minimum diversion requirement of 65% of the project's solid waste to be diverted from the landfill.
- b. Identify the following on the Building Plans:
  - i. Show all existing survey monuments to be preserved and/or tied out in coordination with the County of Santa Barbara's Surveyor's Office and



- provide a security for the resetting of the survey monuments affected by construction as determined to be warranted by the Public Works Director.
- ii. Show Americans with Disability Act (ADA) path of travel from the Public Right-of-Way to on-site structures.
  - iii. Show all proposed improvements within the Public Right-of-Way, and/or access locations to the Public Right-of-Way.
- c. Receive final Building Plan review and approval from Public Works.
- d. Complete improvements for an unimpeded Right Turn for Eastbound Stow Canyon Road. Improvements shall be installed to allow for an unimpeded right-turn at all times for eastbound traffic on Stow Canyon Road at the Fairview Avenue intersection. Improvements may include, but are not limited to, a no parking zone, red curb painting, and restriping. Improvements shall be constructed to the satisfaction of the Public Works Director, and at the Applicant's cost. Encroachment Permitting requirements necessary for work within the Public Right-of-Way are outlined in Condition #48.
- e. Submit and secure approval of a separate Street Improvement Plan prepared, in compliance with City Municipal Code and City Standards, by a registered civil engineer if determined applicable. This plan may be incorporated into the Building Plan set, with additional Street Improvement Plan sheets provided unbound.
- i. Applicant shall include a plan check deposit for the review of the Street Improvement Plans. The Street Improvement Plan check deposit amount shall be estimated by City staff at the time of submittal.
  - ii. As determined by the Public Works Department, the improvements shall include but not be limited to:
    - 1. Show all roadway street names/easements/dedications as required per the project Conditions of Approval.
    - 2. Show all proposed project associated improvements within the Right-of-Way, including any areas of disturbance for demolition.
    - 3. Minimum 3.5-foot parkway with street trees or other landscaping as approved by the City Manager of Parks and Open Space and/or the Planning Department.
    - 4. Drainage improvements in accordance with the approved final drainage report.
    - 5. Preserve and/or reset survey monuments.
    - 6. Provide pavement preparation and slurry seal of street to repair any damage/trench cuts/restriping, for a minimum of ½ width of the street or as necessary.
    - 7. All driveway accesses shall be ADA compliant, including access ramps with truncated domes, as necessary.

- f. Sign a Public Improvement Agreement. The City will provide a template for review and request signatory information. Once the Agreement has been approved to form by the City Attorney, the City Clerk will distribute to the applicant for signatures and recording.
- g. Submit an Engineer's Estimate, signed and stamped by a registered civil engineer. The scope of the Engineer's Estimate must include, but may not be limited to, material, labor, mobilization, traffic control, monument preservation, and a contingency for the cost of all improvements to be installed by this project.
- h. Provide Securities for construction of improvements prior to execution of the agreement. Securities will be submitted at 100% of the engineer's estimate for the performance of the work and 100% of the engineer's estimate for labor and materials.

51. Prior to Encroachment Permit Issuance, the Applicant/Permittee must:

- a. Obtain a Public Works Encroachment Permit for hauling. Any work in the public right of way requires a Public Works Encroachment Permit, including hauling of soil/debris/materials to and from the project location. Clearly identify proposed haul route. Provide a video, reproducible and with clarity of existing field conditions, along the full length of the proposed haul route. Coordinate with the Public Works Inspector a minimum of 48 hours prior to video recording operations. Clearly identify proposed location for placement of export material. Obtain, and provide to Public Works, a copy of all applicable permits for placement of export material at off-site location(s) within City limits.
  - i. Ensure that all haul trucks, hauling debris, sand, soil, and other loose materials shall be covered and/or maintain a minimum 2' freeboard.
  - ii. Ensure that construction vehicles only use the City's designated Truck Routes, as clearly indicated on the Haul Route Exhibit. All other routes are prohibited.
  - iii. Ensure that construction parking is implemented in a manner that will minimize the potential for traffic interference. Include construction parking designated area(s) on Haul Route exhibit.
  - iv. Clearly identify the proposed area for construction vehicle staging and location(s) for construction vehicle ingress and egress. The ingress/egress pattern shall be identified on the Haul Route Exhibit.
  - v. Stormwater BMPs are applicable for the entire length of the haul route as determined by the project engineer in concurrence with Public Works Director or designee.
- b. Submit a Traffic Control Plan, including pedestrian control for any proposed alterations and/or temporary closures of the existing public access sidewalk, during all phases of construction activities to maintain traffic and pedestrian flow in

accordance with the City Municipal Code and/or Manual on Uniform Traffic Control Devices (MUTCD). Separate Plans may be required for different phases of construction

52. Prior to Certificate of Occupancy, the Applicant/Permittee must:

- a. Submit and secure approval of a Post-Construction Waste Reduction and Recycling Summary (WRRS) Report by the Public Works Director or designee. The report shall substantiate how a minimum 65% diversion goal was met by the project during construction, provides the actual amounts of material generated and what the final diversion rate was, along with either scale house receipts or a summary from the diversion facility used substantiating each load brought to the facility, the tonnage, and the diversion achieved.
- b. Submit and secure approval of a Stormwater and Drainage Operations and Maintenance Plan by the Public Works Director or designee. The Stormwater and Drainage Operations and Maintenance Plan shall:
  - i. Identify all stormwater control measures incorporated into the project's design, including, but not limited to vegetated swales, permeable pavers, permeable pavement, bioretention facilities, detention basins, retention basins, in-line inlet filters, and catch basin filters.
  - ii. Identify staff/contractor responsible for maintenance of these facilities.
  - iii. Document how staff/contractor will be trained in maintenance procedures.
  - iv. Require inspections and maintenance in advance of the first rain of the year (typically by October 30). Annual maintenance reports may be required to be submitted to the City for a period of up to 5 years. Annual maintenance reports may be required to be retained on site and made available to City staff upon request after the 5-year period.
  - v. Provide an estimated budget to cover the cost of two years of operations and maintenance of all stormwater control measures incorporated into the Project.
- c. Enter into a Stormwater Facility Maintenance Agreement with the City developed at the applicant's expense. The City shall develop and provide to the applicant, a draft Maintenance Agreement, subject to recordation with the County, and requiring project owners, and their successors in interest to regularly inspect, maintain, and when necessary repair or replace stormwater treatment, retention and detention Stormwater Control Measures and Best Management Practices that are incorporated into the project, in perpetuity. Stormwater Facility Maintenance Agreements shall include a legal description of the project's location, a vicinity map, and the project's approved Stormwater Operations and Maintenance Plan. Applicant shall also post a Bond in a form acceptable to the City and in an amount of 110% of the estimated costs of maintaining Stormwater Control Measures and

Best Management Practices incorporated into the Project for an initial period of 2-years.

- d. Provide an On-Going Solid Waste & Recycling Program/Plan. This shall be submitted and filed with the Public Works Department. The Plan should include a written description of how the facilities will maintain the State mandated diversion of materials from the landfill, currently 50%. This includes a description of how project owner will implement mandatory commercial recycling and mandatory organics recycling for the use of all tenants/owners and have a plan for the proper disposal of any hazardous materials identified or found on-site.
- e. Submit two hard copy sets of Record Drawings and one electronic signed copy of the Record Drawings for any site revisions completed at the site (i.e., drainage infrastructure, finish grade elevations, parking, retaining walls, parking, stormwater control measures).
- f. Reset all existing survey monuments shall that were preserved and/or tied out in coordination with the County of Santa Barbara's Surveyor's Office.
- g. Repair any trip hazards and/or damaged public improvements (curbs, gutters, sidewalks, pavement markings, signage, striping etc.) that was existing and/or caused by construction along the full frontage of the project. Repairs are subject to the review and approval by the Public Works Director or designee. Repairs shall be completed by the Applicant at no cost to the City.

53. Ongoing Maintenance:

- a. After installation of any drainage improvements or erosion control measures, the applicant shall be responsible for on-going maintenance of all improvements in accordance with the manufacturer's specifications and the approved Operation and Maintenance Plan.
- b. The owner shall provide landscape and hardscape maintenance as identified in the Maintenance Agreements.

**CITY DEPARTMENT CONDITIONS – Planning and Environmental Review Department**

54. The following standards/requirements are general/on-going and must be complied with by the Permittee and/or successors in interest:

- a. Any temporary building, trailer, commercial coach etc. installed or used in connection with the construction of this project must comply with the requirements of Section 35-281 Article III of the City's Inland Zoning Ordinance.

- b. The Permittee is responsible for informing all sub-contractors, consultants, engineers, or other business entities providing services related to the project of their responsibilities to comply with these conditions including, without limitation, the GMC. This includes the requirements that a business license be obtained to perform work within the City as well as the City's construction hour limitations.
- c. Prior to the issuance of the grading permit for the site, the Applicant must develop a soil management plan, to the satisfaction of Santa Barbara County Department of Environmental Health (DEHS), in the unlikely event contaminated soils are encountered in the location where an underground storage tank was previously located and removed in 1993 having met the DEHS closure criteria.
- d. The following requirements apply to the use of the existing well to supplement irrigation water:
  - i. All use shall be approved by the Planning and Environmental Review Director, or designee, the Building Official, or designee, and the Goleta Water District. This includes, and is not necessarily limited to, approval of the irrigation plans before construction and inspection of the system before the irrigation lines are buried.
  - ii. Water well extraction reports shall be maintained and submitted annually to the City of Goleta and the Goleta Water District.

55. Prior to the issuance of the Land Use Permit and building permits, the Applicant/Permittee must:

- a. Execute a landscape installation and maintenance agreement (developed at the applicant's expense) in a form approved by the City Attorney, including a 5-year maintenance period. The agreement must be secured with a performance bond or other surety approved by the City Attorney.
- b. All applicable conditions of approval must be included on all plans submitted for a permit (e.g., grading, building permit)
- c. Conduct a pre-construction meeting to review project conditions for compliance before the start of any work on site. This includes, without limitation, the Permittee, construction team and City representatives from the Planning and Environmental Review, Public Works and Building Departments.
- d. Prior to Land Use Permit issuance, the Applicant/Permittee shall pay all applicable permit processing fees in full.
- e. Record a restrictive covenant as developed by the City at the applicant's expense regarding the fee waiver granted as the project has qualified to receive a 100%

Development Impact Fee (DIF) waiver as a qualified 501(c)3 non-profit organization.

As provided for in Council Resolution 19-43, if in the future a change of non-profit status or acquisition of the property by a for-profit entity occurs, then the payment of Development Impact fees will be necessary. The for-profit entity shall pay the difference between the full amount of DIFs at the time the DIF was discounted and the reduced DIFs previously paid, plus annual adjustments for each year the discount was applied. Each annual adjustment shall be in accordance to a percentage equal to the appropriate Engineering Cost Index as published by Engineering News Record, or its successor publication, for the preceding 12 months for which the ECI is available and such ECI shall be specific to California or the nearest region. Such difference in DIFs shall be paid prior to close of escrow before transfer of ownership or possession. For a change of use to another beneficial project category, the applicant shall pay the difference for any greater amount of DIFs owed under the new beneficial project category.

The table below provides the initial DIFs that would be adjusted based on the ECI, as described above, based on the City of Goleta's Commercial DIF rates for Fiscal Year 2019/2020. The square footage upon which the DIFs are based is 6,694 SF, determined by adding the size of the proposed synagogue (7,293 SF) and the proposed storage building (841 SF) and then subtracting the size of the existing barn/storage garage (1,440 SF) to be removed.

In addition, the impact fees established by the Goleta Union/Santa Barbara Unified School Districts (School Fees) shall also be paid in accordance with the requirements of those entities. This condition also serves as notice pursuant to Government Code Section 66020 (d) that the City of Goleta is imposing development impact fees ("DIFs") and the Applicant/Permittee has 90 days after the imposition of the fees to protest fees.

FEE	RATE	ESTIMATED FEE	TIME DUE	AGENCY
Public Administration (6,694 sq. ft.)	\$495 per kSF	\$3,313.53	Waived*	City
Library (6,694 sq. ft.)	\$154 per kSF	\$1,030.88	Waived*	City
Park (6,694 sq. ft.)	\$1,908 per kSF	\$12,772.15	Waived*	City
Storm Drain (6,694 sq. ft.)	\$2,017 per kSF	\$13,501.80	Waived*	City

Transportation (13 PM Peak Hour Trips)	\$12,631 per PM Peak Hour Trip	\$164,203.00	Waived*	City
Bicycle & Pedestrian (6,694 sq. ft.)	\$496 per kSF	\$3,320.22	Waived*	City
Fire (6,694 sq. ft.)	\$883 per kSF	\$5,910.80	Waived*	City
School Fees –	Set by School Districts	--	BP	Goleta Union & SB Unified School Districts
TOTAL	—	\$204,052.38	—	—

\* Applicant shall record a restrictive covenant on the subject property limiting its use to non-profit purposes in exchange for the DIF reduction. Upon change of non-profit status, all waived fees shall be paid as described in text.

kSF = 1,000 sq. ft.

BP = Building Permit

- f. Secure Design Review Board (DRB) Design Review Approval of the carport, site plan, architecture, landscaping, trash enclosure, and lighting. The trash enclosure design must be as stated in Condition #55.i below.
- g. Secure approval of a composite utility plan from the Director and the DRB. All external/roof mounted mechanical equipment (including solar panels, HVAC condensers, switch boxes, etc.) must be included on all building plans and designing this equipment must be integrated into the structure and/or screened in its entirety from public view.
- h. Screening may include a combination of landscaping and/or fencing/walls. All meters must be concealed by matching the color of the building. All backflow prevention devices and communications equipment must be concealed in an enclosed portion of the building, on top of the building, or within a screened utility area. All transformers and vaults installed within the public right-of way must match existing previous installations at the project unless otherwise approved by the Director and the Public Works Director, or designee, and then completely screened from view.
- i. Secure DRB approval of the design and location of all trash/recycling enclosures. The design must be compatible with the architectural design of the project, of adequate size for trash and recycling containers (at least 50 square feet), and accessible by residents and by the trash hauler. The trash/recycling areas must be

enclosed with a solid wall of sufficient height to screen the area, with a solid gate and a roof, to be maintained in good repair in perpetuity.

- j. Prior to issuance of the Building Permits, the Applicant/Permittee must incorporate energy conservation measures into the building design. All new commercial buildings must comply with the energy efficiency standards set forth in the current California Energy Code and the California Green Building Standards Code.
- k. Obtain all the necessary approvals, licenses and permits and pay all of the appropriate fees as required by the City. Before any permit may be issued by the City, the Permittee must obtain written clearance for each development phase from all Departments/Agencies having conditions or project approval. Such clearance, processed as a post-discretionary Land Use Permit, must indicate that the Permittee has satisfied all pre-construction conditions.
- l. Secure approval of landscaping and irrigation plans from the DRB.
  - i. The landscaping plan must meet the following:
    - 1. Screen ground level mechanical equipment, refuse collectors, storage tanks, generators, pool equipment, and other similar facilities with dense landscaping and/or walls. Materials and finishes must be compatible with the overall design of the project and ancillary buildings.
    - 2. Consist of at least 75% drought-tolerant native or Mediterranean type plants which adequately complement the project design and integrate the site with surrounding land use. The plant material used in the landscape palette must be compatible with the Goleta climate pursuant to Sunset Western Garden Book Zone 24 published by Sunset Books, Inc. Revised and Updated 2012 edition or a more current edition. Landscaping is required to be approved by the DRB.
    - 3. Group plant materials by water needs.
    - 4. Limit turf outside of the SPA (refer to Condition of Approval #29) to less than 20% of the total landscaped area outside of the SPA if proposed under the final landscape plan. Alternatively, artificial turf may be used in place of living grass (this may exceed the 20% maximum).
    - 5. No turf is allowed on slopes of over 4%.
    - 6. Use of extensive mulching (2" minimum) in all landscaped areas to improve the water holding capacity of the soil by reducing evaporation and soil compaction.
  - ii. The irrigation plan must:
    - 1. Demonstrate compliance with the City's Water Conservation regulations and Guidelines for Water Conservation in Landscaping.



2. Utilize efficient irrigation systems which minimize runoff and evaporation and maximize the water which will reach plant roots (e.g., drip irrigation, automatic sprinklers equipped with moisture sensors).
  3. Utilize automatic sprinkler systems that must be set to irrigate landscaping during early morning hours or during the evening to reduce water losses from evaporation. Sprinklers must also be reset to water less often in cooler months and during the rainfall season so that water is not wasted by excessive landscaping irrigation.
  4. Include installation of soil moisture sensing devices to prevent unnecessary irrigation.
- m. The Applicant/Permittee must secure approval of an irrigation plan from the Building Official or designee, if the project meets the threshold outlined in Ordinance 16-04 regarding Water Efficient Landscaping, before issuance of a building permit. The landscape and irrigation shall be installed per plan prior to Final Building Inspection Clearance.
- n. Secure approval of an exterior lighting plan and photometric light study plan from the DRB.
- o. Secure the construction site with a minimum 6-foot high fence. The fence must be covered with a material approved by the Director to minimize dust from leaving the site.
- p. Enter into an agreement with the City regarding compliance monitoring and submit the fees cover full costs of compliance monitoring. All costs associated with the development, review, and execution of the compliance monitoring contract is the sole responsibility of the Applicant/Permittee.
- q. Obtain all Building Permits required by Title 15 of the Goleta Municipal Code prior to the construction, erection, moving, alteration, enlarging, rebuilding of any building, structure, or improvement, or any other action(s) as required.
56. During grading and construction activities, the Permittee, to the satisfaction of the Director, must:
- a. Prevent construction and/or employee trash, as well as dust, from blowing offsite by:
    - i. Providing covered receptacles on-site before commencement of any grading or construction activities;
    - ii. Picking up waste weekly or more frequently as directed by the City; and
    - iii. Designating and providing to the Director the name and contact information of the project foreman who will monitor construction

trash/waste and dust. Additional covered receptacles must be provided as determine necessary by the Director.

- iv. Watering the site, install appropriate fencing, and/or utilize other tactics to control dust.

57. Prior to the issuance of the Certificate of Occupancy, the Permittee must:

- a. Complete construction of all new and approved buildings and parking lot improvements in accordance with approved plans.
- b. Install all required trash enclosures in accordance with approved plans.
- c. Screen all mechanical equipment in accordance with approved plans.
- d. Install all landscaping and irrigation in accordance with approved plans.
- e. Screen all new utility service connections and above-ground mounted equipment such as backflow devices, etc. from public view and/or painted in a soft earth tone color so as to blend in with the project (red is prohibited) in accordance with approved plans.
- f. Complete removal/abandonment of driveways serving the residence and any temporary construction driveways in accordance with the approved plans.
- g. Secure final clearance from all applicable Agencies/City Departments as needed.
- h. Obtain a business license for the preschool/daycare if not already in place.

---

By signing this document, Rabbi Mendel Loschak, on behalf of Chabad of Santa Barbara, certifies that he read, understands, and agrees to the Project Conditions listed in this document.

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Mendel Loschak  
Rabbi, Chabad of Santa Barbara

Date

**Attachments:**

- A. Santa Barbara County Fire Department letter dated June 14, 2019
- B. Santa Barbara County Fire Department Red Curb Site Plan dated June 11, 2019
- C. Goleta Sanitary District letter dated February 22, 2018
- D. Santa Barbara County Air Pollution Control District (APCD) letter dated April 17, 2018
- E. Marborg Industries letter dated June 13, 2019

-End of Conditions-

# Memorandum

**DATE:** June 14, 2019

**TO:** Chris Noddings  
Planning and Environmental Review  
City of Goleta

**FROM:** Glenn Fidler, Captain  
Fire Department

**SUBJECT:** APN: 077-170-044; Permit: 18-031-CUPAM-DP  
Site: 6045 Stow Canyon Road, Goleta  
Project: Conditional Use Permit Amendment – New Synagogue



*This Condition Memorandum Supersedes the Previous Condition Memorandum  
Dated December 13, 2018*

The above project is located within the jurisdiction of the Santa Barbara County Fire Department. The Fire Prevention Division must be notified of any changes to the project proposal. A change in the project description may cause a change in conditions to be imposed.

## **NO CONDITIONS FOR CONDITIONAL USE PERMIT AMENDMENT**

### **CONDITIONS FOR DEVELOPMENT PLAN**

#### **PROJECT DEVELOPMENT**

#### **PRIOR TO CONSTRUCTION**

1. All access ways shall be installed, made serviceable and maintained for the life of the project.
  - Access shall be as shown on plans dated May 23, 2019, received June 7, 2019. Including no parking areas.
  - Driveway shall have a minimum width of 20 feet.
  - Surface shall be paved.
  - Access ways shall be unobstructed and extended to within 150 feet of all portions of the exterior walls of the first story of any building.
  - Dead-end access exceeding 150 feet shall terminate with a fire department approved turnaround.
  - A minimum of 13 feet, 6 inches of vertical clearance shall be provided and maintained for the life of the project for emergency apparatus access.
  - Reference Santa Barbara County Fire Department Development Standard #1.\*

#### **PRIOR TO OCCUPANCY CLEARANCE**

2. Designated fire lanes shall include red curbs and signs indicating "Fire Lane – No Stopping" placed as required by the fire department. Refer to current adopted California Fire Code.

3. Portable fire extinguishers are required and shall be in accordance with the current adopted Santa Barbara County Code Chapter 15.
4. An automatic fire sprinkler system shall be installed.
  - Fire sprinkler plans shall be approved by the fire department prior to installation.
  - Water systems shall be installed exactly as the approved plans dictate. No changes or modifications to these plans shall take place without prior fire department approval.
  - No work shall be covered or otherwise rendered inaccessible or unviewable prior to inspection by the fire department.
5. An automatic fire or emergency alarm system shall be installed.
  - Fire alarm system shall meet Santa Barbara County Fire Department requirements.
  - Automatic fire or emergency alarm system plans shall be approved by the fire department.
  - Alarm panel locations and annunciator graphics shall be approved by fire department prior to installation.
6. Address numbers shall be a minimum height of 12 inches.
7. Address numbers shall be as required by the fire department.
  - Address number locations shall be approved by the fire department.
  - Address numbers shall be a color contrasting to the background color.
  - The address numbers shall be elevated at least three feet from the ground for clear visibility and easy directional identification.
  - The numbers shall be visible from the access road when travelling in either direction.
  - If the driveway is over 150 feet in length or the building is obstructed from view at the access road and/or driveway, numbers shall be posted at all road and driveway intersections as is necessary.
8. Access way entrance gates shall conform to fire department requirements.
9. A Knox Box entry system shall be installed for the building. A spare key shall be provided for the Knox Box entry system.\*

#### **ADVISORY**

10. The applicant will be required to pay the City of Goleta Fire Development Impact Fees directly to the City of Goleta.
11. All standard fire department conditions and current codes shall apply at time of development.

As always, if you have any questions or require further information, please call me at 805-681-5528 or 805-681-5523.

GF:mkb

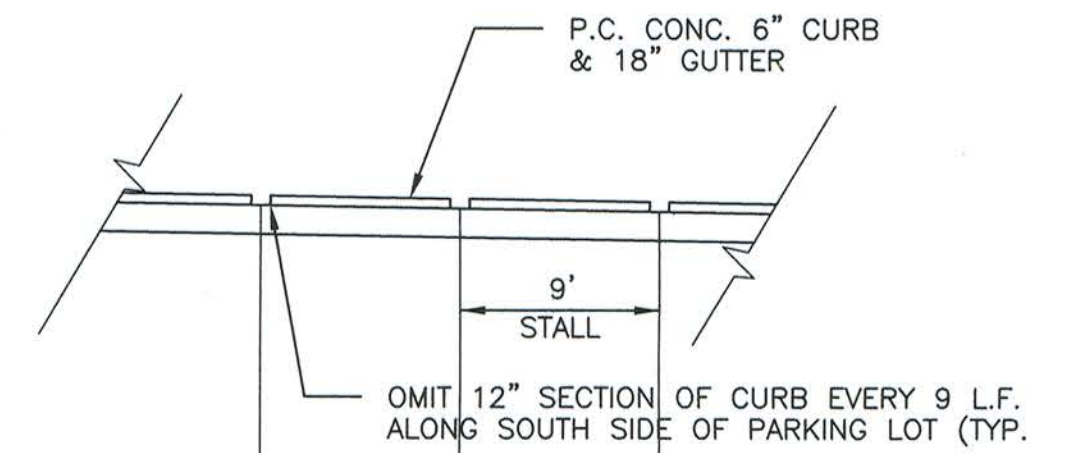
Attachment: plans dated May 23, 2019, and received June 7, 2019.



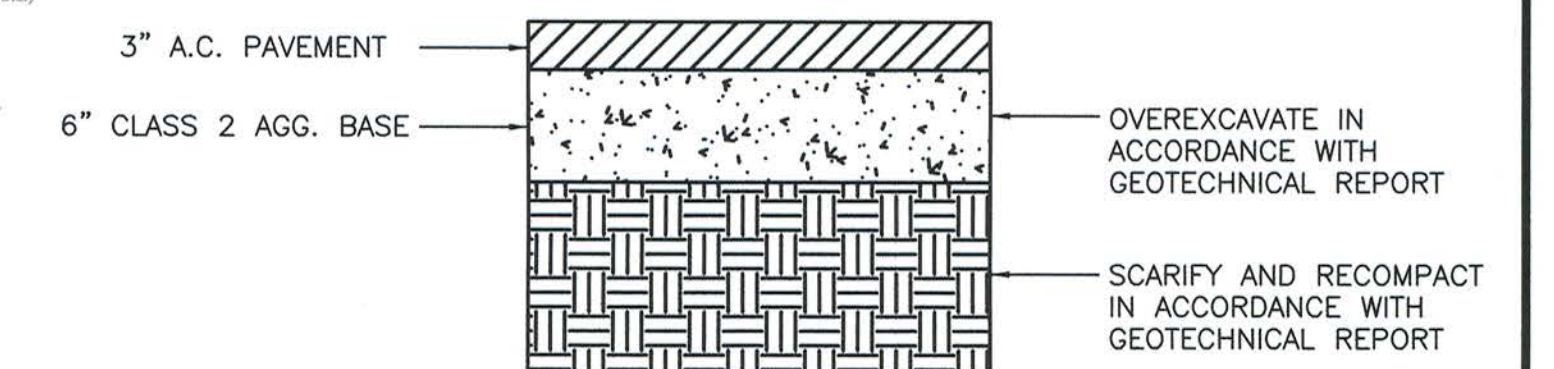
Conditions of Approval  
Attachment B

CONSTRUCTION NOTES

1. CONSTRUCT P.C. CONCRETE SIDEWALK.
2. CONSTRUCT PERMEABLE PAVEMENT PER DETAIL ON SHEET 1.
3. REMOVE EXISTING DRIVEWAY. SAWCUT AT GUTTER FL AND REPLACE WITH CURB, PARKWAY & SIDEWALK PER CITY OF GOLETA STD. DETAILS 3-100 & 4-040.
4. CONSTRUCT DROP CURB DRIVEWAY (W=20") PER CITY OF GOLETA STD. DETAILS 3-090.
5. CONSTRUCT 6" P.C. CONC. CURB PER CITY OF GOLETA STD. DETAIL 3-100.
6. CONSTRUCT 12" WIDE CURB CUT PER DETAIL 1 ON THIS SHEET.
7. CONSTRUCT 8" CMU RETAINING WALL.
8. CONSTRUCT P.C. CONCRETE PAVING.
9. LANDSCAPE AREA PER LANDSCAPE ARCHITECT'S PLANS.
10. PROTECT EXISTING WATER WELL IN PLACE.
11. CONSTRUCT BIORETENTION BASIN PER DETAIL 2 ON SHEET 1.
12. CONSTRUCT UNGROUTED ROCK RIP RAP.
13. CONSTRUCT PEDESTRIAN RAMP PER COUNTY OF SB STD. DETAIL 5-030.
14. CONSTRUCT MASONRY TRASH ENCLOSURE WITH ROOF.
15. CONSTRUCT ASPHALT PAVEMENT DRIVE AISLES PER DETAIL 2 ON THIS SHEET.



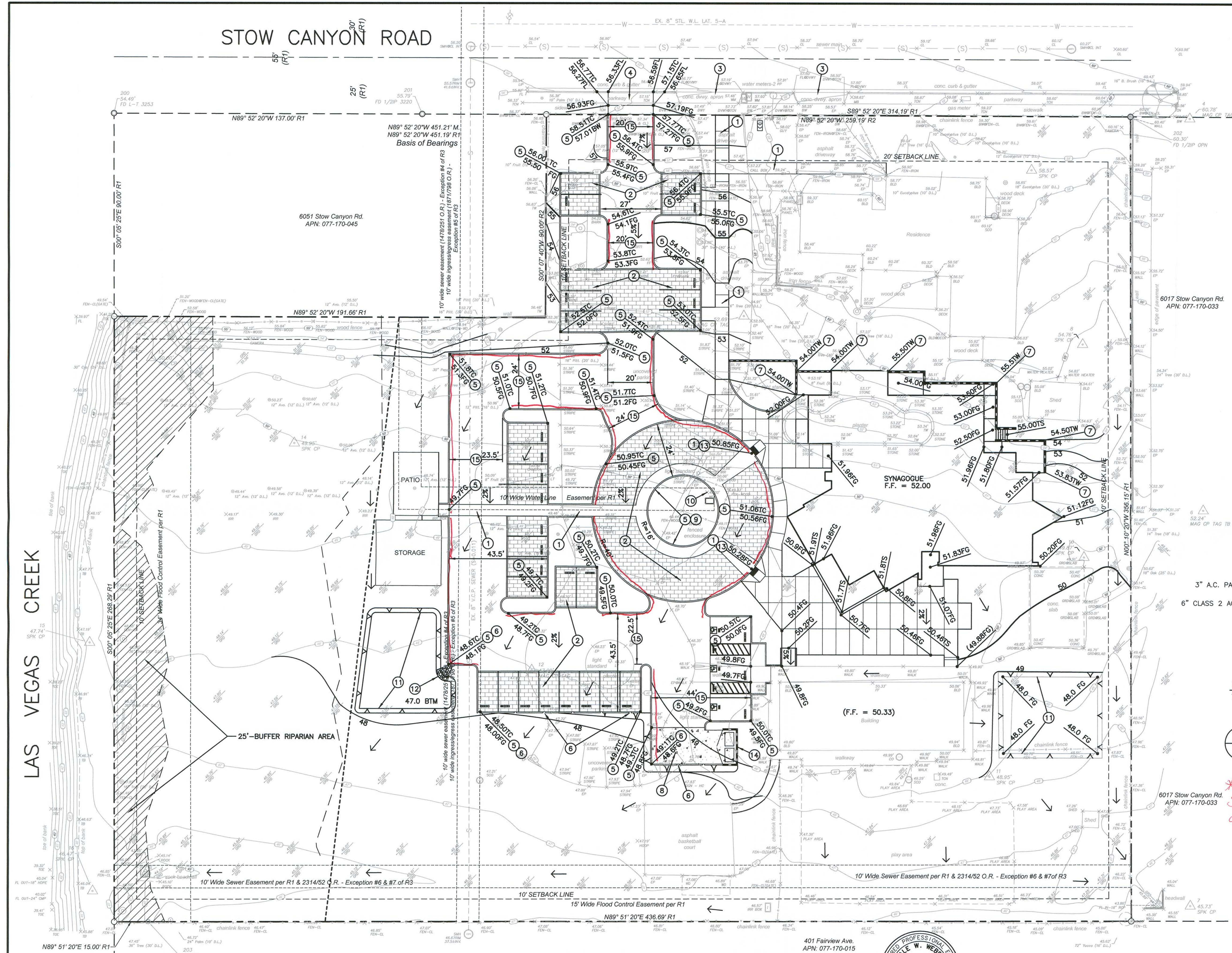
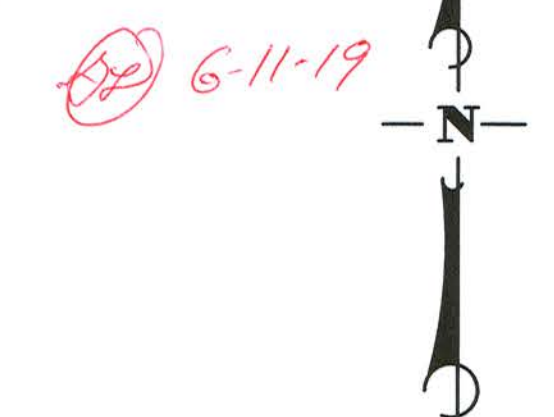
1 CURB CUT DETAIL  
NO SCALE NO SCALE



NOTES  
1. FINAL STRUCTURAL SECTION WILL BE DETERMINED BY FIELD R-VALUE TESTS CONDUCTED BY THE SOILS ENGINEER.

2 TYP. PAVEMENT SECTION  
NO SCALE

\*RED CURBING & PROPER SIGNAGE  
SHALL BE REQUIRED @ TIME  
OF DEVELOPMENT  
6-11-19



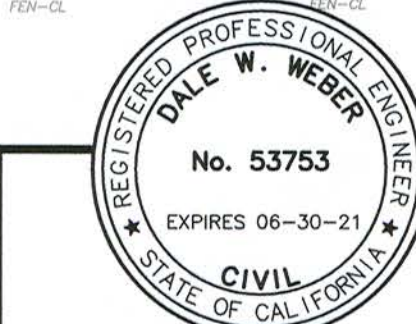
REFERENCES:  
DRAWING FILE:  
DATA FILE:  
PROJECT FILE:  
VIEW NAME:  
XREF NAME(S):

NO.	DATE	REVISIONS	APPD.

DIAL TOLL FREE  
1-800-227-2600  
AT LEAST TWO DAYS  
BEFORE YOU DO  
UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA

MAC Design Associates  
CIVIL ENGINEERING • LAND PLANNING • BRIDGE DESIGN  
1933 CLIFF DRIVE, SUITE 6, SANTA BARBARA, CALIF. 93101 (805) 957-4748

DESIGN DWG  
DRAWN TLA  
CHECKED  
DALE W. WEBER DATE: 5-23-19  
PROJECT ENGINEER  
R.C.E. 53753 (EXP. 6-30-21)



CITY OF GOLETA, CALIFORNIA  
REVIEWED BY:  
FOR  
DATE

PRELIMINARY GRADING & DRAINAGE PLAN  
CHABAD OF SANTA BARBARA  
4065 STOW CANYON ROAD  
CITY OF GOLETA, CALIFORNIA  
WORK ORDER W.O. 0465  
SHEET 3 OF 4  
FILE#  
JUN - 7 2019  
Santa Barbara County Fire Dept  
Fire Prevention Division



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GOVERNING BOARD

STEVEN T. MAJOEWSKY, PRESIDENT  
GEORGE W. EMERSON  
SHARON ROSE  
ROBERT O. WAGENECK  
JERRY D. SMITH



A PUBLIC AGENCY  
[www.goletasanitary.org](http://www.goletasanitary.org)

Conditions of Approval  
Attachment C

GENERAL MANAGER/DISTRICT ENGINEER  
STEVE D. WAGNER, PE

February 22, 2018

Steve Fort  
Suzanne Elledge Planning & Permitting Services, Inc.  
1625 State St., Suite 1  
Santa Barbara, CA 93101

**SUBJECT: Sewer Service Availability  
Proposed Sewer Service Connection for Construction of a new 6,000  
square foot synagogue.  
A.P.N. 077-170-044 at 6045 Stow Canyon Road**

Dear Mr. Fort:

This letter is in response to your recent inquiry dated February 14, 2018 relative to the availability of sewer service for the above-mentioned property.

The subject property, as shown on the attached parcel map, is currently within the Goleta Sanitary District service area (sphere of influence) and is annexed to the District. Based on the District's preliminary understanding from the information you provided, your client proposes to construct and connect to the District's sewage collection facilities a new 6,000 square foot synagogue. Currently the existing parcel is being served for, a single family residence and an assembly / school class rooms building under GSD Permit numbers 3140 and 3360 connected on July 30, 1998.

Please be advised that adequate sewage collection, treatment, and disposal capacity is currently available to serve the proposed project and that the District does not currently have a moratorium or similar restriction on new sewer connections. Subject to the terms specified in this letter, and upon satisfaction of the conditions set forth in the attached Exhibit "A", the District will issue a sewer connection permit and authorize the connection of the project to the District's sewer collection system.

Although adequate sewer capacity is currently available to serve the project, issuing this letter does not guarantee sewer service by the District or reserve capacity for the project. The District provides all new sewer service on a first-come, first-serve basis, as determined from the date on which the connection permit is issued. The District cannot predict the pace of future development in the community and cannot anticipate the demand for new sewer service. In addition the District is unable to predict what new regulatory requirements might be imposed in the future by Federal, State and/or local agencies, or exactly what effects said requirements might have on the District's ability to accept any new connections.

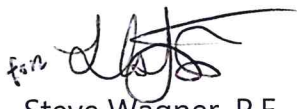
This letter does not constitute a sewer connection permit for the proposed project, but sets forth the terms on which a connection permit is issued. By providing this letter, the responsibility or liability for sewer service or matters pertaining to this project will not be the responsibility of the District.

Please note that the District's current assessment with respect to capacity availability, along with terms and conditions stipulated in Exhibit "A" for this project, are valid for two years from the date of this letter. At the end of the two year period, the applicant, if still interested in the District's availability of service, must submit in writing a request for reassessment of its service conditions and capacity availability outlined in this letter.

If you have any questions regarding this matter please call Mr. Luis Astorga at this office.

Very truly yours,

**GOLETA SANITARY DISTRICT**

A handwritten signature in black ink, appearing to read 'Steve Wagner', is written over a horizontal line.

Steve Wagner, P.E.

General Manager/District Engineer

KSA: JR

Attachments

1. Exhibit A
2. Parcel Map

cc: Luis Astorga, Goleta Sanitary District



## **EXHIBIT "A"**

### **TERMS AND CONDITIONS**

Applicant shall comply with all applicable District provisions of its Standards and Ordinances.

Applicant/owner(s) must submit for the District's review, approval and files, a complete copy of the final building structure site, floor and plumbing plans to the District. The District will contact the applicant after plans are reviewed. The City of Goleta Building and Safety Division may require that you apply for additional permits.

The site plans need to show the proposed 6" diameter building structure sewer connection, building floor and rim elevation of the upstream manhole from the proposed connection to the structure.

A sampling manhole, per District Standards, if required after our review of the project plans, needs to be shown on the plans and constructed and installed at the property line or within the private property.

A grease interceptor, if required after review of the project plans, needs to be shown on the plans and installed outside the building within the private property.

Building structures on the lot, not directly connected to a public sewer, will have to be separately connected with the public sewer upon subsequent subdivision of the lot.

Each property has to be separately connected to District facilities.

If there is an inability to achieve gravity flow from the building structure to the District's sewage collection facilities, an injector pump system design will need to be submitted to the City of Goleta Building and Safety Division for approval prior to connection of any portion of your force main sewer system. The design must include dual pump and alarm system.

A backflow preventer encased in a concrete vault with a metal lid, embossed with "sewer" or "clean-out", must be installed within the private property whenever the residential interior plumbing fixtures are lower than the District's upstream manhole rim elevation. This manhole is the next immediate manhole upstream from the structure sewer service connection to the main sewerline.

Once the plans have been received, reviewed and accepted, the District will stamp the plans approved. A sewer connection permit may be obtained by the applicant once they have paid all applicable fees, posted all required bonds and satisfied all applicable ordinances, regulations, standards and requirements of the District and any other local, state or federal agency with jurisdiction over the project.

As of the date of this letter, the required District fees are as follows:

Connection Fees:

Single Family Dwelling Unit: **\$2,058.00 / Unit**

Apartment, Duplex, Mobile Home Space, Condominium Unit: **\$1,441.00 / Unit**

Connection fees for commercial/industrial and other non-residential establishments are based on the number of equivalent residential units (ERUs) of the proposed development. The number of ERUs are defined as the ratio of the proposed total number of plumbing fixtures of the proposed development and that of a single family dwelling (20 fixture units per dwelling). The connection fee for the proposed development is determined by multiplying the proposed ERUs by the connection fee of a single family dwelling (\$2,058 at present). Under no circumstance shall the fee be less than that of a single family dwelling.

Permit fee: **\$170.00** (for project)

Permit fee: **\$170.00** (for cleanout installation at property line only, inspection fee waived)

Industrial Waste Control Annual Permit fee: **\$226.00 to \$1,817.00** (Based on Discharger Classification)

Inspection fee: **\$170.00** (per residential or commercial building structure connection)

Inspection fee: **\$226.00** (per industrial/manufacturing building structure connection)

Inspection fee: **\$454.00** (per 100 feet of mainline extension)

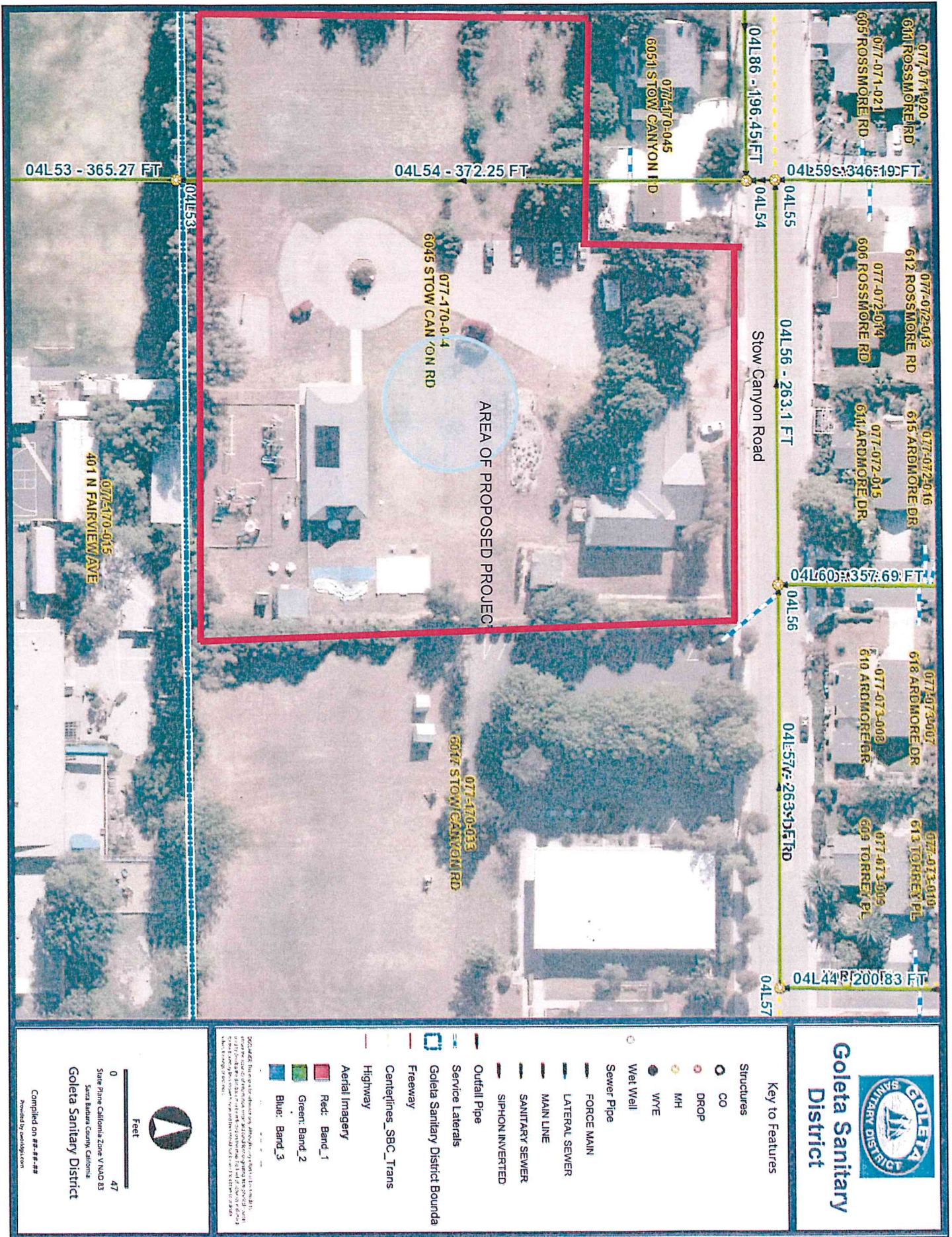
Plan check and review fee: **\$114.00** per hour (**\$114.00** minimum fee)

Deposit, as required **\$500.00**

Credit will be given for the existing connection and existing plumbing fixtures.

These fees are subject to periodic adjustments and applicant shall pay the fees in effect at the time application is made for a connection permit.





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## Conditions of Approval Attachment D

April 17, 2018

Chris Noddings  
City of Goleta  
Planning and Environmental Services  
130 Cremona Drive, Suite B  
Goleta, CA 93117

**Re: APCD Suggested Conditions on Chabad of Santa Barbara New Synagogue, 18-031-DP-CUPAM**

Dear Mr. Noddings:

The Air Pollution Control District (APCD) has reviewed the referenced project, which consists of a proposed revision to an existing conditional use permit (92-CP-018) to continue and expand synagogue operations and approval of development plan to allow construction of a new 7,389 square foot (SF) structure that will serve as a new synagogue. Existing development on the site includes a 2,455 SF building currently used for educational purposes and synagogue services, a 4,093 SF single family dwelling, a 352 SF bathhouse, and 1,440 SF storage garage that is to be demolished. New (permeable) parking, landscaping, and hardscape are also proposed. Maximum attendance at the site is estimated at approximately 120 people during event and services related to religious holidays. The educational component of the project would not operate during services or events. Grading is estimated at 525 cubic yards of cut and 575 cubic yards of fill. The subject property, a 3.29-acre parcel zoned DR-4.6 Design Residential and identified in the Assessor Parcel Map Book as APN 077-170-044, is located at 6045 Stow Canyon Road in the City of Goleta.

Air Pollution Control District staff offers the following suggested conditions:

1. Standard dust mitigations (**Attachment A**) are recommended for all construction and/or grading activities. The name and telephone number of an on-site contact person must be provided to the APCD prior to grading/building permit issuance.
2. APCD Rule 345, *Control of Fugitive Dust from Construction and Demolition Activities* establishes limits on the generation of visible fugitive dust emissions at demolition and construction sites. The rule includes measures for minimizing fugitive dust from on-site activities and from trucks moving on- and off-site. Please see [www.ourair.org/wp-content/uploads/rule345.pdf](http://www.ourair.org/wp-content/uploads/rule345.pdf).
3. The State of California considers particulate matter emitted by diesel engines carcinogenic. Therefore, during project grading, construction, and hauling, construction contracts must specify that contractors shall adhere to the requirements listed in **Attachment B** to reduce emissions of particulate matter (as well as of ozone precursors) from diesel equipment. Recommended measures should be implemented to the maximum extent feasible.
4. All portable diesel-fired construction engines rated at 50 bhp or greater must have either statewide Portable Equipment Registration Program (PERP) certificates or APCD permits prior to grading/building permit issuance. Construction engines with PERP certificates are exempt from APCD permit, provided they will be on-site for less than 12 months.

5. The applicant is required to complete and submit an **Asbestos Demolition/Renovation Notification or an EXEMPTION** from Notification for Renovation and Demolition (APCD Form ENF-28 or APCD Form ENF-28e), which can be downloaded at [www.ourair.org/compliance-forms/](http://www.ourair.org/compliance-forms/) for each regulated structure to be demolished or renovated. Demolition notifications are required regardless of whether asbestos is present or not. The completed exemption or notification should be presented, mailed, or emailed to the Santa Barbara County Air Pollution Control District with a minimum of 10 working days advance notice prior to disturbing asbestos in a renovation or starting work on a demolition. The applicant should visit [www.ourair.org/asbestos/](http://www.ourair.org/asbestos/) to determine whether the project triggers asbestos notification requirements or whether the project qualifies for an exemption.
6. Natural gas-fired fan-type central furnaces with a rated heat input capacity of less than 175,000 Btu/hr and water heaters rated below 75,000 Btu/hr must comply with the emission limits and certification requirements of APCD Rule 352. Boilers, water heaters, and process heaters (rated between 75,000 and 2.0 million Btu/hr) must comply with the emission limits and certification requirements of APCD Rule 360. Note: Units fired on fuel(s) other than natural gas still need to be certified under Rule 360. Please see [www.ourair.org/wp-content/uploads/rule352.pdf](http://www.ourair.org/wp-content/uploads/rule352.pdf) and [www.ourair.org/wp-content/uploads/rule360.pdf](http://www.ourair.org/wp-content/uploads/rule360.pdf) for more information.
7. At a minimum, prior to occupancy, any feasible greenhouse gas reduction measures from the following sector-based list should be applied to the project:
  - Energy use (energy efficiency, low carbon fuels, renewable energy)
  - Water conservation (improved practices and equipment, landscaping)
  - Waste reduction (material re-use/recycling, composting, waste diversion/minimization)
  - Architectural features (green building practices, cool roofs)
  - Transportation (pedestrian- and bicycle-friendly features such as sidewalks and bike racks)
8. The application of architectural coatings, such as paints, primers, and sealers that are applied to buildings or stationary structures, shall comply with APCD Rule 323.1, *Architectural Coatings* that places limits on the VOC-content of coating products.
9. Asphalt paving activities shall comply with APCD Rule 329, *Cutback and Emulsified Asphalt Paving Materials*.

If you or the project applicant have any questions regarding these comments, please feel free to contact me at (805) 961-8890 or via email at [BarhamC@sbcapcd.org](mailto:BarhamC@sbcapcd.org).

Sincerely,



Carly Barham  
Technology and Environmental Assessment Division

Attachments: Fugitive Dust Control Measures  
Diesel Particulate and NO<sub>x</sub> Emission Measures

cc: Steve Fort, SEPPS  
TEA Chron File



**ATTACHMENT A**  
**FUGITIVE DUST CONTROL MEASURES**

These measures are required for all projects involving earthmoving activities regardless of the project size or duration. Proper implementation of these measures is assumed to fully mitigate fugitive dust emissions.

- During construction, use water trucks or sprinkler systems to keep all areas of vehicle movement damp enough to prevent dust from leaving the site. At a minimum, this should include wetting down such areas in the late morning and after work is completed for the day. Increased watering frequency should be required whenever the wind speed exceeds 15 mph. Reclaimed water should be used whenever possible. However, reclaimed water should not be used in or around crops for human consumption.
- Minimize amount of disturbed area and reduce on site vehicle speeds to 15 miles per hour or less.
- If importation, exportation and stockpiling of fill material is involved, soil stockpiled for more than two days shall be covered, kept moist, or treated with soil binders to prevent dust generation. Trucks transporting fill material to and from the site shall be tarped from the point of origin.
- Gravel pads shall be installed at all access points to prevent tracking of mud onto public roads.
- After clearing, grading, earth moving or excavation is completed, treat the disturbed area by watering, or revegetating, or by spreading soil binders until the area is paved or otherwise developed so that dust generation will not occur.
- The contractor or builder shall designate a person or persons to monitor the dust control program and to order increased watering, as necessary, to prevent transport of dust offsite. Their duties shall include holiday and weekend periods when work may not be in progress. The name and telephone number of such persons shall be provided to the Air Pollution Control District prior to grading/building permit issuance and/or map clearance.

**Plan Requirements:** All requirements shall be shown on grading and building plans and/or as a separate information sheet listing the conditions of approval to be recorded with the map. **Timing:** Requirements shall be shown on plans prior to grading/building permit issuance and/or recorded with the map during map recordation. Conditions shall be adhered to throughout all grading and construction periods.

**MONITORING:** Lead Agency shall ensure measures are on project plans and/or recorded with maps. Lead Agency staff shall ensure compliance onsite. APCD inspectors will respond to nuisance complaints.

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**ATTACHMENT B**  
**DIESEL PARTICULATE AND NO<sub>x</sub> EMISSION REDUCTION MEASURES**

Particulate emissions from diesel exhaust are classified as carcinogenic by the state of California. The following is a list of regulatory requirements and control strategies that should be implemented to the maximum extent feasible.

The following measures are required by state law:

- All portable diesel-powered construction equipment shall be registered with the state's portable equipment registration program OR shall obtain an APCD permit.
- Fleet owners of mobile construction equipment are subject to the California Air Resource Board (CARB) Regulation for In-Use Off-Road Diesel Vehicles (Title 13, California Code of Regulations (CCR), §2449), the purpose of which is to reduce oxides of nitrogen (NO<sub>x</sub>), diesel particulate matter (DPM), and other criteria pollutant emissions from in-use off-road diesel-fueled vehicles. Off-road heavy-duty trucks shall comply with the State Off-Road Regulation. For more information, see [www.arb.ca.gov/msprog/ordiesel/ordiesel.htm](http://www.arb.ca.gov/msprog/ordiesel/ordiesel.htm).
- Fleet owners of mobile construction equipment are subject to the CARB Regulation for In-Use (On-Road) Heavy-Duty Diesel-Fueled Vehicles (Title 13, CCR, §2025), the purpose of which is to reduce DPM, NO<sub>x</sub> and other criteria pollutants from in-use (on-road) diesel-fueled vehicles. On-road heavy-duty trucks shall comply with the State On-Road Regulation. For more information, see [www.arb.ca.gov/msprog/onrdiesel/onrdiesel.htm](http://www.arb.ca.gov/msprog/onrdiesel/onrdiesel.htm).
- All commercial off-road and on-road diesel vehicles are subject, respectively, to Title 13, CCR, §2449(d)(3) and §2485, limiting engine idling time. Idling of heavy-duty diesel construction equipment and trucks during loading and unloading shall be limited to five minutes; electric auxiliary power units should be used whenever possible.

The following measures are recommended:

- Diesel equipment meeting the CARB Tier 3 or higher emission standards for off-road heavy-duty diesel engines should be used to the maximum extent feasible.
- On-road heavy-duty equipment with model year 2010 engines or newer should be used to the maximum extent feasible.
- Diesel powered equipment should be replaced by electric equipment whenever feasible.
- Equipment/vehicles using alternative fuels, such as compressed natural gas (CNG), liquefied natural gas (LNG), propane or biodiesel, should be used on-site where feasible.
- Catalytic converters shall be installed on gasoline-powered equipment, if feasible.
- All construction equipment shall be maintained in tune per the manufacturer's specifications.
- The engine size of construction equipment shall be the minimum practical size.
- The number of construction equipment operating simultaneously shall be minimized through efficient management practices to ensure that the smallest practical number is operating at any one time.
- Construction worker trips should be minimized by requiring carpooling and by providing for lunch onsite.

**Plan Requirements:** All requirements shall be shown on grading and building plans and/or as a separate information sheet listing the conditions of approval to be recorded with the map. **Timing:** Requirements shall be shown on plans prior to grading/building permit issuance and/or recorded with the map during map recordation. Conditions shall be adhered to throughout all grading and construction periods.

**MONITORING:** Lead Agency shall ensure measures are on project plans and/or recorded with maps. Lead Agency staff shall ensure compliance onsite. APCD inspectors will respond to nuisance complaints. 193

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June, 13, 2019

Ben Krintzman  
Designer  
Jeffrey Berkus Architects, Inc.  
924 Anacapa Street, Suite 1B  
Santa Barbara, CA 93101

Re: 6045 Stow Canyon Rd.

Dear Ben,

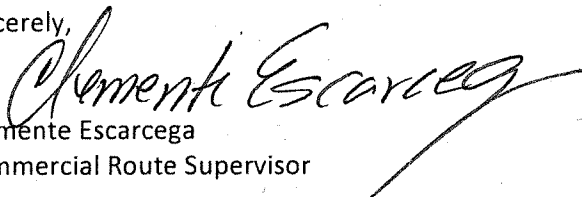
In response to your e-mail, this letter will serve as MarBorg's agreement that the following is correct for us to be able to service containers:

- Roadway drive/pavers are durable enough to support the weight of MarBorg heavy trash trucks
- Truck is 14' tall and will require proper tree maintenance to allow for clearance
- 6" wide by 8" tall curb to be provided at interior base of trash enclosure to ensure dumpsters don't collide with interior face of enclosure wall.

The enclosure meets the standards of MarBorg Industries.

If I can be of further service, please contact me.

Sincerely,

  
Clemente Escarcega  
Commercial Route Supervisor

P.O. Box 4127  
728 Yanonali Street  
Santa Barbara  
California 93103  
Phone 805-963-1852  
Fax 805-963-0550



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## ATTACHMENT 1

### EXHIBIT 3

#### General Plan/Coastal Land Use Plan Consistency

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### **EXHIBIT 3**

#### **GENERAL PLAN/COASTAL LAND USE PLAN CONSISTENCY ANALYSIS FOR NEW SYNAGOGUE BUILDINGS FOR CHABAD OF SANTA BARBARA**

The Project is consistent with the Goleta General Plan/Coastal Land Use Plan (GP/CLUP) as follows:

##### **Land Use Element**

The project includes high quality construction that will improve the existing site and provide expanded services to synagogue attendees and is consistent with the Land Use Element. Land Use Element Figure 2-1 designates the project site as Single-Family (R-SF). The R-SF land use designation is intended to identify and protect appropriately located land areas for family living in low-density residential environments. Uses in the R-SF designation include, but are not limited to, single-family attached and detached dwellings, religious institutions, small-scale residential care facilities and small-scale day care centers. The project would expand the existing religious service and educational services approved under County Permit 92-CP-019. The proposed site improvements do not conflict with the land use designations or the Zoning Ordinance designation of Design Residential (DR).

Land Use (LU) Element Policy 2.3 and Table 2-1 provide development standards, including a maximum structure height of 35 feet and a maximum lot coverage of 30%. The project's proposed maximum height observes this height maximum with the main roof line at just over 24.5' and the focal point of the architecture at 34.6'. The proposed lot coverage is approximately 11%.

With respect to LU 1.8 and LU 1.9, the project design, including landscaping, is appropriate and compatible with the neighborhood and are subject to final approval by the Design Review Board to ensure compliance with City design standards. Pursuant to LU 1.13, adequate infrastructure and services are available to serve the Project.

##### **Open Space Element**

Given the scope and nature of the proposed site improvements (a 7,380 S.F. synagogue and 841 S.F. storage building for an existing synagogue and daycare/school), the project would not create a demand for, nor increase the use of, existing park/recreational facilities or open spaces within the community. As such, the Project would not result in any significant effects on recreational facilities or create any substantial new demand for public amenities. While small portions of the site are located within the 100-year flood zone from Las Vegas Creek (FEMA 2012), the new buildings are located outside the 100-year flood zone.

Open Space Policy OS 8 requires the protection of Native American/Paleontological Resources. Pursuant to OS 8.4, a Phase I Archaeological report and a supplemental memorandum were completed that concluded that shell fragments on the ground surface are not associated with a prehistoric or historic archaeological site and that grading and excavating the site would not have the potential to disturb unknown buried archaeological resources.

However, project mitigation measure require appropriate action is taken should unanticipated cultural or tribal resources be discovered. Mitigation requires work to be stopped if a cultural resource is encountered during soil disturbance until the find can be evaluated including by local Chumash representatives. As conditioned, the Project is consistent with the Open Space Element policies for cultural resources.

### **Conservation Element**

The project is consistent with the Conservation Element (CE) with implementation of the Conditions of Approval that require: (1) a 100-foot Stream Protection Area within which no grading, construction activities, or structural development would occur; (2) a nesting birds and raptors survey prior to construction; (3) the incorporation of best management practices to protect storm water quality and sediment control through the implementation of a Storm Water Control Plan and an Erosion and Sediment Control Plan or a Stormwater Pollution Prevention Plan, as appropriate based on the amount of disturbed area; (4) diversion reporting after construction in accordance with the City of Goleta's Construction and Demolition Debris Recycling Program Waste Reduction and Recycling Guidance Document; and (5) compliance with standards for dust control, noise control, washout areas and asbestos during construction.

As designed and with the above-mentioned standard conditions of approval and development regulations, the project is consistent with General Plan Policies CE 1.2 (k) and CE 1.9 (h) (nesting birds), CE 2.2 (SPA buffer), and CE 8.4 (buffer area for raptors). Further, the project complies or will comply with Policies CE 10.2, CE 10.3, CE 10.6, CE 10.7, CE 10.8, CE 10.9, CE 12.2, CE 12.3, CE 13.1, CE 13.2, and CE 13.3 that require limiting the area of disturbance, incorporation of Best Management Practices for Stormwater Management, stormwater management and erosion control requirements, control of emissions from new development and during grading and construction, energy efficiency, and use of renewable energy sources.

In addition, the project will not use invasive plant species that could unintentionally spread into the adjacent Las Vegas Creek. While one plant species, Cordyline "torbay dazzler" (*Cordyline australis*), is considered as having "limited" invasive tendencies in moist forest areas, given that the placement of the plant on the site (adjacent to the synagogue building) will be hundreds of feet away from Las Vegas Creek and the nature of Goleta's climates, the invasive potential of this plant species is minimal. Therefore, the proposed landscape plan would not conflict with Policy VH 4.9.



### **Safety Element**

The project is consistent with the Safety Element with implementation of the required Conditions of Approval. The topography of the inland site and surrounding developed parcels is gently sloped and the site is not mapped in an area of moderate or high landslide potential. As mentioned in the Open Space Element, small portions of the project site are located within the 100-year flood zone (FEMA 2012). However, the new structures are not located within the 100-year flood zone and the grading and hydrology plans will be designed to with this factor in mind and minimize the effects of flooding hazards. These conditions indicate that the site is appropriate for development of the proposed project. Furthermore, project grading and drainage plans have been designed to accommodate on-site stormwater.

Required Conditions of Approval include the preparation of a Geotechnical and Soils Engineering Report and incorporation of the recommendations from the Report regarding liquefaction, seismic-related settlement, and soil compaction ratios into the Project grading and building plans, pursuant to SE 4.3 and 4.11. These recommendations will be incorporated into the project design, and the project will also be designed to meet the California Building Code requirements for seismic and soil parameters.

The Project is adequately served by fire protection services and the Santa Barbara County Fire Department has reviewed and approved the access and circulation plan and has provided Conditions of Approval, pursuant to SE 7.2. The Project would comply with Fire Department standard conditions such as an approved turnaround, fire sprinklers, proper addressing, automatic fire or emergency alarm system, and gated access accessible by the Fire Department.

The Project is not within an airport approach zone or runway protection zone and is not subject to the Santa Barbara County Airport Land Use Plan.

The project would retain the existing residential, religious service, and educational service operations of the site. Collectively these uses do not involve the routine transport, use or disposal of hazardous substances, other than minor amounts typically used for the regular maintenance and replacement of the buildings and landscaping. Therefore, risk of upset is less than significant, and the Project is consistent with the policies of the Safety Element.

### **Visual and Historic Resources Element**

The project is consistent with the Visual and Historic Resources Element with the incorporated Mitigation Measures. The project site has been used for ranching and/or agriculture since the mid-1800's and has been used as a synagogue since the mid 1990's. The proposed project is not located on a scenic corridor and is consistent with the Visual and Historic Resources Element. Visual and Historic Resources Element Policies VH 1.1, VH 1.2, VH 1.4, VH 1.5, VH 2.1, VH 3.1, VH 3.2, VH 3.3, and VH 3.4

address scenic resources, views, scenic corridors, maintenance of community character, neighborhood identity, site design, and building design. The proposed project is consistent with these policies and would not interfere with existing private views of the Santa Ynez Mountains or be readily visible from scenic viewpoints. The proposed building designs are compatible with the neighborhood and abundant landscaping is proposed to blend the proposed development with surrounding vegetation.

VH 5.1 identifies local historic landmarks, VH 5.2 protects locally significant historic resources, VH 5.4 preserves historic resources, and VH 5.6 addresses the demolition of historic structures. Construction would result in the removal of a structure (the barn/storage garage) that is at least 80 years old and is associated with the prior agrarian activities that occurred on the site; and is somewhat associated with the Sexton family, which has historical importance in the community. The historical report prepared by Bookspan (2007) found that the barn is not an exceptional example and is in disrepair. Further, the report finds that the barn is not particularly historic given its simple utilitarian design, and its context and setting have been lost. What is notable about the barn is that it is an older structure in a modern neighborhood. The implementation of Mitigation Measures MM-CUL-1 and -2, Historic Building Relocation and Historic Building Documentation, are proposed to document the structure and relocate if possible consistent with the applicable policies.

### **Transportation Element**

The project is consistent with the Transportation Element. Transportation Element Policies TE 3.3 and TE 3.5 address major principal arterials (including Fairview Avenue) and collector streets (including Stow Canyon Road) that serve the project site. Conditions of Approval required the delivery of materials and equipment outside of non-school peak traffic periods as well as install improvements to allow to aid in the flow of traffic on nearby streets. TE 4.1 sets Level of Service Standard C for roadways and intersections, including the aforementioned collector streets and principal arterials in the project vicinity. According to the *Updated Traffic Study For The 6045 Stow Canyon Road Synagogue Project – City of Goleta* (Associated Transportation Engineers 2019) the project will not degrade principal arterial, collector street, or intersection level LOS in the project vicinity.

TE 9.2 addresses the adequacy of parking supply. Table 2 of the Draft Initial Study and Mitigated Negative Declaration determined that the proposed parking supply is adequate; moreover, this analysis is considered to be conservatively high as many attendees at services and holiday events often do not drive but will walk to the site in keeping with religious practices and observances. TE 11.4 requires bicycle parking in new developments; a Condition of Approval would be imposed to require on-site bicycle parking. Policy TE 9.5 addresses parking lot design; the project's parking design is consistent with this policy. TE 12.1 requires controlling the location and spacing of

driveways, as well as the design of parking lots, to avoid traffic and pedestrian conflicts; the project would reduce the number of driveways on the property from two to one, thereby reducing potential pedestrian conflicts, and the parking lot design is consistent with this policy.

### **Public Facilities Element**

The project is consistent with the Public Facilities Element. Public Facilities Policies PF 3.1, PF 4.1, PF 4.2, PF 5.1, PF 6.1, PF 9.1, PF 9.2, PF 9.3, and PF 9.7 address new development and provision of essential public services. Policy PF 4.1(l) “encourages existing development [to] be connected to the public water system of GWD and that the private wells be properly abandoned and closed.”

The New Synagogue Buildings for Chabad of Santa Barbara project will not result in any significant new demands on public facilities or services. Coordination with agencies providing public services has been performed throughout the development of this project. The project is designed to comply with fire safety design standards identified in the California Fire Code, as adopted by the Goleta Municipal Code, and Fire Department development standards. The Project would not result in any significant new demands on police or fire protection services than already anticipated with the originally entitled project. The new synagogue building (and all existing buildings) will obtain water from the GWD and sewer service from Goleta Sanitary District. The Goleta Water District and the Goleta Sanitary District are capable of providing water supplies and sanitation services, respectively. Given the non-residential nature of the project, there would be no impacts on schools, parks or other public facilities.

### **Noise Element**

The project is consistent with the Noise Element. Noise Element Policy NE 1.1 sets noise and land use compatibility criteria and NE 6.4 restricts construction hours. The project is consistent with land use compatibility standards identified in Noise Element Policy NE 1.1 and the Conditions of Approval would restrict construction noise consistent with NE 6.4.

### **Housing Element**

The project would retain the existing residential, religious service, and educational service operations of the site; would not substantially induce unplanned population growth; and would not result in any significant effects on housing or create any substantial new demand for housing. In addition, the project would not remove existing housing units, displace people, nor necessitate the construction of replacement housing. In addition, on-site housing will continue to be provided for the Rabbi and family. Further, the project will not increase the number of employees on site as the number of students will be decreasing. As such, no measurable impact due to

population growth and housing demand in the area would occur, and no mitigation of employee housing impacts is required per Housing Element Policy 2.2.

## ATTACHMENT 1

### EXHIBIT 4

#### Zoning Ordinance Consistency Analysis

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## EXHIBIT 4

### ZONING CONSISTENCY ANALYSIS FOR NEW SYNAGOGUE BUILDINGS FOR CHABAD OF SANTA BARBARA

Zoning Requirements	Proposed Project	Consistency
<b>Front Yard Setback:</b> Twenty (20) feet from the right of way line of any street.	Existing Residence: 31 feet from Stow Canyon Road  Proposed Synagogue: 122 feet from Stow Canyon Rd  Proposed Storage Bldg: 159 feet from Stow Canyon Rd	Yes
<b>Side and Rear Yard Setback:</b> Ten (10) feet from any side or rear property line, however the Planning Commission may increase this requirement to provide reasonable light, air, and privacy requirements.	Existing Residence: ~257 feet (rear) ~51 feet (side)  Proposed Synagogue: ~146 feet (rear) ~30 feet (side)  Proposed Storage Bldg: ~148 feet (rear) ~116 feet (side)	Yes
<b>Distance Required Between Buildings on the Same Building Site:</b> Five (5) feet between any buildings designed for or used for human habitation and any other building on the same building site.	Closest building to residence is 16 feet away	Yes
<b>Building Coverage:</b> No more than thirty (30) percent of the net area of the property shall be covered by buildings containing dwelling units.	Existing Residence Coverage: 3.37%	Yes

<p><b>Height limit:</b> No building or structure can exceed a height of thirty-five (35) feet.</p>	<p>Existing Residence: Single Story (less than 35')</p> <p>Proposed Synagogue Roofline: 24.55 feet</p> <p>Synagogue Architectural Projection: 34.6 feet Storage Building: 16 feet</p>	<p>Yes</p>
<p><b>Landscape/Screening of Parking Areas:</b></p> <p>a) Where non-residential parking abuts residential property, a wall or solid fence not less than five feet shall be erected and maintained between the parking area and the adjoining property.</p> <p>b) Screening shall be provided along each property line consisting of a five-foot wide strip, planted with sufficient shrubbery to effectively screen the parking area, or a solid fence or wall not less than four feet in height.</p> <p>c) Planting, fences, or walls abutting streets shall not exceed 30 inches in height for a distance of 25 feet on either side of entrances or exits to the property.</p> <p>d) When the total uncovered parking area on the property exceeds 3,600 square feet, trees, shrubbery and ground cover shall be provided at suitable intervals in order to break up the continuity of the parking area. All ends of parking lanes shall have landscaped islands.</p>	<p>a) A 5' solid fence is provided between the synagogue property and the adjacent Residence at 6051 Stow Canyon Road.</p> <p>b) Screening is currently provided with fence/wall/vegetation of 4' tall. The vegetation will be augmented as allowed by the Goleta Sanitary and Flood Control Districts.</p> <p>c) No plantings, fences or walls abutting streets are proposed to exceed 30 inches in height.</p> <p>d) The project provided landscaped parking islands at all ends of the parking lanes as well as at suitable intervals to break up the parking area.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>



<p><b>Additional Requirements:</b></p> <p>a) Uncovered parking areas shall be located no closer than fifteen (15) feet to the street right-of-way line nor closer than five (5) feet to any property line.</p> <p>b) Uncovered parking areas shall be screened from the street and adjacent residences to a height of at least four feet with hedges, dense plantings, solid fences, or walls.</p>	<p>a) Parking Spaces: 35 feet from street ROW and are a minimum of 5.96 feet from any other property line.</p> <p>b) Parking Spaces are screened from view with landscaping or fences/walls of at least 4 feet in height.</p>	<p>Yes</p> <p>Yes</p>
<p><b>Required Parking:</b></p> <p>a) Residence over 3,000 SF: 3 spaces within a garage</p> <p>b) Synagogue: 1 space/4 fixed seats (Includes after-school and adult religious/educational programs, as well as a seasonal summer camp)</p> <p>c) Pre-School/Daycare: 1 space/2 employees + 1 space/10 students</p> <p>d) The Planning Commission is to determine if there is a need to provide for bicycle parking.</p>	<p>a) Residence: 3 uncovered spaces</p> <p>b) Synagogue: 23 spaces</p> <p>c) Preschool/Day Care: 5 spaces</p> <p>d) 5 Bicycle Parking spaces proposed</p> <p><b>Total Provided: 31 spaces + 5 Bicycle</b></p> <p><b>Total Required: 29 spaces</b></p>	<p>Yes with Approval of Modification for no garage spaces</p> <p>Yes</p> <p>Yes</p> <p>PC Determination</p>

<p><b>Open Space and Landscaping:</b></p> <p>a) Not less than forty (40) percent of the net area of the property shall be devoted to common open space.</p> <p>b) Any driveway or uncovered parking area shall be separated from property lines by a landscaped strip not less than five (5) feet in width.</p>	<p>a) Open Space/Landscaping Provided: 64.4 % of site</p> <p>b) Landscape strip: minimum of 5.96 feet from property lines</p>	<p>Yes</p> <p>Yes</p>
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## ATTACHMENT 2

DRB Minutes of December 11, 2018

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# **MINUTES – APPROVED**

## **DESIGN REVIEW BOARD MEETING**

### **Tuesday, December 11, 2018**

---

3:00 P.M.  
City Hall – Council Chambers  
130 Cremona Drive, Suite B,  
Goleta, California

#### **Members of the Design Review Board**

Scott Branch (Architect), Chair  
Erin Carroll (Landscape Architect)  
Karis Clinton (Landscape Professional)  
Jennifer Fullerton (At-Large Member)

Bill Shelor (At-Large Member)  
Craig Shallenberger (Architect)  
Dennis Whelan (Alternate)

Mary Chang, Secretary  
Linda Gregory, Recording Clerk

#### **SIGN SUBCOMMITTEE - < 2:30 P.M. >**

Members: Scott Branch, Erin Carroll, Bill Shelor

#### **CALL TO ORDER AND PLEDGE OF ALLEGIANCE**

The meeting of the City of Goleta Design Review Board was called to order by Chair Branch at 3:03 p.m. in the City Hall Council Chambers, 130 Cremona Drive, Suite B, Goleta, California, followed by the Pledge of Allegiance.

#### **ROLL CALL OF DESIGN REVIEW BOARD**

Board Members present: Chair Branch, Member Carroll, Member Clinton,  
Member Fullerton, Member Shallenberger,  
Member Shelor, Alternate Whelan

Board Members absent: None

Staff Present: Mary Chang, Supervising Senior Planner; Brian Hiefield, Associate Planner; Darryl Mimick, Associate Planner; Joe Pearson II, Associate Planner; Chris Noddings, Assistant Planner; and Linda Gregory, Recording Clerk.

## **Design Review Board Minutes - Approved**

December 11, 2018

Page 2 of 8

### **PUBLIC FORUM**

No speakers.

#### **A. ADMINISTRATIVE AGENDA**

##### **A.1 Review and Approve the Design Review Board Minutes for November 13, 2018**

[Review and Approve the Design Review Board Minutes for November 13, 2018](#)

MOTION: Member Carroll moved, seconded by Member Shelor, to approve the Design Review Board Minutes for November 13, 2018, as submitted.

VOTE: Motion carried by the following voice vote: Ayes: Chair Branch, Member Carroll, Member Clinton, Member Shallenberger, Member Shelor. Noes: None. Abstain: Member Fullerton and Member Whelan.

##### **A.2 SELECTION OF VICE CHAIR AND SIGN SUBCOMMITTEE MEMBER**

MOTION: Member Shelor moved, seconded by Chair Branch, to elect Member Shallenberger for Vice Chair.

VOTE: Motion carried by the following voice vote: Ayes: Chair Branch, Member Carroll, Member Clinton, Member Fullerton, Member Shallenberger, Member Shelor, and Member Whelan. Noes: None.

MOTION: Member Shelor moved, seconded by Member Carroll, to appoint Dennis Whelan to the Sign Subcommittee.

VOTE: Motion carried by the following voice vote: Ayes: Chair Branch, Vice Chair Shallenberger, Member Carroll, Member Clinton, Member Fullerton, Member Shelor, and Member Whelan. Noes: None.

##### **A.3 REVIEW OF AGENDA**

Mary Chang, Supervising Senior Planner, reported no changes on today's agenda. The January 8, 2019, Design Review Board meeting is cancelled. The next Design Review Board meeting will be held on January 22, 2019.

#### **B. SIGN REVIEW**

##### **B.1 5892 Calle Real (APN 069-110-061) Bank of America Freestanding Sign, Ground Sign and Wall Sign**

## Design Review Board Minutes - Approved

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### Case No. 18-149-DRB

[Back of America Freestanding Sign Staff Report](#)

[Back of America Freestanding Sign Project Plans](#)

Staff Speaker

Darryl Mimick, Associate Planner

Member Shelor reported that today the Sign Subcommittee reviewed Item B.1 with Darryl Mimick, Associate Planner, and Brian English, agent for Keven Webb, property owner; and recommended approval of the freestanding sign to remain as is, and the new wall sign as proposed. Member Shelor noted that two members expressed concern that the ground sign exceeds existing size specifications, and one member recommended that the ground sign is acceptable.

MOTION: Member Shelor moved, seconded by Member Carroll, to continue to January 22, 2019, Item B.1, Bank of America Freestanding Sign, Ground Sign, and Wall Sign (5892 Calle Real (APN 069-110-061), case No. 18-149-DRB, for the applicant to resubmit plans per the Design Review Board review.

VOTE: Motion carried by the following voice vote: Ayes: Chair Branch, Vice Chair Shallanberger, Member Carroll, Member Clinton, Member Fullerton, Member Shelor, and Member Whelan. Noes: None.

### **B.2 146 South Fairview Avenue (APN 071-051-025) McDonald's Drive-Through Signage Case No. 18-098-DRB**

[McDonald's Drive-Through Signage Staff Report](#)

[McDonald's Drive-Through Signage Project Plans](#)

Member Shelor reported that today the Sign Subcommittee reviewed Item B.2 with Brian Hiefield, Associate Planner, and Scott Wilkeson of McDonald's USA, LLC; and recommended approval, noting that the signage proposed for the back of the building is hidden from public view and would be acceptable.

MOTION: Member Shelor moved, seconded by Member Carroll, to grant Design review approval of Item B.2, McDonald's Drive-

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Through Signage, 146 South Fairview Avenue (APN 071-051-025), Case No. 18-098-DRB, as submitted.

VOTE: Motion carried by the following voice vote: Ayes: Chair Branch, Vice Chair Shallenberger, Member Carroll, Member Clinton, Member Fullerton, Member Shelor, and Member Whelan. Noes: None.

### C. CONCEPTUAL REVIEW

#### C.1 475 S. Kellogg Avenue and 495 S. Kellogg Avenue; (APNs 071-140-067, -068)

**Toyota, Honda, and Nissan Revised Landscape Plan  
Case No. 17-122-DRB**

[Toyota, Honda, and Nissan Revised Landscape Plan Staff Report](#)

[Toyota, Honda, and Nissan Revised Landscape Plan Previously Approved Landscape Plans](#)

[Toyota, Honda, and Nissan Revised Landscape Plan Site Plans](#)

Site visits and ex-parte conversations. Site visits reported by Members, Previous site visits reported by Members Carroll, Clinton, Fullerton, and Shallenberger. No ex-parte conversations reported.

Staff Speaker:

Joe Pearson II, Associate Planner

The plans were presented by agent Steve Fort with SEPPS, on behalf of Catalina Barber Corp., property owner; and Charles McClure, project landscape architect.

ACTION: The Design Review Board conducted Conceptual review of Item C.1, Toyota, Honda, and Nissan Revised Landscape Plan, 475 S. Kellogg Avenue and 495 S. Kellogg Avenue (APN's 071-040-067, -068), Case No. 17-122-DRB, with the following comments:

#### Landscaping:

1. Consider planting native Sycamore or Oak in place of the Canary Island Pine species in the southern planting area.
2. Consider planting native Sycamore trees or other native species such as Oak trees in the larger planter areas.
3. Consider planting Orange or Lemon trees in reference to the history of this area in Goleta Old Town.
4. Plant tight groundcovers to eliminate weeds in the bare areas of dirt.



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5. The Golden Medallion species may have excessive litter.
6. In general, the proposal received positive comments.

### **C.2 7000 Hollister Avenue (APN's 073-030-026; -027; -028; -033) Hollister Village Apartments Case No. 18-149-DRB**

[Hollister Village Apartments Staff Report](#)

[Hollister Village Apartments Project Plans](#)

Site visits and ex-parte conversations: Site visits reported by Members Branch, Clinton, Shallenberger, and Shelor. Member Fullerton reported no new site visit. No ex-parte conversations reported.

Staff Speaker:

Mary Chang, Supervising Senior Planner

The plans were presented by Connor Best on behalf of Goleta Hollister Triangle, LLC, property owner; and Thomas Ochsner AIA, project architect.

Public Speaker:

Barbara Massey stated that she believes the new project is an improvement. Ms. Massey expressed the following concerns: 1) the southwest corner of the building is only eight feet from the setback which is a concern regarding the proximity of the closest residences to the cars; 2) the night lighting on the Glen Annie side will need to be considered carefully to prevent light spillage; and 3) dark colors on stucco should not be used in the development because a lighter color, other than white, seems more appropriate and inviting.

**ACTION:** The Design Review Board conducted Conceptual review of Item C.2, Hollister Village Apartments, 7000 Hollister Avenue (APN's 073-030-026, -027; -028, -033), Case No. 18-149-DRB, with the following comments;

#### Architecture:

1. The roof plan needs to be clarified. Submit floor plans for the review.
2. Break up the row of garage doors with colors and possibly other elements and/or materials.
3. The north elevation does not reflect the richness in articulation and materials as is achieved on the south elevation.
4. Consider a way to add landscaping on the north side of the building to create a visual line of sight for the neighbors living across the street.

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5. Refine the stone areas at the entrance and define the area as a place of entry with a positive character.
6. Study the alignment of the driveway being so close to the intersection.
7. The massing is appropriate. There are plenty of amenities and open space. A suggestion was made to consider adding more amenities
8. Consider photovoltaics for the roof.
9. Ideally, if the building could be split into two buildings, or if there is any way to get 10 or 15 feet of a split and offset, there could be some separation. Consider giving the building a more distinct identify rather than one long band.
10. One member commented that at this point, the project is an infill and something different would look out of place; however, the architecture does not have a sense of tie to the Goleta community.

### Landscaping:

11. Plant the native Sycamore species rather than the non-native species.
12. Include drought tolerant plantings.
13. Expand the plant list, adding grasses in the park that are more durable and adding more flowering plants and shrubs in the private area.
14. Relocate the benches to take advantage of the mountain views.
15. Add mulch for the walkways in the residential area.
16. Add a few more trees in the area near "Tree 5".

### Site Plan:

17. Submit a grading plan showing stormwater capture and study using the water more effectively, and for irrigation.
18. Incorporate an area for dogs in the open space area.
19. Add access for residents to the open space area and walkway.

### **C.3 6045 Stow Canyon Road (APN 071-140-044) New 7,389 SF Synagogue and 841 SF Storage Building Case 18-031-DRB**

[New Synagogue and Storage Building Staff Report](#)

[New Synagogue and Storage Building Project Plans](#)

Site visits and ex-parte conversations: Site visits reported by Member Fullerton and Member Shelor who each reported a limited view as the site was cordoned off. Vice Chair Shallenberger and Member Whelan each reported visiting the site on Google Earth. Member Clinton reported she is familiar with the site. Chair Branch reported a site visit and a chat with Jeffrey Berkus, project architect, which did not include a discussion of substance. No other ex-parte conversations reported.

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Staff Speaker:

Chris Noddings, Assistant Planner

The plans were presented by agent Steve Fort, SEPPS, on behalf of Rabbi Mendel Loschak of Chabad of Santa Barbara, property owner; and the project team including Jeffrey Berkus with Jeffrey Berkus Architects, project architect; Sam Maphis, Earthform Design, project landscape architect; Ben Krintzman with Jeffrey Berkus Architects, project designer; and Dale Weber, MAC Design Associates, project civil engineer

ACTION: The Design Review Board conducted Conceptual review of Item C.3, New 7,389 SF Synagogue and 841 SF Storage Building, 6045 Stow Canyon Road (APN 071-140-044), Case No. 18-031-DRB, with the following comments:

### Architecture:

1. In general, the project received positive comments and support. It is compatible and well-integrated into the topography.
2. The 3D and visuals provided by the applicant are gorgeous and helpful.
3. Submit a view of the streetscape.
4. Submit details for the materials and textures. The craftsmanship and artistry of the features are key to enhancing the design.
5. The finish and materials are beautiful. The project reflects a good sense of peace and serenity.
6. The plan for the skylight needs to be refined; consider putting the skylight in the circular corridor.
7. Add some kind of treatment or detail to the underside of the solar panels.
8. One member commented that the imagery of the building does not tie with the Goleta community; however, it will not be visible outside the site.
9. One member commented that there are schools and religious facilities that are also located in this neighborhood.
10. Play with the width of spaces to encourage faster or slower movement.
11. Craftsmanship of the vertical light well will be key. It should be decorative and not industrial. Do not make it look like a metal shop.
12. Consider placing a light well structure into the center water well for parity.
13. The storage building is designed well; the Dutch gable works with the main building.

### Landscaping:

14. Study the existing landscaping along the street to add additional landscaping to make the streetscape more attractive, particularly near the residence.
15. Add a few trees to limit the view of the building and parking from the street.

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16. Incorporate some fruit trees or some edible plantings.
17. A suggestion was made to add a private space for memorial services or to grieve.
18. Soften the hardscape with vertical landscaping. This is a fine balance softening the architecture with landscape and screening it.
19. Consider the opportunity to have an angled walkway going through the pavers.
20. One member does not like Italian Buckthorn.

### Site Plan:

21. The two entry places are important and just look like pavement. Enhance the design of the area with landscape and hardscape, especially at the west entry. Add a garden and seating; seating is particularly important where people want to sit or have to wait.
22. The walkways, well, turnaround, and the circulation plan do not flow well and need to be refined. Consider designing the site so that the turnaround can be used as a gathering space.
23. Consider adding a low planter wall in the center on the south elevation.
24. Take into account a plan for overflow parking when needed.
25. It would be nice and feel appropriate to have some covered parking that is associated to the residence, including a covered pathway from the parking spaces to the residence. A garage would be too much.
26. Define the location of the entry on the south elevation, possibly with landscaping or some other form.
27. Submit a lighting plan.
28. Submit a grading plan showing stormwater capture.

## **D. ANNOUNCEMENTS BY MEMBER**

Member Carroll announced that he may not be able to renew another term on the Design Review Board.

## **E. ADJOURNMENT: 5:25 P.M.**

## ATTACHMENT 3

### Applicant's Modification Justification

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**CHABAD OF SANTA BARBARA  
PARKING MODIFICATION JUSTIFICATION STATEMENT  
November 2019**

1. A garage was previously proposed east of the existing residence but was never built. Rather than propose another garage in that location at the intersection of Stow Canyon Road and Ardmore Drive, the proposed location results in fewer driveways, fewer potential traffic conflicts, less structural development and impermeable surface, and reduced visual impact.
2. The proposed location of the uncovered parking spaces will be screened from view from Stow Canyon Road. This is accomplished by being proposed in a location outside of the front setback, utilizing existing topography and relatively minor grading to establish the finish grade of parking spaces approximately 2 feet below the elevation of Stow Canyon Road, and utilizing existing and proposed landscaping to screen the spaces from view from Stow Canyon Road. The spaces will also be shaded by large existing trees to be retained.
3. The proposed location is consistent with the need to utilize only one driveway with access to Stow Canyon Road, as required by Public Works. An existing driveway for the residence is proposed to be removed.
4. Uncovered spaces proposed on permeable pavers reduce the amount of impermeable surface, notably on a site adjacent to Las Vegas Creek.
5. The placement and design of the uncovered parking spaces are consistent with neighborhood character, which includes Goleta Valley Church and Goleta Valley Junior High, along with residential uses and will not be detrimental to the enjoyment of any residential use on the parcels adjoining the subject site. The closest neighboring residential improvement is a large, concrete parking area and the rear side of a two-story garage/accessory structure.
6. The proposal does not adversely affect the demand for on-street parking in the immediate area. All required parking is provided on-site.
7. The project is not detrimental to existing physical access, light, solar exposure, ambient noise levels or ventilation on or off site.
8. Cars using the residential spaces will never have to back into Stow Canyon Road to leave the property. For example, cars would have had to back out, if the originally approved driveway and garage were built on the east side of the house.
9. The site design is intended to position the synagogue as a focal point in the distance for drivers entering the driveway, and not have a garage or carport interfere with that entry view/experience.

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## ATTACHMENT 4

### Council Resolution 19-43

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## **RESOLUTION NO. 19-43**

### **A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF GOLETA, CALIFORNIA ADOPTING A DEVELOPMENT IMPACT FEE REDUCTION PROGRAM FOR BENEFICIAL PROJECTS**

**WHEREAS** the City of Goleta's General Plan includes policies encouraging the development of incentives for development of projects deemed beneficial within the City of Goleta; and

**WHEREAS**, on February 19, 2019, the City Council adopted the Development Impact Fee (DIF) Ordinance, which became effective on April 20, 2019; and

**WHEREAS** the adopted DIF Ordinance established the City Council's ability to adopt a resolution to reduce, adjust or waive DIFs for categories of projects deemed beneficial by the City Council; and

**WHEREAS** the purpose of establishing DIF reductions for projects deemed beneficial is to identify specific types of development that reflect City policy priorities and need relief from paying DIFs.

**NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF  
THE CITY OF GOLETA AS FOLLOWS:**

#### **SECTION 1. Recitals**

The City Council hereby finds and determines that the foregoing recitals, which are incorporated herein by reference, are true and correct.

#### **SECTION 2. Adoption**

The City Council hereby finds and declares the categories of projects listed and defined in Exhibit "A", attached hereto and incorporated herein, to be beneficial projects for purposes of the DIF Ordinance.

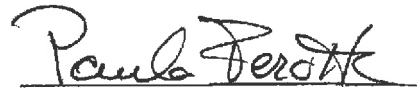
#### **SECTION 3. Documents**

The documents and other materials, which constitute the record of proceedings upon which this decision is based, are in the custody of the City Clerk, City of Goleta, 130 Cremona Drive, Suite B, Goleta, California, 93117.

**SECTION 4. Certification**

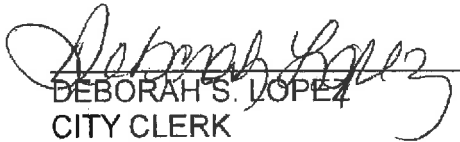
The City Clerk shall certify to the passage and adoption of this resolution and enter it into the book of original resolutions.

**PASSED, APPROVED AND ADOPTED** this 16<sup>th</sup> day of July 2019.



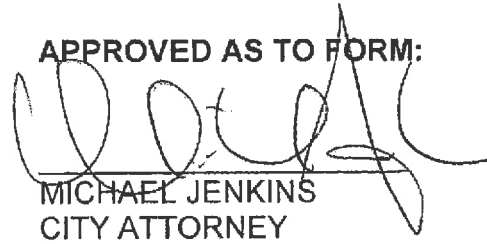
PAULA PEROTTE  
MAYOR

**ATTEST:**



DEBORAH S. LOPEZ  
CITY CLERK

**APPROVED AS TO FORM:**



MICHAEL JENKINS  
CITY ATTORNEY

STATE OF CALIFORNIA            )  
COUNTY OF SANTA BARBARA    )  
CITY OF GOLETA                )       ss.

I, DEBORAH S. LOPEZ, City Clerk of the City of Goleta, California, DO  
HEREBY CERTIFY that the foregoing Resolution No. 19-43 was duly adopted by  
the City Council of the City of Goleta at a regular meeting held on the 16<sup>th</sup> day of  
July 2019, by the following vote of the Council:

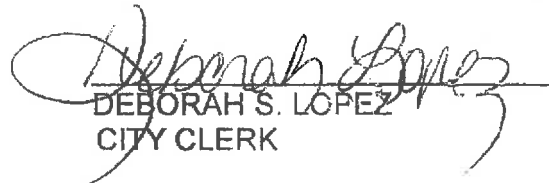
AYES:       MAYOR PEROTTE, MAYOR PRO TEMPORE RICHARDS,  
              COUNCILMEMBERS ACEVES, KASDIN AND KYRIACO

NOES:       NONE

ABSENT:     NONE

ABSTAIN:    NONE

(SEAL)

  
DEBORAH S. LOPEZ  
CITY CLERK

**Exhibit A  
Resolution No. 19-43**

**DIF Ordinance Beneficial Project Categories**

**DEVELOPMENT IMPACT FEE REDUCTION  
BENEFICIAL PROJECT CATEGORIES**

**Section 1. Definitions**

For purposes of beneficial project development impact fee reductions, the following terms shall be defined as follows:

"Accessory Dwelling Unit (ADU)": As presently defined in the Accessory Dwelling Units Ordinance 18-01, or may hereafter be amended, in the City of Goleta's Zoning Ordinances.

"Child Care Facility", "Family Day Care" and "Day Care": As presently defined, or may hereafter be amended, in the City of Goleta's Zoning Ordinances.

"Non-Profits": Any 501(c)(3) non-profit entity or governmental agency which provides public access to sites of significant historical, cultural or natural resource value, and/or provides essential health, safety, welfare or other community service needs, such as community recreational facilities.

"Special Care Home", "Residential Care Facilities", "Assisted Living", "Supportive Housing", "Transitional Housing" and "Special Needs Housing": As presently defined, or may hereafter be amended, in the City of Goleta's Zoning Ordinances.

**Section 2.** ADUs shall receive a Development Impact Fee waiver/reduction as follows:

- 1) ADUs with a floor area of 500 square feet or less will receive a 100% DIF waiver.
- 2) ADUs with a floor area exceeding 500 square feet will be charged a flat \$5,000 DIF to be distributed proportionally across all applicable DIF categories.

**Section 3.** In addition to the requirements of Section 2, ADUs qualifying for a Development Impact Fee reduction shall agree to all of the following additional terms in exchange for a reduction in DIFs:

1. As a condition of approval, ADU owners shall not rent the ADU as a short-term rental (for less than 30 days).
2. As a condition of approval, ADU owners shall agree to provide the City of Goleta documentation of ADU rents on an annual or other periodic basis.

**Section 4.** All projects by qualifying Non-Profit Organizations, as defined herein, shall receive a 100% Development Impact Fee reduction up to the first 15,000 square feet of the project.

An applicant receiving a DIF beneficial reduction for a project qualifying as a non-profit organization as defined in this resolution shall record a restrictive covenant on the subject property limiting its use to non-profit purposes in exchange for the DIF reduction. Upon change of non-profit status or acquisition of the property by a for-profit entity that does not qualify as a beneficial project, the for-profit entity shall pay the difference between the full amount of DIFs at the time the DIF was discounted and the reduced DIFs previously paid, plus annual adjustments for each year the discount was applied. Each annual adjustment shall be in accordance to a percentage equal to the appropriate Engineering Cost Index as published by Engineering News Record, or its successor publication, for the preceding 12 months for which the ECI is available and such ECI shall be specific to California or the nearest region. Such difference in DIFs shall be paid prior to close of escrow before transfer of ownership or possession. For a change of use to another beneficial project category, the applicant shall pay the difference for any greater amount of DIFs owed under the new beneficial project category.

**Section 5.** All qualifying non-profit Special Care Homes, Residential Care Facilities, Assisted Living, Supportive Housing, Transitional Housing, Special Needs Housing, Child Care Facility, Family Day Care or Day Care shall receive an 100% Development Impact Fee reduction.

All other for-profit Special Care Homes, Residential Care Facilities, Assisted Living, Supportive Housing, Transitional Housing, Special Needs Housing, Child Care Facility, Family Day Care or Day Care shall receive an 85% Development Impact Fee reduction.

An applicant receiving a DIF beneficial reduction for a project qualifying as either a non-profit or for-profit Special Care Home, Residential Care Facilities, Assisted Living, Supportive Housing, Transitional Housing, Special Needs Housing, Child Care Facility, Family Day Care or Day Care as defined in this resolution shall record a restrictive covenant on the subject property limiting its use to either a non-profit or for-profit Special Care Home, Residential Care Facilities, Assisted Living, Supportive Housing, Transitional Housing, Special Needs Housing, Child Care Facility, Family Day Care or Day Care in exchange for the DIF reduction.

Upon change of use or non-profit status, the applicant or its successor in interest shall pay the difference between the full amount of DIFs at the time the DIF was discounted and the reduced DIFs previously paid, plus annual adjustments for each year the discount was applied. Each annual adjustment shall be in accordance to a percentage equal to the appropriate Engineering Cost Index (ECI) as published by Engineering News Record, or its successor publication, for the preceding 12 months for which the ECI is available and such ECI shall be specific to California or the nearest region. Such difference in DIFs shall be paid prior to close of escrow before transfer of ownership or possession. For a change of use to another beneficial project category, the applicant shall pay the difference for any greater amount of DIFs owed under the new beneficial project category.

Section 6. All requests for DIF reductions shall be made prior to the time of a project's planning approval or, where no planning permit is required, prior to building permit issuance. An untimely DIF reduction request shall be denied.

Section 7. Any ADU project with a floor area of 500 square feet or less approved since adoption of the current ADU regulations (Ord. 18-01) shall be entitled to a refund of all Development Impact Fees paid for the project. Any ADU project with a floor area exceeding 500 square feet approved since adoption of Ord. 18-01 shall be entitled to a refund of any DIFs paid in excess of \$5,000.



## ATTACHMENT 5

### Project Plans

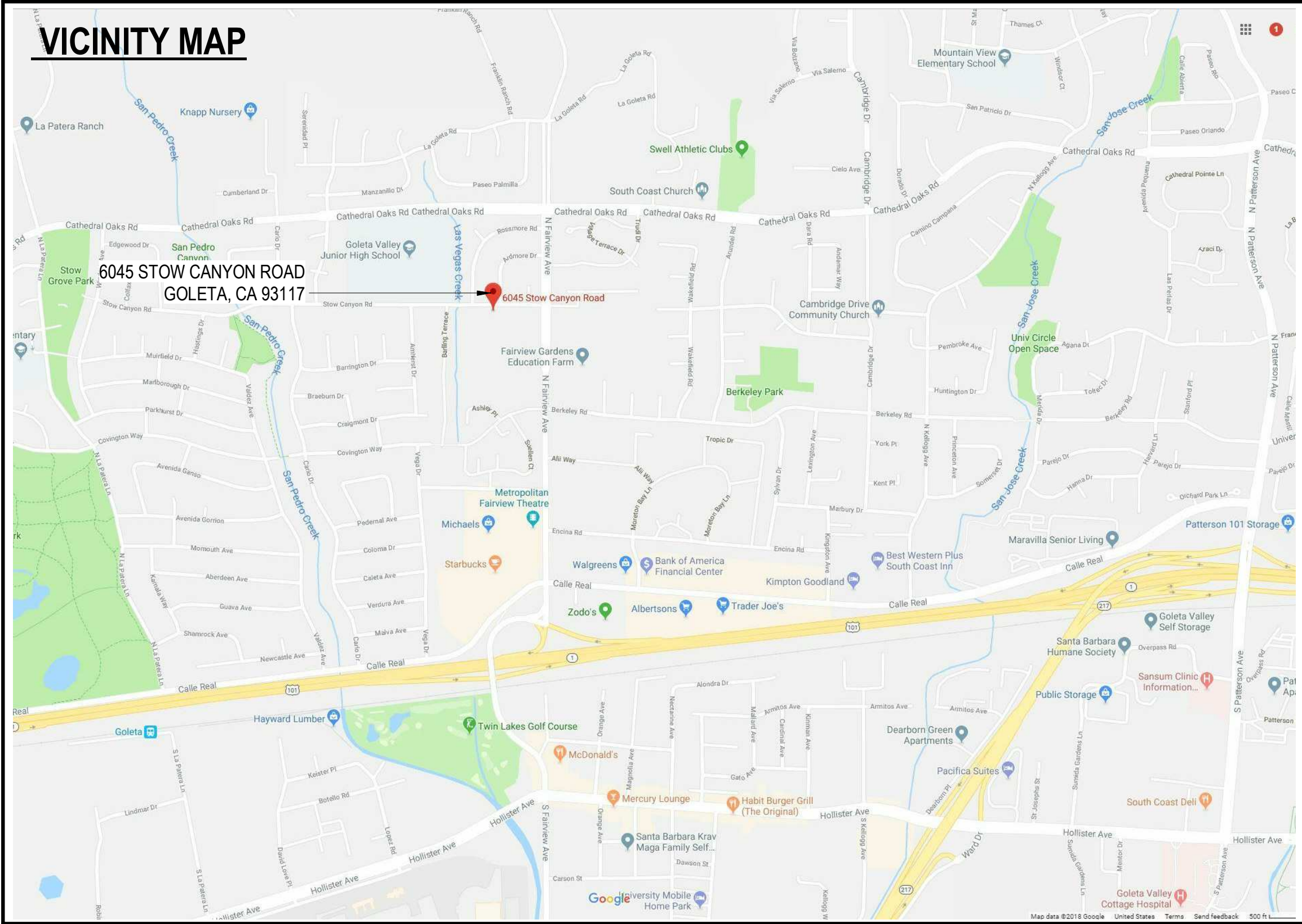
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ABBREVIATIONS

A.D.R.	ACCESS DOOR	F.E.	FIRE EXTINGUISHER	OFF.	OFFICE
A.F.F.	ABOVE FINISH FLOOR	F.E.C.	FIRE EXTINGUISHER WITH CABINET	OPNG.	OPENING
A.F.R.	ABOVE FINISH ROOF	F.H.C.	FIRE HOSE CABINET	OPP.	OPPOSITE
A.P.	ACCESS PANEL	F.SP.	FIRE STANDPIPE	PASS.	PASSAGE
A.S.	ALUMINUM SADDLE	F.D.	FLOOR DRAIN	P.L.	PROPERTY LINE
A.V.	AUDIO VISUAL	FDN.	FOUNDATION	P.LAM.	PLASTIC LAMINATE
AC.	ACOUSTIC, ACOUSTICAL	FIN.	FINISH, FINISHED	PH.	PENTHOUSE
AC.BD.	ACOUSTICAL BOARD	FIXT.	FIXTURE	PLAS.	PLASTER
AC.T.	ACOUSTICAL TILE	FLUOR.	FLUORESCENT	PLYWD.	PLYWOOD
ADD.	ADDITIONAL	FT. OR '	FEET OR FOOT	PNL.	PANEL
ADJ.	ADJUSTABLE	FTG.	FOOTING	PRNFAB.	PREFABRICATED
ALT.	ALTERNATE	FURR.	FURRING	PRIN.	PRINCIPAL
ALUM.	ALUMINUM	FUT.	FUTURE	PROD.	PRODUCTION
APPROX.	APPROXIMATE	FVC.	FIRE VALVE CABINET	PROP.	PROPERTY
ARCH.	ARCHITECTURAL	G.C.	GENERAL CONTRACTOR	PRTN.	PARTITION
ASST.	ASSISTANT	GA.	GAGE	PTD.	PAINTED
&	AND	GALV.	GALVANIZED	QTR.	QUARTER
@	AT	GEN.	GENERAL	QTY.	QUANTITY
		G.F.R.C.	GLASS FIBER REINFORCED CONCRETE		
		G.F.R.G.	GLASS FIBER REINFORCED GYPSUM		
		GL.	GLASS, GLAZE	R.	RADIUS
		GR.	GRADE	R.C.P.	RECEPTION
B	B LABEL DOOR	GUID.	GUIDANCE	RCPT.	ROOF DRAIN
B.M.	BENCH MARK	GYP.	GYPSUM	R.D.	RAIN LEADER
B.U.R.	BUILT-UP ROOF	GYP. BD.	GYPSUM BOARD	R.O.	ROUGH OPENING
BD.	BOARD	H.C.	HANDICAP, HANDICAPPED	REC.	RECESS
BIT.	BITUMINOUS	H.M.	HOLLOW METAL	REF.	REFRIGERATOR
BLDG.	BLOCK	H.P.	HIGH POINT	REINF.	REINFORCEMENT
BLK.	BLOCKING	HDWD.	HARDWOOD	RELOC.	RELOCATED
BM.	BEAM	HDWR.	HARDWARE	REQD.	REQUIRED
BOT.	BOTTOM	HGT.	HEIGHT	RESIL.	RESILIENT
BRK.	BRICK	HORIZ.	HORIZONTAL, HORIZONTALLY	REV.	REVISION
		H.V.A.C.	HEATING VENTILATING	RIS.	RISER
			AIR CONDITIONING	RM.	ROOM
				RUB.	RUBBER
C. TO C.	CENTER TO CENTER	I.D.	INSIDE DIAMETER	S.	SOUTH
C.B.	CHALKBOARD	I.P.S.	IRON PIPE SIZE	S.S.	STAINLESS STEEL
C.J.	CONTROL JOINT	IN. OR'	INCH OR INCHES	S.F.	SQUARE FEET
C.M.U.	CENTER LINE	INCL.	INCLUSIVE, INCLUDE, INCLUDING	SCHED.	SCHEDULED
CAB.	CONCRETE MASONRY UNIT	INSTR.	INSTRUCTION	SEC.	SECRETARIAL
CEM.	CEMENT	INT.	INSULATION	SECT.	SECTION
C.F.R.C.	CEMENTITIOUS FIBER	INSUL.	INSULATION	SH. MT.	SHEET METAL
	REINFORCED CONCRETE	INTER.	INTERMEDIATE	SHWR.	SHOWER
C.F.R.G.	CEMENTITIOUS FIBER			SK.	SIMILAR
	REINFORCED GYPSUM	JAN.	JANITOR	SKETCH.	SKETCH
CHG.	CHANGE	JT.	JOINT	SPCL.	SPECIAL
CLO.	CLOSET	K.E.C.	KITCHEN EQUIPMENT	SPEC.	SPECIFICATIONS
CNTR.	COUNTER		CONTRACTOR	ST.	STREET
COL.	COLUMN	L.P.	LOW POINT	STD.	STANDARD
COMM.	COMMUNICATIONS	LAB.	LABORATORY	STL.	STEEL
CONC.	CONCRETE	LAD.	LADDER	STOR.	STORAGE
CONF.	CONFERENCE	LAM.	LAMINATED	STRUCT.	STRUCTURAL
CONN.	CONNECTION	LANG.	LANGUAGE	SUPP.	SUPPORT
CONST.	CONSTRUCTION	L.A.S.S.	LAND, AIR, SEA AND SPACE	SUSP.	
CONT.	CONTINUOUS, CONTINUE	LAV.	LAVATORY		
CONTR.	CONTRACTOR	LDR.	LEADER	T. & G.	TONGUE AND GROOVE
CORR.	CORRIDOR	L.F.	LINEAR FEET	T.B.	TACKBOARD
OPT.	CARPET	LKR.	LOCKER	T.O.S.	TOP OF STEEL
CTND.	CONTAINED	LOC.	LOCATION	T.O.W.	TOP OF WALL
CTR.	CENTER	LT.	LIGHT	TD.	TREAD
CUST.	CUSTODIAL	LTWT.	LIGHTWEIGHT	TELE. CAB.	TELEPHONE CABINET
				TELE.	TELEPHONE
DEPT.	DEPARTMENT	M.B.	MARKERBOARD	THK.	THICKNESS
DET.	DETAIL	M.E.R.	MECHANICAL EQUIPMENT ROOM	THRES.	THRESHOLD
DIA.	DIAMETER	M.O.	MASONRY OPENING	THRU	THROUGH
DN.	DOWN	MACH. RM.	MACHINE ROOM	TLT.	TOILET ROOM
DNSPT.	DOWNSPOUT	MAS.	MASONRY	TRANS.	TRANSFER
DR.	DOOR	MAX.	MAXIMUM	TV.	TELEVISION
DWG.	DRAWING	MATL.	MATERIAL	TYP.	TYPICAL
		MECH.	MECHANICAL	U.N.O.	U.N.O.
		MET.	METAL	U.L.	U.L.
		MEZZ.	MEZZANINE	UR.	URINAL
		MFR.	MANUFACTURER	UR.H.	URINAL HANDICAPPED
E.F.S.	EXTERIOR FINISH SYSTEM	MIN.	MINIMUM	V.C.T.	VERIFY IN FIELD
EL.	ELEVATION	MISC.	MISCELLANEOUS	V.I.F.	VINYL WALLCOVERING
ELEC.	ELECTRIC	MLDG.	MOULDING	V.W.C.	VESTIBULE
ELEV.	ELEVATOR	MTD.	MOUNTED	VIT.	VITREOUS
ENCL.	ENCLOSURE				
ENT.	ENTRANCE	N.	NORTH	W.	WEST
EQ.	EQUAL	N.I.C.	NOT IN CONTRACT	W. GL.	WIRE GLASS
EQUIP.	EQUIPMENT	N.T.S.	NOT TO SCALE	W.M.	WIRE MESH
EXIST.	EXISTING	N.A.	NOT APPLICABLE	WITH	
EXP.	EXPANSION	N.O. OR#	NUMBER	WAINS.	WAINSCOT
EXP. JT.	EXPANSION JOINT	NOM.	NOMINAL	WD.	WOOD
EXT.	EXTERIOR	OA.	OVERALL	WKRM.	WORKROOM
		O.D.	OBSERVATION	WC-XX	
		O.H.	OPPOSITE HAND	WS-XX	STOREFRONT SYSTEM

VICINITY MAP



CHABAD OF SANTA BARBARA  
PROPOSED DEVELOPMENT PLAN

DEVELOPMENT PLAN / CONDITIONAL USE PERMIT

PROJECT DIRECTORY

CLIENT:	<b>CHABAD OF S. BARBARA</b> 6045 Stow Canyon Road Goleta, CA 93117 Ph. (805) 896-7265 Mendel Loschak rabbimendel@sbcchabad.org
ARCHITECT:	<b>JEFFREY BERKUS ARCHITECTS, INC.</b> 924 Anacapa Street, Suite 1B Santa Barbara, CA 93101 Ph. (805) 963-8902 Jeffrey Berkus, AIA, Principal jeff@berkusdesign.com
LAND USE / AGENT:	<b>SUZANNE ELLEDGE PLANNING &amp; PERMITTING SERVICES, INC.</b> 1625 State Street, Suite 1 Santa Barbara, CA 93101 Ph. (805) 966-2758 x 101 Steve Fort stevef@sepps.com
STRUCTURAL ENGINEER:	<b>TBD</b>
CIVIL ENGINEER:	<b>MAC Design Associates</b> 1933 Cliff Drive, Suite 6 Santa Barbara, CA 93109 Ph. (805) 957-4748 Dale W. Weber, PE dweber@macdesignsnb.com
GENERAL CONTRACTOR:	<b>TBD</b>
SURVEYOR:	<b>Waters Land Surveying, Inc.</b> 5553 Hollister Avenue, Suite 7 Goleta, California 93117 Ph. (805) 967-4416 Barry J. Waters, PLS barrywaters55@gmail.com
GEOTECHNICAL ENGINEER:	<b>Pacific Materials Lab</b> 35 South La Patera Lane Goleta, CA 93117 Ph. (805) 964-6901 Ron Pike pml@pml.sbcocmail.com
LANDSCAPE ARCHITECT:	<b>Earthform Design</b> 1227 De La Vina Street, Suite A Santa Barbara, CA 93101 Ph. (805) 963-2006 Sam Maphis, ASLA sam@earthform.com
TRANSPORTATION ENGINEER:	<b>Associated Transportation Engineers</b> 100 North Hope Avenue, Suite 4 Santa Barbara, CA 93101 Ph. (805) 687-4418 Scott Schell, AICP, PTP sschell@atesb.com
BIOLOGY:	<b>Hunt &amp; Associates Biological Consulting Services</b> 5290 Overpass Road, Suite 108 Santa Barbara, CA 93111 Ph. (805) 967-8512 Lawrence Hunt anniella@verizon.net
ARCHAEOLOGY:	<b>AMEC FOSTER WHEELER</b> 104 West Anapamu Street, Suite 204A Santa Barbara, CA 93101 Ph. (805) 962-0992 David Stone david.f.stone@amecfw.com

PROJECT DATA

LOT SIZE:	143,996 SQ. FT. (3.31 ACRES)
GROSS LOT AREA:	
NET LOT AREA:	PER THE INLAND ZONING ORDINANCE DEFINITION OF "LOT AREA NET," THE NET LOT AREA EQUALS THE GROSS LOT AREA AS NO PORTION OF THE PROPERTY CONTAINS A PUBLIC STREET.
EXISTING BUILDING SQUARE FOOTAGE (GROSS):	
SINGLE FAMILY DWELLING:	4,855 SQ. FT.
(4,093 sq. ft. int. + 762 sq. ft. ext. deck)	
EDUCATION / SYNAGOGUE SERVICES:	2,445 SQ. FT.
SPA BUILDING:	352 SQ. FT.
SHED (TO BE REMOVED):	53 SQ. FT.
BARN (TO BE REMOVED):	1,440 SQ. FT.
STORAGE TRAILER (TO BE REMOVED)	120 SQ.FT.
OPEN-SIDED STORAGE	176 SQ.FT.
TOTAL EXISTING BUILDING SQ. FT.:	9,441 SQ. FT.

PROPOSED NEW BUILDING SQUARE FOOTAGE (GROSS):	
SYNAGOGUE:	7,293 SQ. FT.
ROOF TERRACE:	815 SQ. FT.
TOTAL SYNAGOGUE & ROOF TERRACE	8,108 SQ. FT.
STORAGE BUILDING:	841 SQ. FT.
COVERED TRASH ENCLOSURE:	114 SQ. FT.

PROPOSED BUILDING LOT COVERAGE:	
EXISTING BUILDINGS TO REMAIN:	7,828 SQ. FT.
(dwelling+decks/education building/spa)	
PROPOSED SYNAGOGUE:	7,293 SQ. FT.
PROPOSED STORAGE:	841 SQ. FT.
PROPOSED TRASH ENCLOSURE:	114 SQ. FT.
TOTAL BUILDING AREA:	16,076 SQ. FT.

PROPOSED TOTAL BUILDING LOT COVERAGE (GROSS): 11.2%  
(16,076 SQ. FT. / 143,996 SQ. FT.)

PROPOSED TOTAL BUILDING LOT COVERAGE (NET): 12.5%  
(16,076 SQ. FT. / 128,115 SQ. FT.)

PROPOSED LOT COVERAGE BREAKDOWN (GROSS):	
LANDSCAPE / OPEN SPACE:	81,798 SQ. FT. (56.8%)
PARKING / DRIVE AISLES (pervious):	9,224 SQ. FT. (6.4%)
ARCHERY RANGE (pervious):	2,480 SQ. FT. (1.7%)
PLAY AREA (pervious):	5,030 SQ. FT. (3.5%)
BIO-RETENTION AREAS (pervious):	3,462 SQ. FT. (2.4%)
SUBTOTAL PERVIOUS AREAS:	101,994 SQ. FT. (70.8%)

PARKING / DRIVE AISLES (impervious):	15,319 SQ. FT. (10.6%)
PATHWAYS / OTHER HARDSCAPE (impervious):	9,369 SQ. FT. (6.5%)
BASKETBALL COURT (impervious):	1,238 SQ. FT. (0.9%)
BUILDING COVERAGE:	16,076 SQ. FT. (11.1%)
SUBTOTAL IMPERVIOUS AREAS:	42,002 SQ. FT. (29.2%)

TOTAL: 143,996 SQ. FT. (100.0%)

PROPOSED PARKING:	(5) BICYCLE / (31) AUTOMOBILE SPACES
RESIDENTIAL:	3 SPACES
SYNAGOGUE: (25 STANDARD, 3 ACCESSIBLE)	28 SPACES

REQUIRED PARKING BY CITY : (TBD by Planning Commision) BICYCLE SPACES (29) AUTOMOBILE SPACES

Existing Residence over 3,000 SF: 3 spaces

New 7,293-SF Synagogue\* : One per four fixed seats : 81 fixed seats/4 = 21 spaces  
\*Includes after-school and adult religious/educational programs, as well as a seasonal summer camp.

Existing 2,445-SF Synagogue/Pre-School Building :  
465 SF of pre-school : One space per two employees, 4 employees/2 = 2 spaces  
One space per ten students : 30 students/10 = 3 spaces

PROJECT INFORMATION

PROJECT:	CHABAD OF S. BARBARA 6045 STOW CANYON ROAD GOLETA, CA 93117
APN:	077-170-044
ZONING / LAND USE:	DR-4.6 / R-SF SINGLE FAMILY RESIDENTIAL
FIRE ZONE:	LRA -INCORPORATED NON-VHFHSZ
PROPERTY AREA:	GROSS: 143,996 SQ. FT. (3.29 ACRES)
MAX HEIGHT:	35' PER ORDINANCE; 34'-7 1/4" PROPOSED (AT HIGHEST ARCHITECTURAL PROJECTION
LOT SLOPE:	APPROX. 6%
OCCUPANCY TYPE:	A-2, A-3
FEMA FLOOD ZONE:	YES
FLOOD ZONE:	AE
SPRINKLERS:	YES
SANITARY DISTRICT:	GOLETA SANITARY DISTRICT
WATER DISTRICT:	GOLETA WATER DISTRICT

SHEET INDEX

GENERAL	
A0.00	COVER SHEET
ARCHITECTURAL	
A1.00	EXISTING SITE PLAN
A1.01	PROPOSED SITE PLAN
A2.01	MAIN FLOOR PLAN
A2.02	TERRACE / ROOF PLAN
A2.03	STORAGE PLAN & ELEVATIONS
A3.01	NORTH & SOUTH ELEVATIONS
A3.02	EAST & WEST ELEVATIONS
A4.01	PARTIAL SITE SECTIONS
A5.01	PERSPECTIVE STUDIES
A5.02	PERSPECTIVE RENDERING
A5.03	PERSPECTIVE RENDERING

CIVIL

C-1	GENERAL INFORMATION
C-2	DEMOLITION PLAN
C-3	PRELIMINARY GRADING & DRAINAGE PLAN
C-4	PRELIMINARY UTILITY PLAN

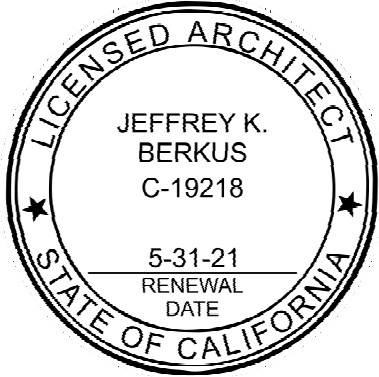
LANDSCAPE

L-1	CONCEPTUAL PLANTING PLAN
L-2	IRRIGATION SCHEMATIC PLAN
L-3	PRELIMINARY LIGHTING PLAN

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PROJECT NAME

CHABAD OF  
S. BARBARA

ADDRESS

6045 STOW  
CANYON ROAD  
APN 077-170-044

PROJECT No.

15013

SCALE

12" = 1'-0"

SHEET TITLE

COVER SHEET

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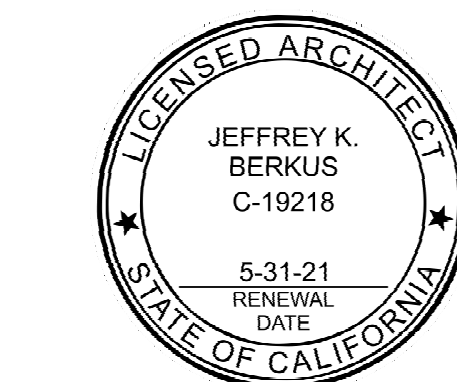






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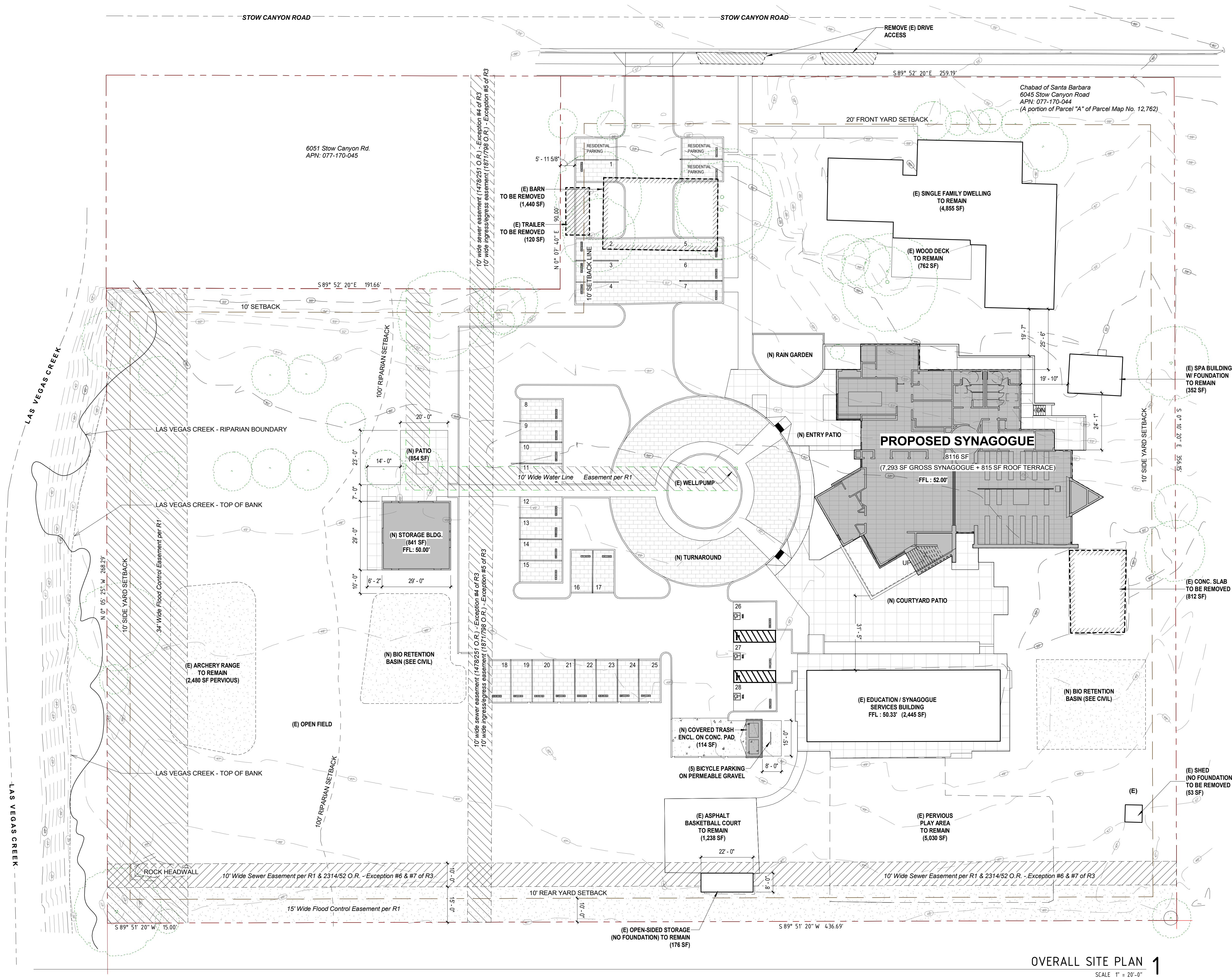
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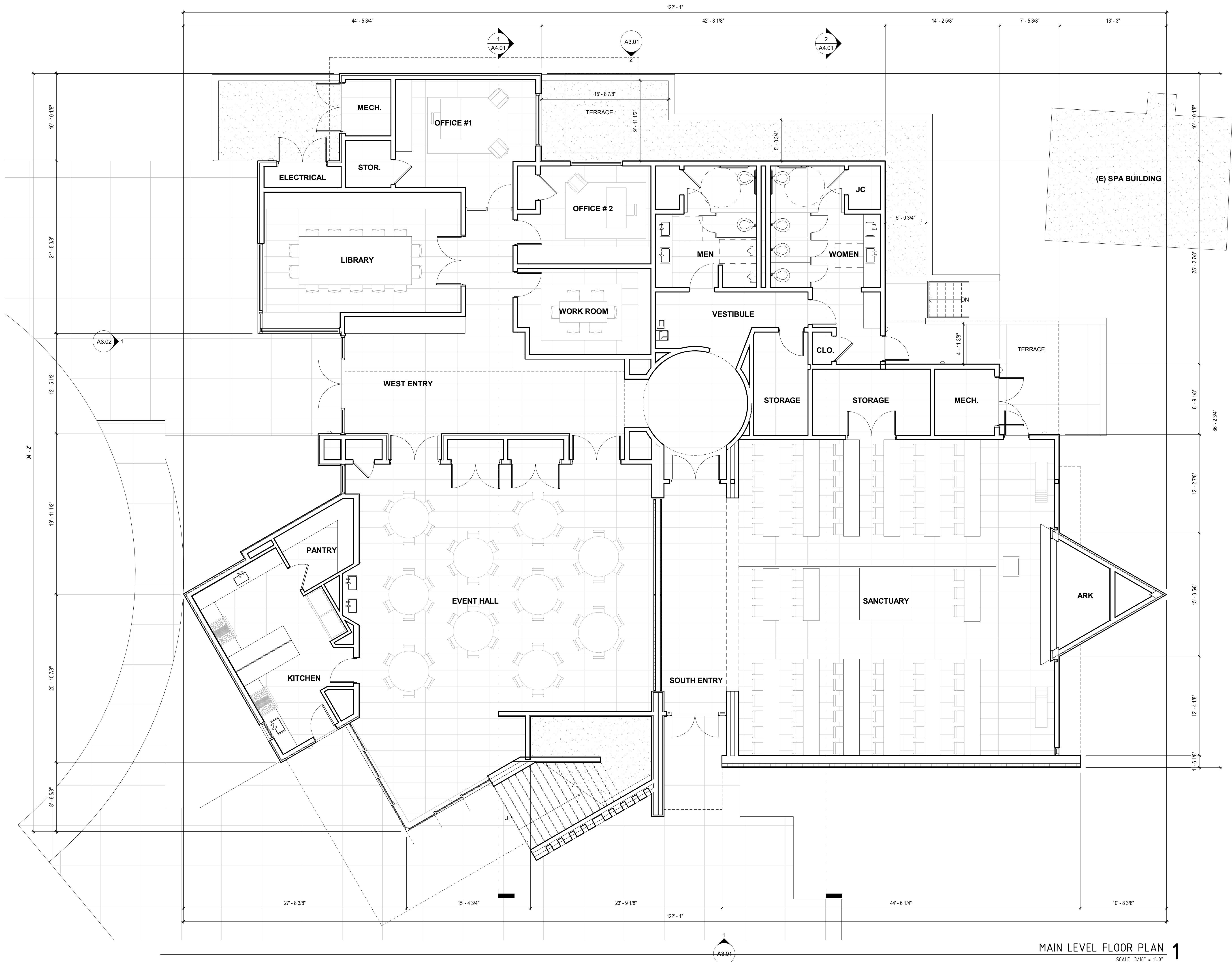
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OVERALL SITE PLAN

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MAIN LEVEL FLOOR PLAN 1  
SCALE 3/16" = 1'-0"

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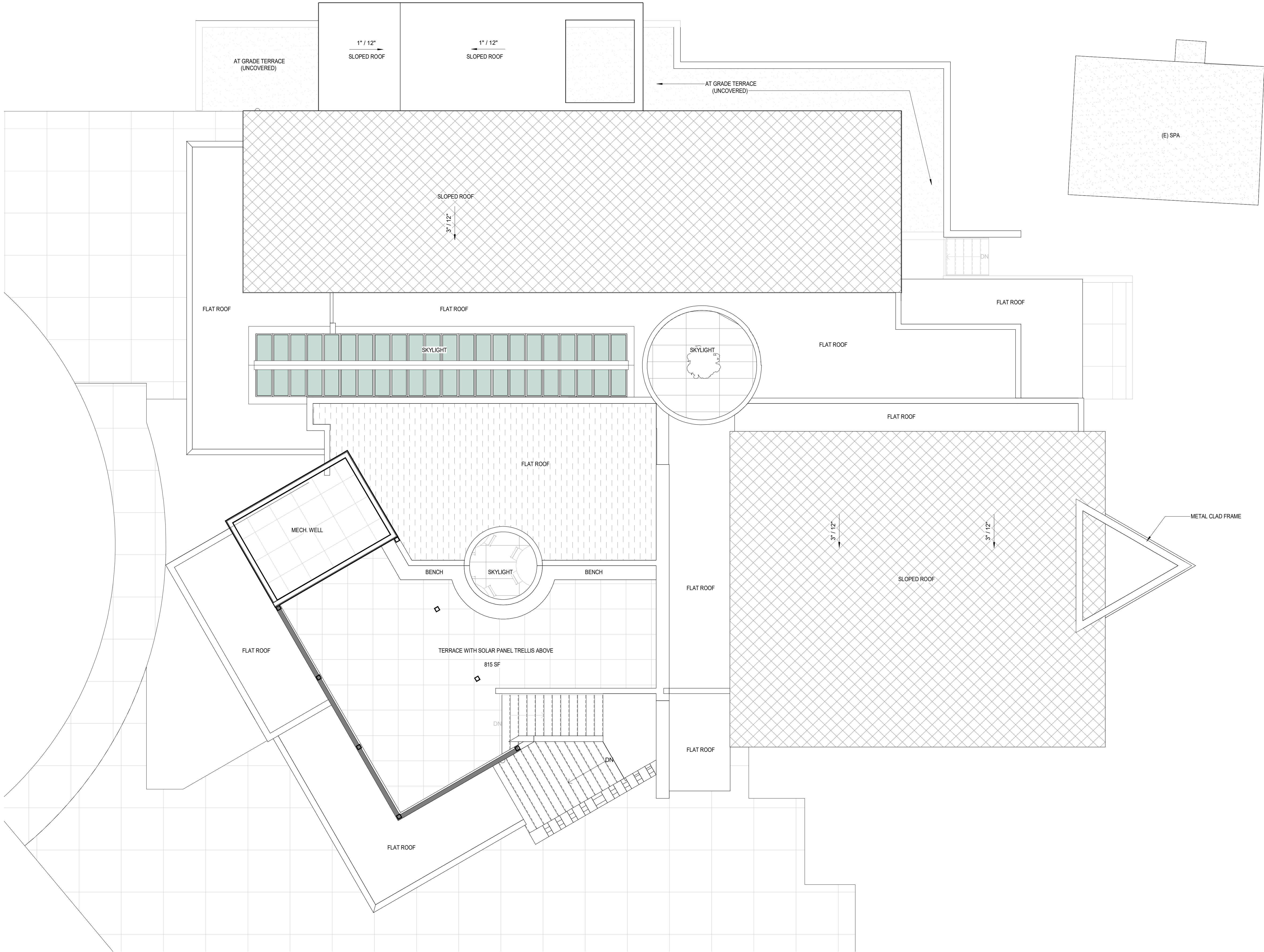
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**3/16" = 1'-0"**

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**MAIN LEVEL FLOOR  
PLAN**

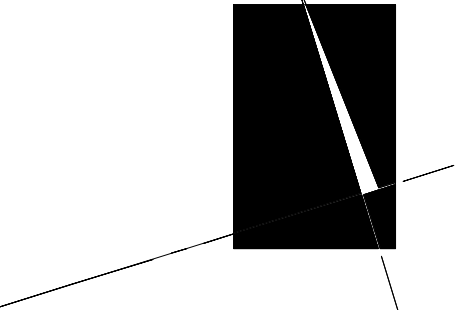
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ROOF PLAN 1  
SCALE 3/16" = 1'-0"



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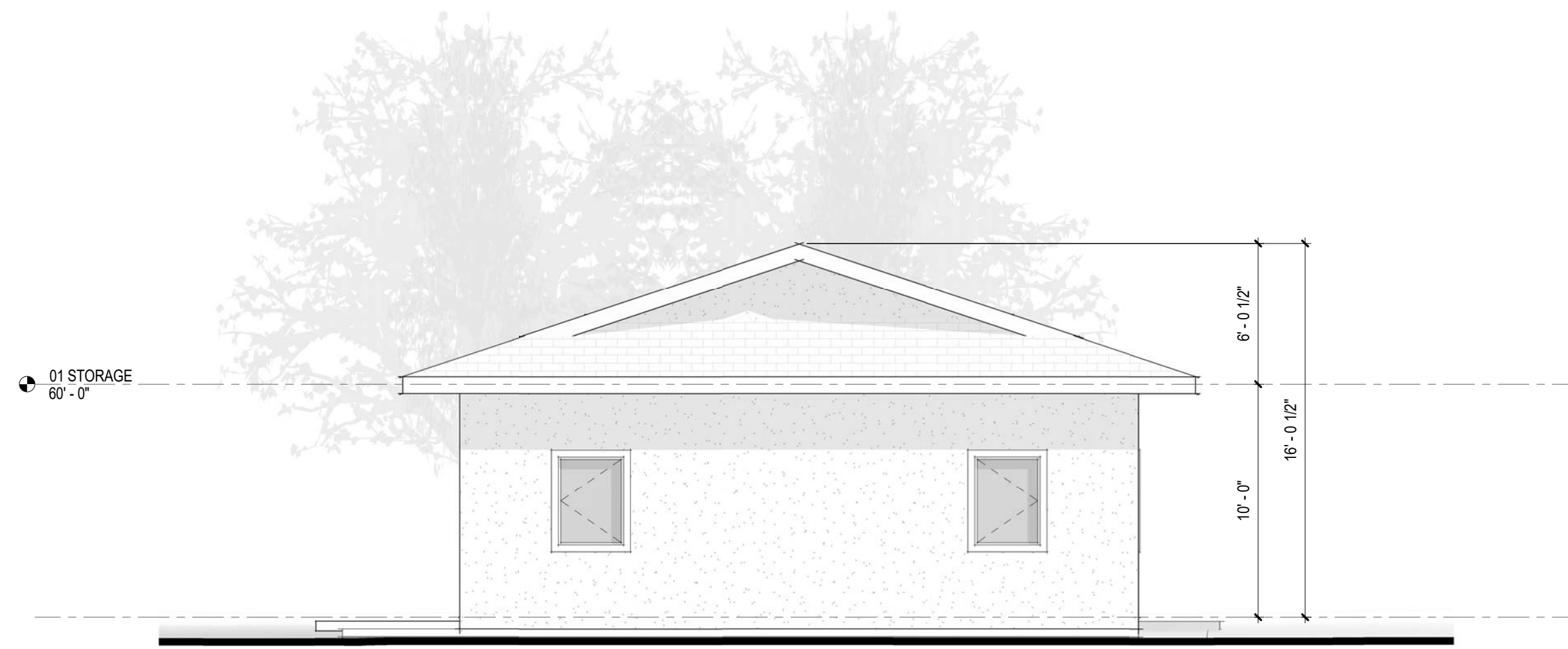
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**TERRACE / ROOF  
PLAN**

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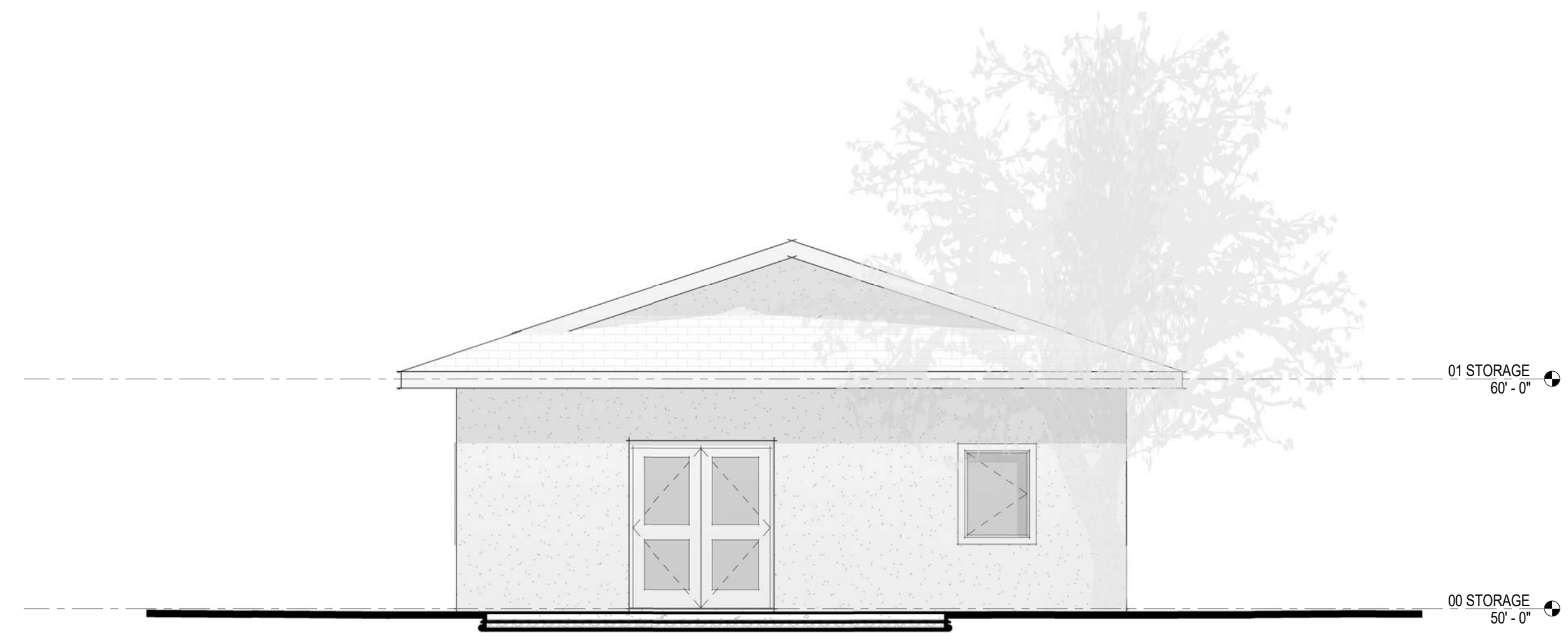
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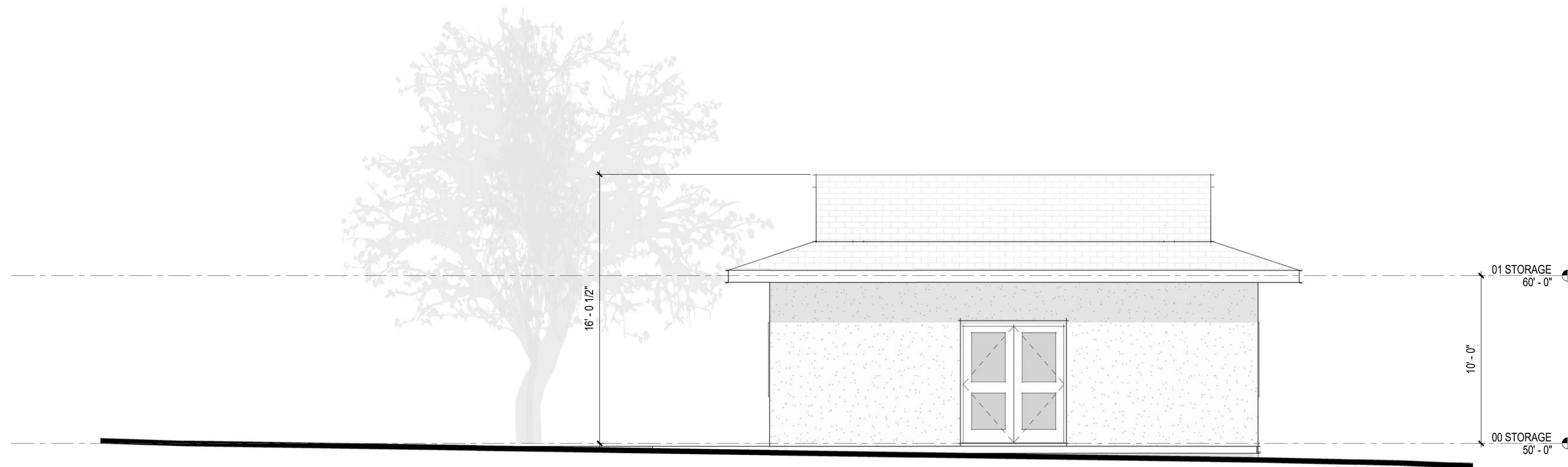
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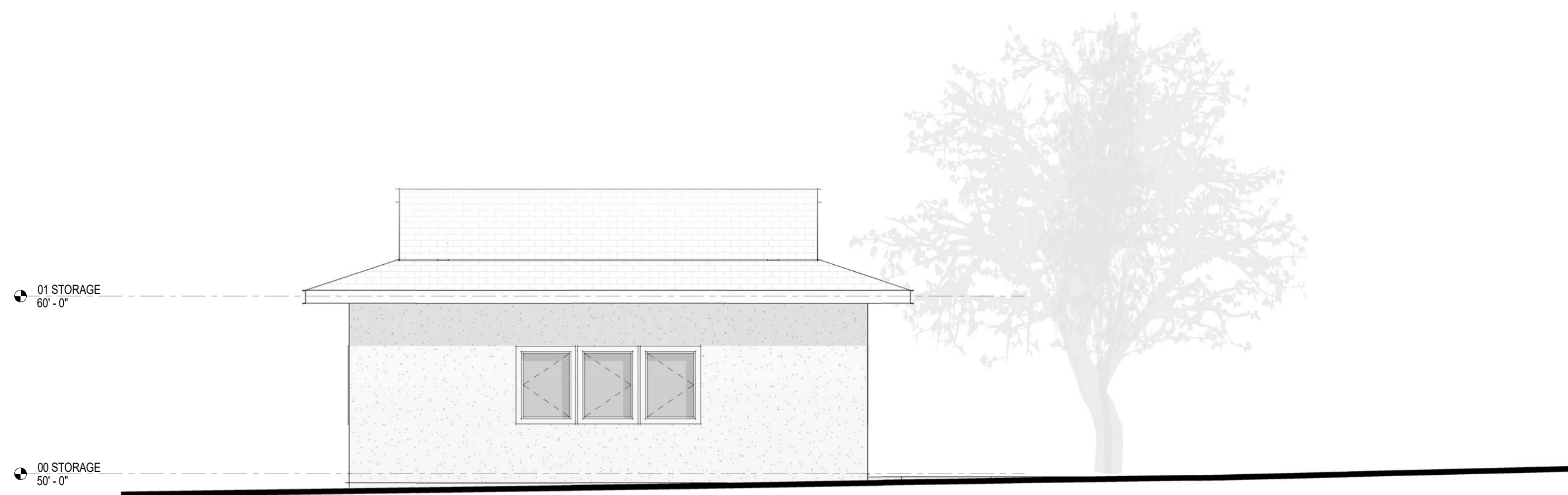
STORAGE BUILDING SOUTH ELEVATION 5  
SCALE 3/16" = 1'-0"



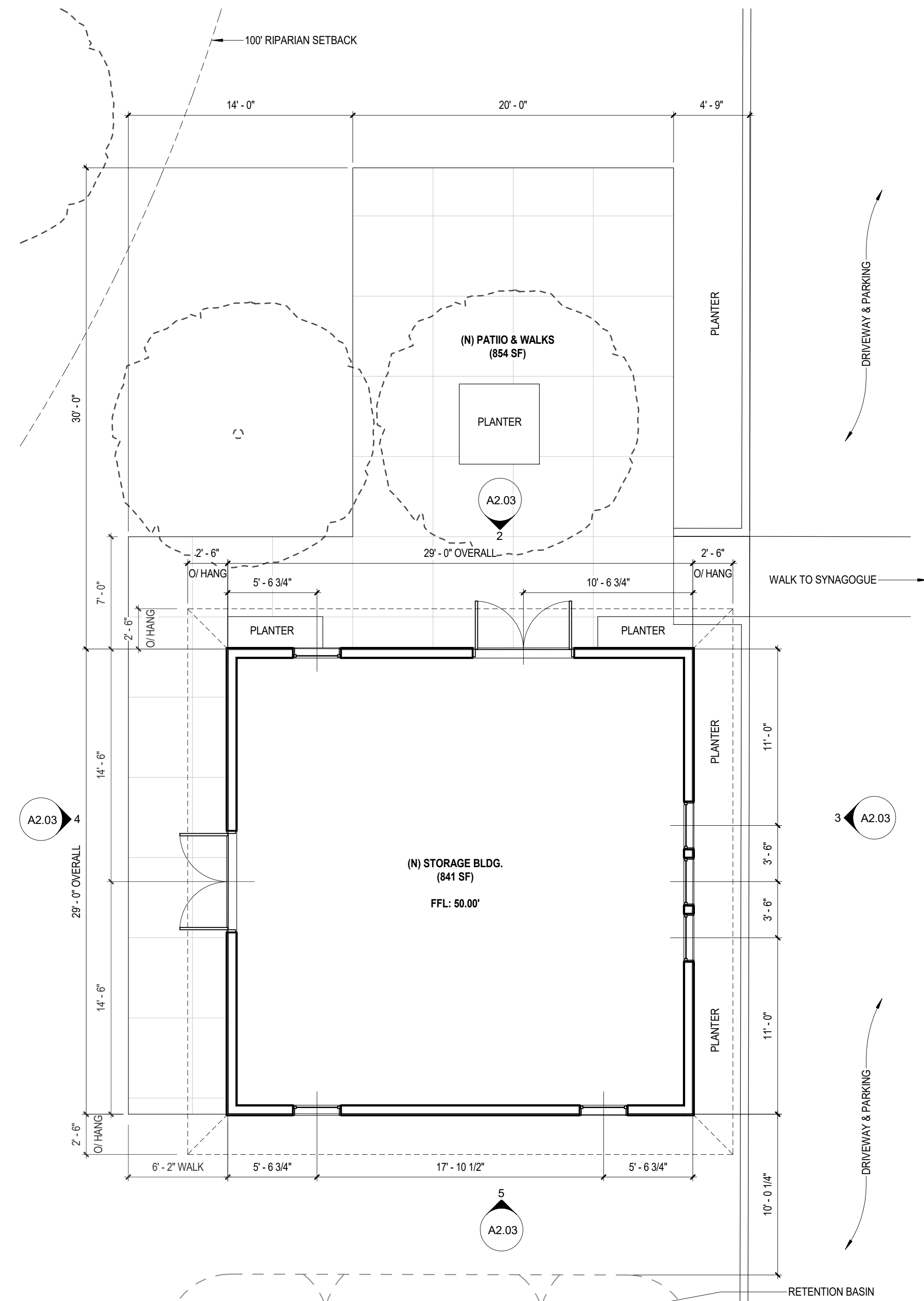
STORAGE BUILDING NORTH ELEVATION 2  
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STORAGE BUILDING WEST ELEVATION 4  
SCALE 3/16" = 1'-0"

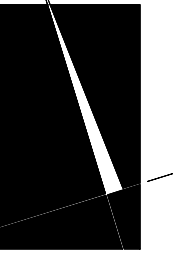


STORAGE BUILDING EAST ELEVATION 3  
SCALE 3/16" = 1'-0"



STORAGE BUILDING FLOOR PLAN 1  
SCALE 3/16" = 1'-0"

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FOR CONSTRUCTION

PROJECT No.

**15013**

SCALE

**3/16" = 1'-0"**

SHEET TITLE

**STORAGE PLAN &  
ELEVATIONS**

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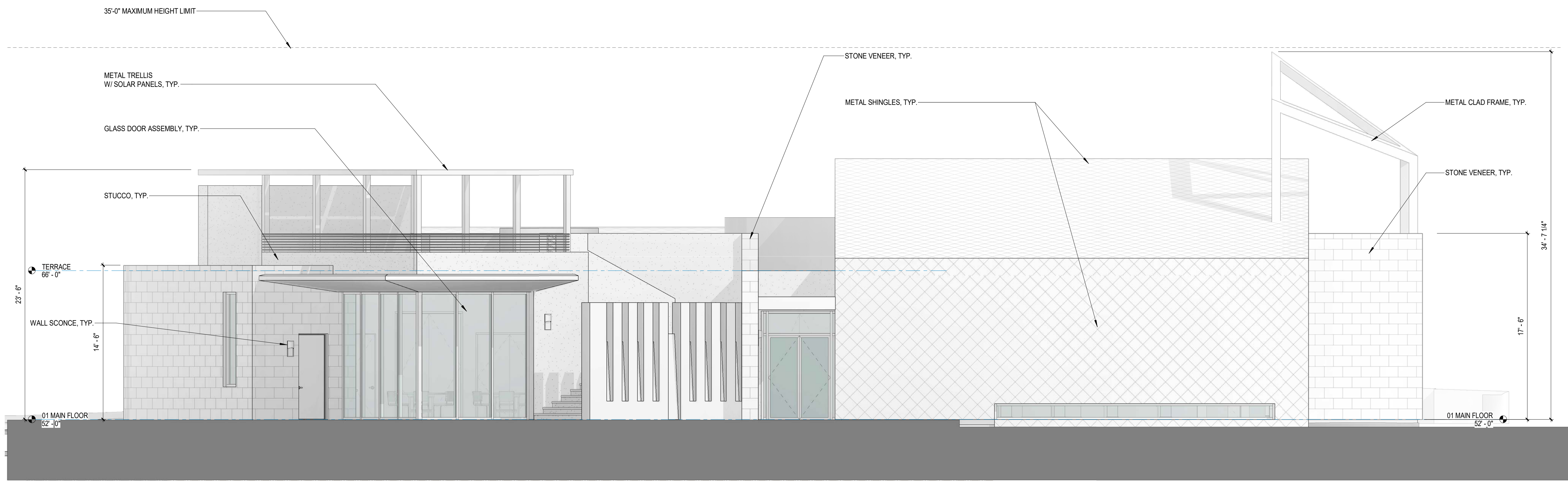
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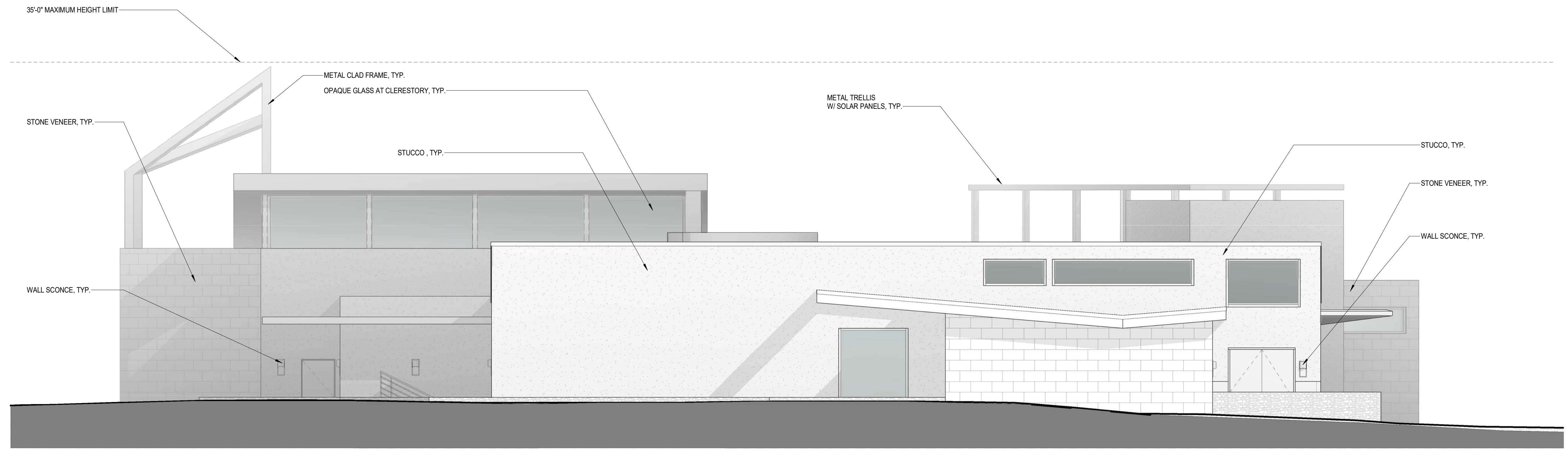
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SOUTH ELEVATION 1  
SCALE 3/16" = 1'-0"

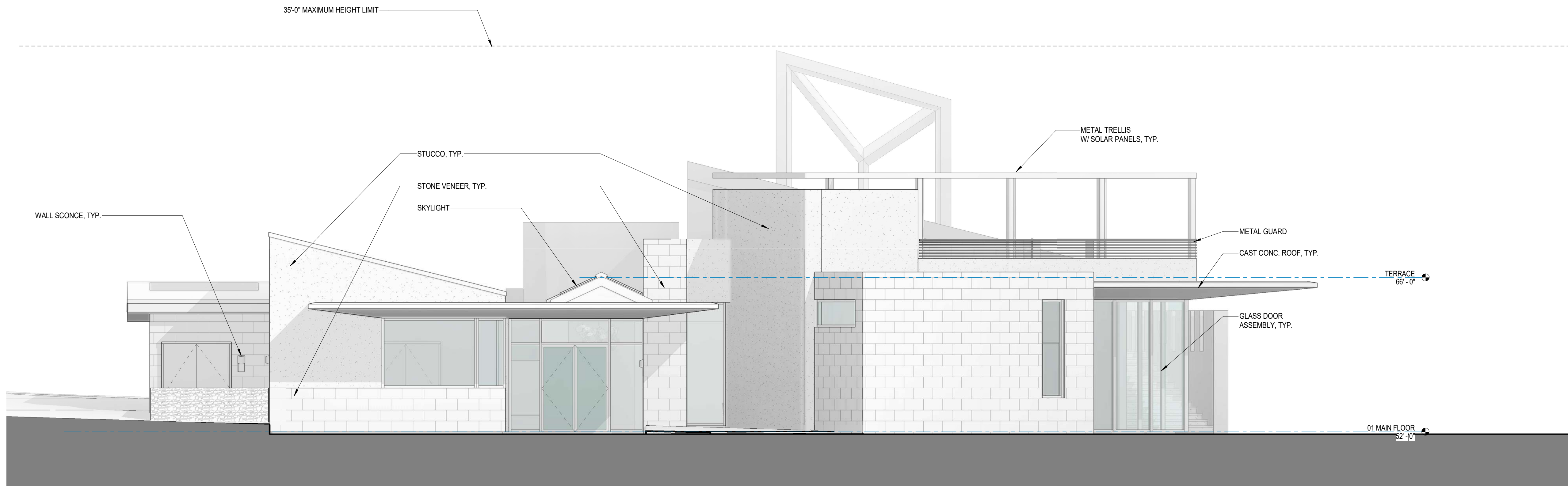


NORTH ELEVATION 2  
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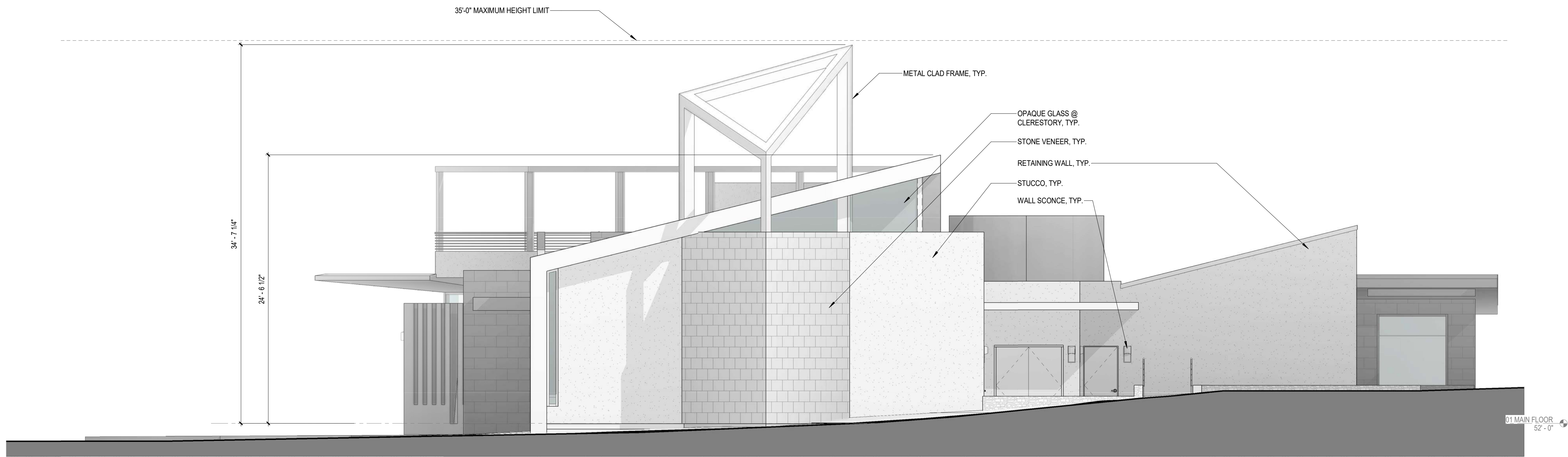


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WEST ELEVATION 1  
SCALE 3/16" = 1'-0"



EAST ELEVATION 2  
SCALE 3/16" = 1'-0"

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SCALE

3/16" = 1'-0"

SHEET TITLE

EAST & WEST  
ELEVATIONS

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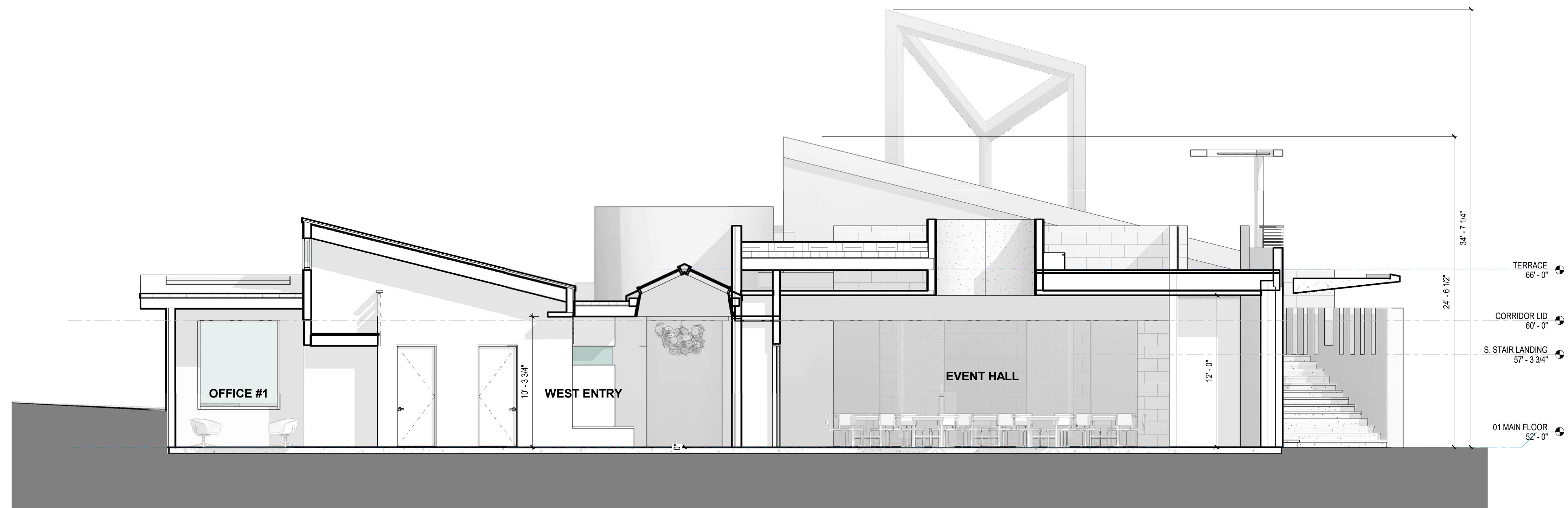
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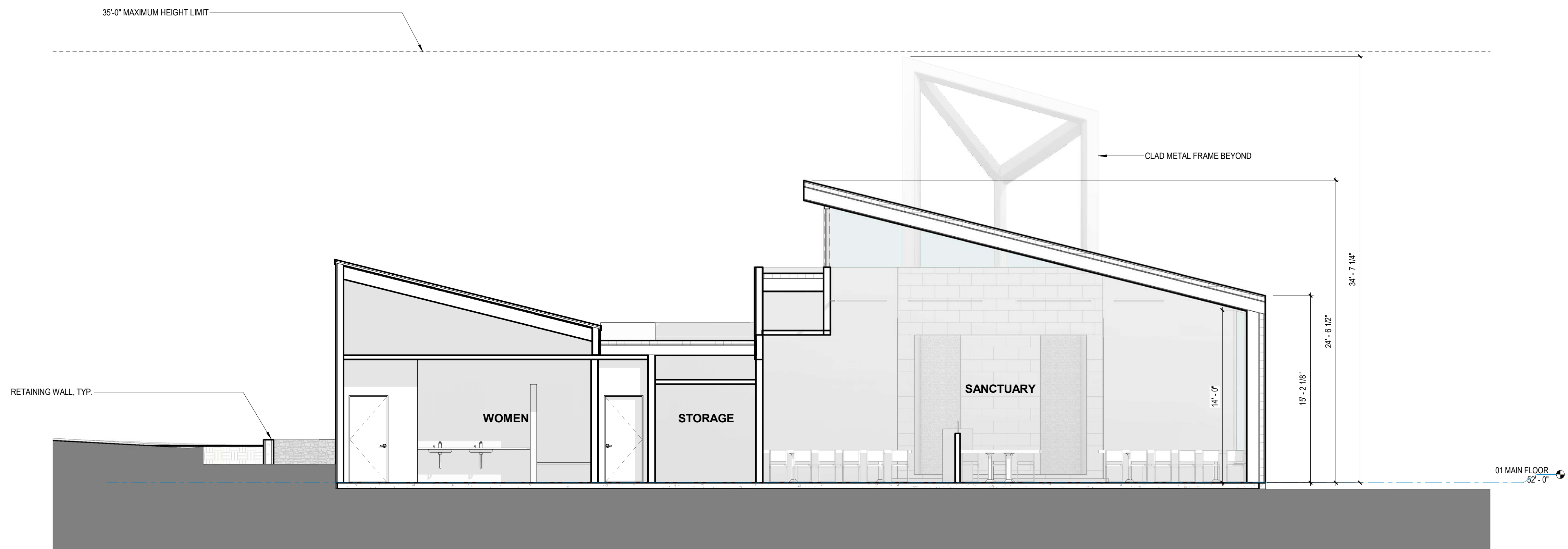
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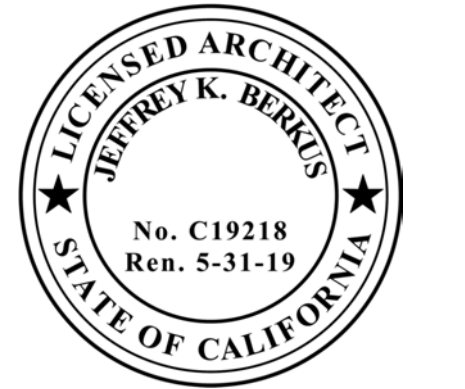


Section 4 2  
SCALE 3/16" = 1'-0"

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FOR CONSTRUCTION

PROJECT No.

**15013**

SCALE

**3/16" = 1'-0"**

SHEET TITLE

**BUILDING / PARTIAL  
SITE SECTION**

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SOUTHWEST PATIO - STAIR TO TERRACE - 2



SOUTHEAST VIEW OF SANCTUARY - 3



SOUTH ENTRY - STAIR TO TERRACE - 1

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PROJECT No.  
**15013**

SCALE

SHEET TITLE  
**PERSPECTIVE  
STUDIES**

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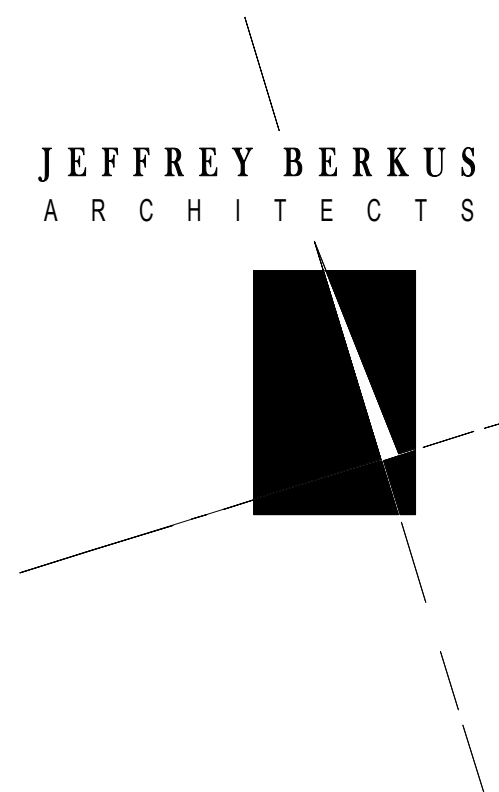


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COURTYARD VIEW OF SOUTH ENTRY



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**CHABAD OF  
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**15013**  
SCALE

SHEET TITLE  
**PERSPECTIVE  
RENDERING**

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SHEET No.

**A5.02**

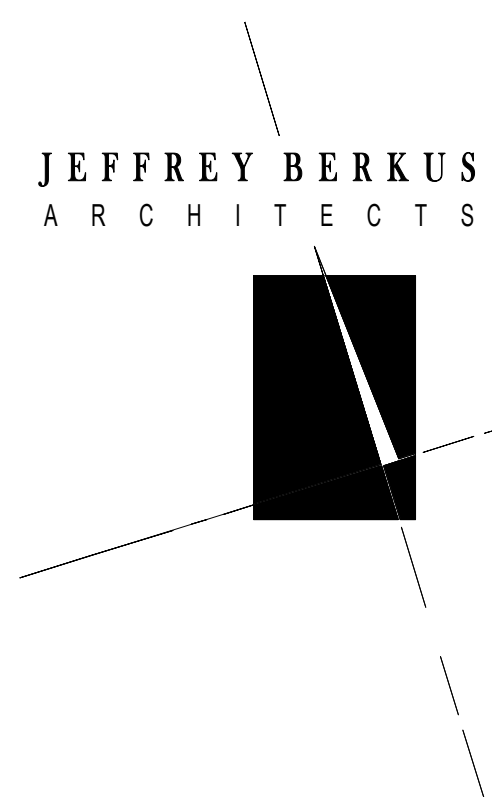


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COURTYARD VIEW OF WEST ENTRY



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SEAL



ISSUE

03-21-2018 INITIAL SUBMITTAL - CITY OF GOLETA  
11-09-2018 RESUBMITTAL - CITY OF GOLETA  
12-11-2018 DRB PRESENTATION - CITY OF GOLETA

PROJECT NAME

**CHABAD OF  
S. BARBARA**

FOR CONSTRUCTION

PROJECT No.

**15013**

SCALE

SHEET TITLE

**PERSPECTIVE  
RENDERING**

LAST PRINTED : 4/2/2019 1:57:15 AM

SHEET No.

**A5.03**

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GRADING NOTES:

1. GRADING SHALL BE IN CONFORMANCE WITH THESE PLANS AND SANTA BARBARA COUNTY GRADING ORDINANCE NO. 4477 AND THE STANDARDS AND REQUIREMENTS PERTAINING THERETO.

2. GRADING SHALL BE IN CONFORMANCE WITH THE SITE SPECIFIC GRADING RECOMMENDATIONS AND STANDARD GRADING SPECIFICATIONS CONTAINED IN THE GEOTECHNICAL REPORT AND THOSE CONTAINED IN THE GEOLOGY REPORT REFERENCED IN THE GENERAL NOTES ON THESE PLANS.

GRADING SHALL ALSO BE IN CONFORMANCE WITH RECOMMENDATIONS MADE BY THE GEOTECHNICAL ENGINEER AND THE GEOLOGIST DURING OBSERVATION AND TESTING OF SITE DEMOLITION, PREPARATION, GRADING, AND DEVELOPMENT WORK.

FOR ANY CONFLICT BETWEEN THESE PLANS AND THE RECOMMENDATIONS AND/OR SPECIFICATIONS OF THE GEOTECHNICAL ENGINEER AND THE GEOLOGIST, THE MORE STRINGENT PROVISION SHALL GOVERN.

3. CONTRACTOR SHALL EMPLOY ALL LABOR, EQUIPMENT AND METHODS REQUIRED TO PREVENT HIS OPERATIONS FROM PRODUCING DUST IN AMOUNTS DAMAGING TO PROPERTY, CULTIVATED VEGETATION AND DOMESTIC ANIMALS OR CAUSING A HAZARD TO VEHICULAR TRAFFIC OR CAUSING A NUISANCE TO PERSONS OCCUPYING BUILDINGS, IN THE VICINITY OF THE JOB SITE. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE CAUSED BY DUST RESULTING FROM HIS OPERATIONS. DUST ABATEMENT MEASURES SHALL BE CONTINUED UNTIL RELIEF IS GRANTED BY THE CITY INSPECTOR.

4. AREAS TO BE GRADED SHALL BE CLEARED OF ALL VEGETATION (EXCEPT TREES INDICATED TO REMAIN), INCLUDING ROOTS AND ROOT STRUCTURES, OTHER ORGANIC MATERIAL, DEBRIS, NON-COMPLYING FILL, AND OTHER MATERIAL UNSUITABLE FOR SUPPORT OF FILL AND/OR PROPOSED IMPROVEMENTS, AS RECOMMENDED BY AND UNDER THE OBSERVATION AND TESTING OF THE GEOTECHNICAL ENGINEER.

5. THE CONTRACTOR SHALL MAKE A THOROUGH SEARCH FOR ALL ABANDONED MAN-MADE FACILITIES SUCH AS SEPTIC TANK SYSTEMS, FUEL OR WATER STORAGE TANKS, AND PIPELINES OR CONDUITS. ANY SUCH FACILITIES ENCOUNTERED SHALL BE REMOVED AND THE VOIDS FILLED AND COMPACTED AS RECOMMENDED BY, AND UNDER THE OBSERVATION AND TESTING OF, THE GEOTECHNICAL ENGINEER AND THE CITY OF GOLETA.

VOIDS LEFT FROM OTHER REMOVALS, SUCH AS TREES AND STRUCTURES, SHALL ALSO BE FILLED AND COMPACTED AS RECOMMENDED BY, AND UNDER THE OBSERVATION AND TESTING OF, THE GEOTECHNICAL ENGINEER AND THE CITY OF GOLETA.

IF ABANDONED SEPTIC TANKS OR FUEL TANKS OR OTHER POTENTIAL SOURCES OF CONTAMINATION OR HAZARDOUS WASTE ARE ENCOUNTERED OR IF SOIL WHICH APPEARS TO BE CONTAMINATED IS ENCOUNTERED, CONTRACTOR SHALL IMMEDIATELY NOTIFY OWNER AND COUNTY HAZARDOUS MATERIALS OFFICE.

ABANDONED PIPELINES AND CONDUITS SHALL BE REMOVED UNLESS OTHERWISE ALLOWED BY THE PROJECT ENGINEER, THE GEOTECHNICAL ENGINEER AND THE CITY OF GOLETA. PIPELINES OR CONDUITS ALLOWED TO BE ABANDONED-IN-PLACE SHALL BE CRUSHED OR SHALL HAVE ALL EXPOSED OPENINGS PLUGGED WITH STIFF CONCRETE RODDED TO REMOVE VOIDS WITHIN THE PIPELINE/CONDUIT TO A MINIMUM OF 5 LINEAL FEET BEYOND THE OPENING, UNDER THE OBSERVATION OF THE GEOTECHNICAL ENGINEER AND THE CITY OF GOLETA.

6. UNDOCUMENTED FILL AND/OR UNSTABLE SOILS ENCOUNTERED DURING GRADING SHALL BE EXCAVATED TO FIRM NATIVE SOILS UNDER THE OBSERVATION AND TESTING OF THE GEOTECHNICAL ENGINEER AND THE CITY OF GOLETA.

OVERWET AND/OR PUMPING AREAS ENCOUNTERED DURING GRADING SHALL BE REVIEWED FOR CORRECTIVE MEASURES BY THE GEOTECHNICAL ENGINEER AND THE CITY OF GOLETA.

7. ALL UNSUITABLE SOIL MATERIALS AND RUBBISH AND DEBRIS RESULTING FROM DEMOLITION AND GRADING OPERATIONS SHALL BE REMOVED FROM THE JOB SITE, BE TRANSPORTED TO A SUITABLE LOCATION, AND BE DISPOSED OF IN A PROPER AND LEGAL MANNER.

8. AREAS TO RECEIVE FILL MATERIAL AND AREAS TO RECEIVE WALKWAYS, PAVEMENT AND OTHER STRUCTURAL IMPROVEMENTS SHALL BE PREPARED AS RECOMMENDED BY, AND UNDER THE OBSERVATION AND TESTING OF, THE GEOTECHNICAL ENGINEER AND THE CITY OF GOLETA. RECOMMENDATIONS FOR OVEREXCAVATION, ADDITIONAL SCARIFICATION, BACKFILL AND RECOMPACTION ARE CONTAINED IN THE GEOTECHNICAL REPORT REFERENCED IN THE GENERAL NOTES ON THESE PLANS.

9. PRIOR TO PLACEMENT OF FILL AND BACKFILL MATERIAL, THE PREPARED AREA SHALL BE INSPECTED AND APPROVED BY THE CITY INSPECTOR. THE GEOTECHNICAL ENGINEER SHALL ALSO OBSERVE THE AREAS TO BE FILLED. ALLOW A MINIMUM 48-HOUR NOTICE. FILL AND BACKFILL PLACED ON THE PREPARED AREA WITHOUT THE REQUIRED OBSERVATION SHALL BE REMOVED.

10. ALL FILL MATERIAL, WHETHER EXCAVATED ON-SITE OR IMPORTED FROM OFF-SITE, SHALL BE TESTED AND APPROVED BY THE GEOTECHNICAL ENGINEER AND THE CITY OF GOLETA PRIOR TO PLACEMENT.

IMPORTED FILL MATERIAL SHALL BE EQUAL TO OR BETTER IN QUALITY THAN THE ON-SITE SOILS AND SHALL CONFORM TO THE RECOMMENDATION OF THE GEOTECHNICAL ENGINEER. THE GEOTECHNICAL ENGINEER SHALL TEST AND APPROVE THE SOIL PROPOSED FOR IMPORT FOR STRUCTURAL FILL PRIOR TO IMPORTATION TO THE SITE.

THE LANDSCAPE ARCHITECT AND THE GEOTECHNICAL ENGINEER SHALL TEST AND APPROVE THE SOIL PROPOSED FOR IMPORT FOR LANDSCAPE AREA SURFACE MATERIAL PRIOR TO IMPORTATION TO THE SITE.

11. FILL MATERIAL SHALL BE SPREAD IN LIFTS NOT EXCEEDING 8-INCHES IN LOOSE THICKNESS, BE MOISTENED OR DRIED AS RECOMMENDED BY THE GEOTECHNICAL ENGINEER, AND BE COMPACTED TO THE MINIMUM RELATIVE COMPACTION RECOMMENDED BY THE GEOTECHNICAL ENGINEER. THINNER LIFTS MAY BE NECESSARY TO ACHIEVE THE REQUIRED MINIMUM COMPACTION.

12. FILL AND BACKFILL MATERIAL SHALL BE COMPACTED AT LEAST TO THE SPECIFIED RELATIVE COMPACTION BUT IN NO CASE TO LESS THAN 90% OF MAXIMUM DENSITY, DETERMINED IN CONFORMANCE WITH A.S.T.M. STANDARD D-1557, LATEST REVISION. SOME AREAS REQUIRE COMPACTION TO GREATER DENSITY AS CALLED FOR ON THESE PLANS OR BY THE GEOTECHNICAL ENGINEER.

COMPACTION TESTS SHALL BE CONDUCTED AS DETERMINED BY THE GEOTECHNICAL ENGINEER AND AT NOT LESS THAN ONE (1) TEST FOR EACH 2,500 SQUARE FEET OF FILL PLACED UNDER PAVEMENT AREAS AND AT NOT LESS THAN ONE (1) TEST FOR EACH 10,000 SQUARE FEET OF FILL PLACED OUTSIDE PAVEMENT AREAS, WITH THESE TESTS BEING CONDUCTED AT LEAST FOR EACH LIFT VERTICALLY AS THE FILL IS PLACED.

GRADING NOTES:

13. FINISH CUT SLOPES, FILL SLOPES AND COMBINATION CUT AND FILL SLOPES SHALL NOT EXCEED A GRADE OF 2 HORIZONTAL TO 1 VERTICAL UNLESS SPECIFICALLY CALLED FOR ON THESE PLANS AND CONSTRUCTED AS RECOMMENDED BY THE GEOTECHNICAL ENGINEER AND THE ENGINEERING GEOLOGIST.

14. ALL SLOPES OVER 3 FEET HIGH SHALL BE PLANTED WITH A NATIVE SEED MIX APPROVED BY THE CITY PLANNING AND DEVELOPMENT DEPARTMENT AND SHALL BE DENSE AND GROWING PRIOR TO FINAL INSPECTION BY THE CITY.

15. CONTRACTOR SHALL REFER TO STREET IMPROVEMENT PLANS FOR INTERFACING WITH PUBLIC GRADING, PAVING AND STORM DRAINAGE IMPROVEMENTS.

16. IF NOT DIMENSIONED, LOCATION OF FINISH GRADE ELEVATIONS AND FEATURES SUCH AS SWALES, RIDGE LINES, ETC. SHALL BE DETERMINED BY SCALE FROM KNOWN POINTS SHOWN ON THE PLANS. UNIFORM GRADIENTS OR VERTICAL CURVES, AS APPROPRIATE, SHALL BE ASSUMED BETWEEN CONTROL ELEVATIONS SHOWN ON THE PLANS.

17. SURFACE DRAINAGE GRADIENT ON SOIL OR LANDSCAPED AREAS SHALL NOT BE AT LESS THAN 2% (1/4" PER FOOT).

18. PLAN ELEVATIONS SHOWN ON WALKWAYS, RAMPS, STEPS AND OTHER HARDSCAPE ARE INTENDED TO ESTABLISH GRADING AND SURFACE DRAINAGE CONTROL FOR THESE IMPROVEMENTS. THESE CONTROL ELEVATIONS SHALL BE ADHERED TO UNLESS OTHERWISE NECESSARY FOR CONSTRUCTION OF THESE IMPROVEMENTS IN CONFORMANCE WITH BUILDING CODE AND A.D.A. REQUIREMENTS. IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO PERFORM DETAILED LAYOUT FOR AND TO CONSTRUCT WALKWAYS, RAMPS, STEPS, AND OTHER HARDSCAPE IN CONFORMANCE WITH ALL BUILDING CODE AND ACCESSIBILITY REQUIREMENTS INCLUDING THOSE FOR DIMENSIONING, SLOPE, CROSS-SLOPE, SURFACE TEXTURE, WARNING CURBS, AND HAND-RAILS.

19. DROP-OFF FROM TOP OF WALK OR FLATWORK EDGE TO ADJACENT FINISH GRADE SHALL NOT EXCEED 3 INCHES UNLESS A 6-INCH HIGH WARNING CURB IS CONSTRUCTED ALONG THE EDGE AND THE EDGE IS THICKENED (DEEPEMED) TO 6-INCHES MINIMUM BELOW ADJACENT FINISH GRADE.

20. SITE FINISH GRADING SHALL BE COMPLETED AND CONFIRMED BY SURVEY TO CONFORM TO APPROVED PLANS AND SPECIFICATIONS.

21. PAVEMENT STRUCTURAL SECTIONS SHOWN ON THESE PLANS ARE PRELIMINARY ESTIMATES. ACTUAL THICKNESS OF PAVEMENT SURFACING AND BASE COURSES SHALL BE AS DETERMINED BY THE GEOTECHNICAL ENGINEER AFTER COMPLETION OF ROUGH GRADING, BASED ON "R"-VALUE TESTS OF COMPLETED SUBGRADE MATERIAL AND THE TRAFFIC INDEXES (T.I.'S) SHOWN ON THE PLAN DETAILS. SUBJECT TO APPROVAL BY THE CITY ENGINEER. ADDITIONAL GRADING MAY BE NECESSARY TO ADJUST SUBGRADE ELEVATIONS TO ACCOMMODATE THE FINAL STRUCTURAL THICKNESS.

22. THE UPPER 12 INCHES OF FILL OR NATIVE SOIL BELOW THE BOTTOM OF AGGREGATE BASE OR SUBBASE COURSE (BELOW SUBGRADE) IN PAVEMENT AREAS, AND THE UPPER 6 INCHES OF FILL OR NATIVE SOIL BELOW THE CONCRETE OR SAND COURSE IN WALKWAY AREAS SHALL BE COMPACTED TO A MINIMUM OF 95% OF MAXIMUM DENSITY.

23. BEFORE PLACEMENT OF AGGREGATE BASE OR SUBBASE MATERIAL IN PAVEMENT AREAS, THE SUBGRADE SOIL SHALL BE REVIEWED AND TESTED BY THE GEOTECHNICAL ENGINEER.

DURING PAVING OPERATIONS, STRUCTURAL SECTION COMPACTION SHALL BE OBSERVED AND TESTED BY THE GEOTECHNICAL ENGINEER.

24. BEFORE BEGINNING WORK, CONTRACTOR SHALL OBTAIN APPROVAL FROM THE CITY ENGINEER FOR IMPORT/EXPORT HAUL ROUTES AND TIMES.

25. CONTRACTOR SHALL NOTIFY THE CITY COMMUNITY DEVELOPMENT AND COMMUNITY SERVICES DEPARTMENTS, THE GEOTECHNICAL ENGINEER, THE GEOLOGIST, AND THE ENGINEER AT LEAST 48 HOURS BEFORE START OF ANY GRADING WORK AND OF THE TIME AND LOCATION OF PRECONSTRUCTION CONFERENCE.

26. AT THE TIME OF THE PRECONSTRUCTION CONFERENCE THE GEOTECHNICAL ENGINEER IS TO DISCUSS WITH THE CITY COMMUNITY SERVICES DEPARTMENT STAFF THE LEVEL OF OBSERVATION AND TESTING TO BE REQUIRED.

27. SITE DEVELOPMENT FINISH SURFACE GRADING CRITERIA

IT IS THE DESIGN ENGINEER'S INTENT THAT THE FOLLOWING MINIMUM AND MAXIMUM SLOPES (GRADIENTS) BE MAINTAINED ON CONSTRUCTED FINISH SURFACES. IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO VERIFY THAT THESE MINIMUM AND MAXIMUM SLOPES ARE ADHERED TO AND MAINTAINED DURING CONSTRUCTION.

SURFACE TYPE	MINIMUM SLOPE	MAXIMUM SLOPE
CONCRETE WALK, CROSS-SLOPE	0.5% (200:1)	2.0% (50:1)
CONCRETE WALK, LONGITUDINAL SLOPE	0.5% (200:1)	*5.0% (20:1)
CONCRETE PAVEMENT	0.5% (200:1)	*5.0% (20:1)
ASPHALT PAVEMENT	1.0% (100:1)	*5.0% (20:1)
GRADED LANDSCAPE SWALES	1.5% (67:1)	16% (6:1)
GRADED LANDSCAPE AREAS	2.0% (50:1)	50% (2:1)

\*ACCESSIBLE RAMPS MAY EXCEED THESE MAXIMUMS BUT MUST COMPLY WITH ADA REQUIREMENTS.

EARTHWORK

ESTIMATED EARTHWORK QUANTITIES:	EXCAVATION	EMBANKMENT
SITE GRADING	1030 C.Y.	450 C.Y.
LOSS DUE TO CLEARING & GRUBBING	-150 C.Y.	+125 C.Y.
SUBTOTAL	880 C.Y.	575 C.Y.
SHRINKAGE @ 25%	220 C.Y.	
TOTAL	660 C.Y.	575 C.Y.
	EXPORT = 85 C.Y.	

- (1) ESTIMATED QUANTITIES SHOWN ABOVE ARE COMPUTED FROM EXISTING GROUND ELEVATIONS TO THE PROPOSED ELEVATIONS ON THIS PLAN.
- (2) CLEARING AND GRUBBING OPERATIONS ARE ASSUMED TO RESULT IN A LOSS OF 0.15' OVER THE GRADED AREA.
- (3) FOR THE PURPOSE OF THESE EARTHWORK CALCULATIONS, THE PAVEMENT STRUCTURAL SECTION IS ASSUMED TO BE 0.75' .
- (4) ESTIMATED QUANTITIES DO NOT INCLUDE EXCAVATION FOR UTILITY TRENCHES.

GRADING NOTES:

28. QUALITY REVIEW AND REPORTING REQUIREMENTS

- A. CONCLUSION OF ROUGH GRADING, A LICENSED LAND SURVEYOR RETAINED BY CONTRACTOR SHALL SPOT CHECK AND PROVIDE TO THE CITY OF GOLETA, THE OWNER AND THE ENGINEER WRITTEN CERTIFICATION THAT PAVEMENT SUBGRADE ELEVATIONS ARE IN CONFORMANCE WITH APPROVED GRADING PLAN. CERTIFICATION SHALL INCLUDE A DRAWING SHOWING LOCATION OF SPOT ELEVATION READINGS.
- B. UPON COMPLETION OF FINISH GRADING AND AFTER INSTALLATION OF LANDSCAPE PLANTINGS AND IRRIGATION BUT BEFORE PLACEMENT OF ANY MULCH OR WOOD CHIPS, LICENSED LAND SURVEYOR RETAINED BY CONTRACTOR SHALL DETERMINE CONSTRUCTION RECORD ELEVATIONS AT LOCATIONS REQUIRED BY THE ENGINEER, GENERALLY DESCRIBED AS FOLLOWS:

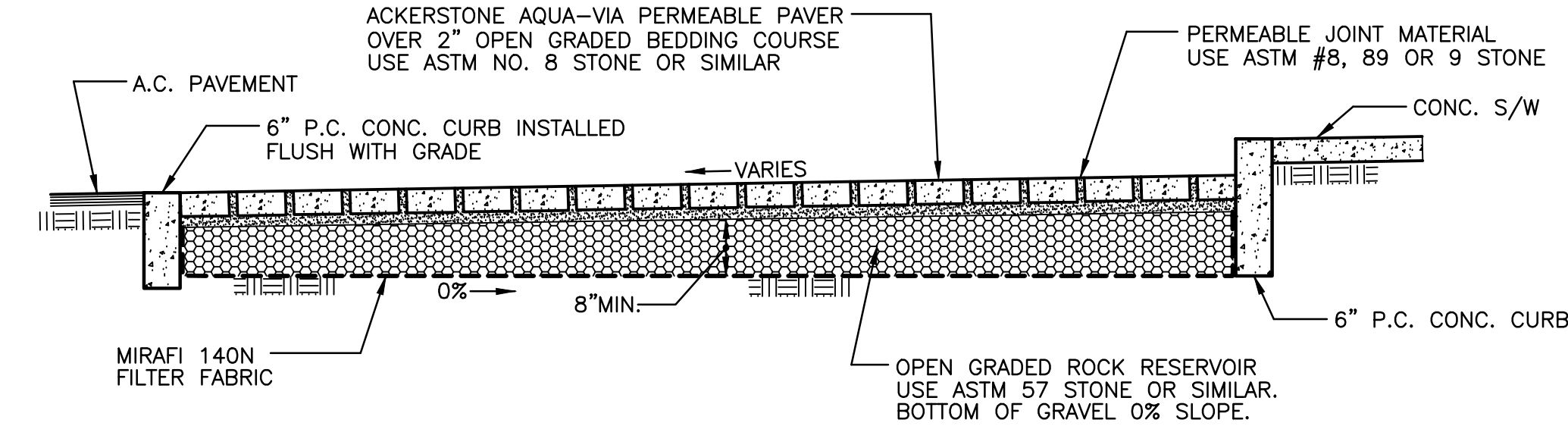
1. WALKWAY/RAMP SLOPES AND CROSS-SLOPES.
2. TOP OF CURB, TOP OF PAVEMENT & GUTTER FLOWLINE WITHIN PAVED AREAS.
3. FINISH SUBGRADE AT HIGH POINTS, GRADE BREAKS, FLOWLINES & ADJACENT TO CATCH BASINS WITHIN DIRT AREAS TO BE COVERED WITH MULCH OR WOOD CHIPS.

ELEVATIONS SHALL BE RECORDED ON A SCREENED REPRODUCIBLE OF THE IMPROVEMENT PLAN PROVIDED BY THE ENGINEER.

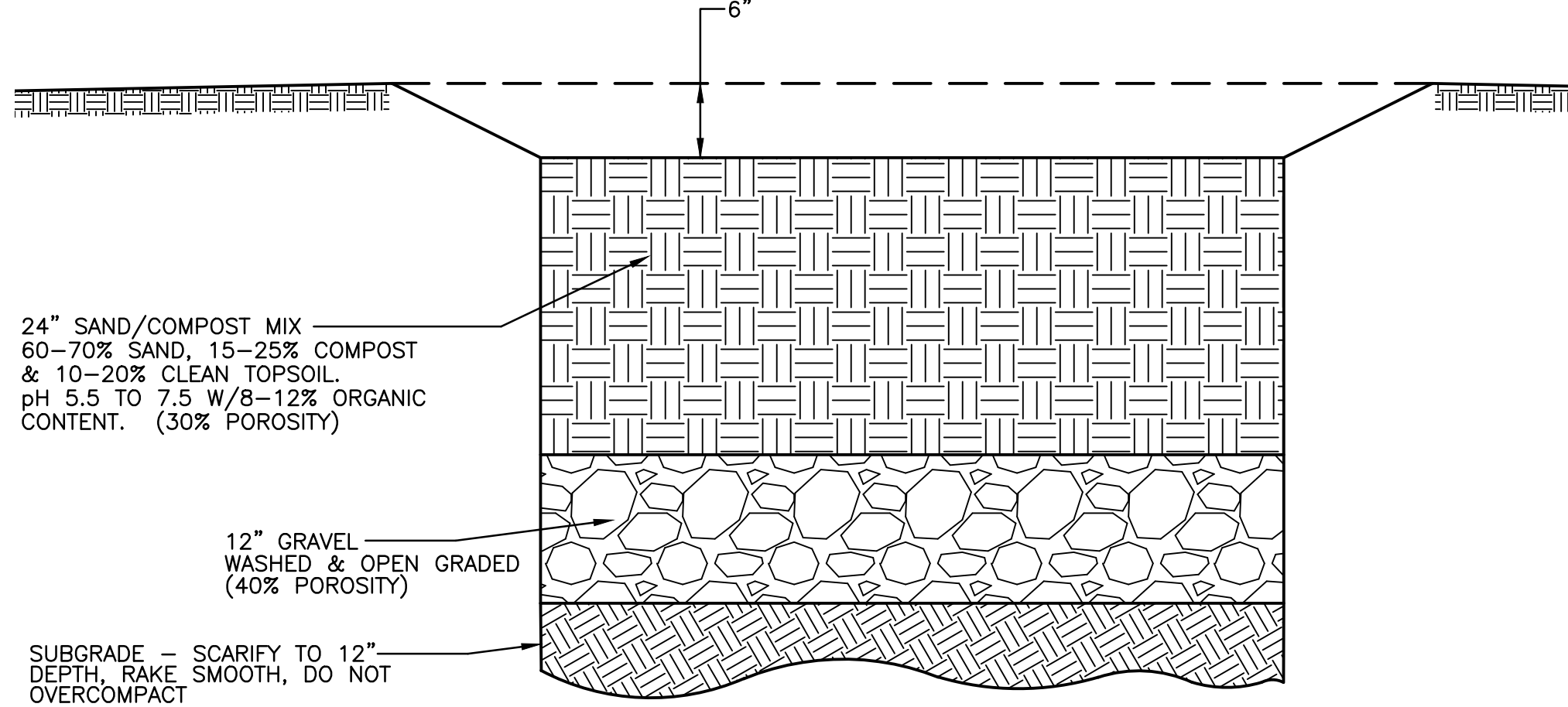
- C. GRADING AND IMPROVEMENTS FOUND NOT IN CONFORMANCE WITH APPROVED PLANS AND DESIGN INTENT SHALL BE CORRECTED BY CONTRACTOR AT CONTRACTOR'S EXPENSE. ADDITIONAL SURVEYING TO CONFIRM ELEVATIONS AFTER CORRECTIVE MEASURES SHALL ALSO BE AT CONTRACTOR'S EXPENSE. REQUIREMENTS FOR VARIOUS SURFACING CONDITIONS ARE AS FOLLOWS:

- DIRT: NOT LESS 2% (1/4 INCH PER FOOT) SLOPE IN DIRECTION OF SURFACE DRAINAGE AND 0.10 FOOT MAXIMUM DEVIATION FROM DESIGN ELEVATION AT ANY LOCATION.
- A.C. PAVEMENT: NOT LESS 1% (1/8 INCH PER FOOT) SLOPE IN DIRECTION OF SURFACE DRAINAGE AND 0.04 FOOT MAXIMUM DEVIATION FROM DESIGN ELEVATION AT ANY LOCATION.
- CONCRETE: NOT LESS 0.5% (1/16 INCH PER FOOT) SLOPE IN DIRECTION OF SURFACE DRAINAGE AND 0.02 FOOT MAXIMUM DEVIATION FROM DESIGN ELEVATION AT ANY LOCATION.

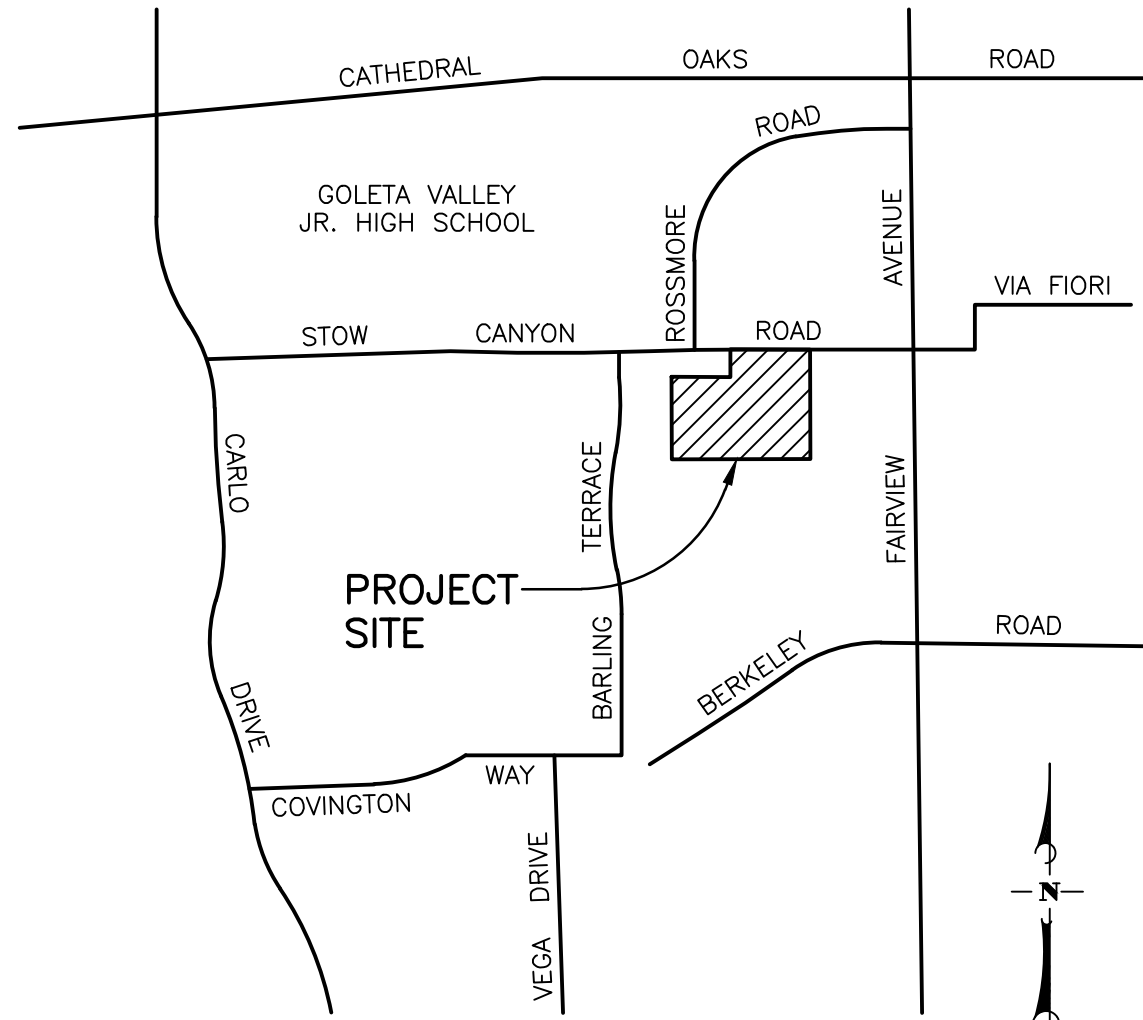
- D. FOLLOW-UP REVIEW BY ENGINEER OF MULCHED AREAS SHALL NOT RELIEVE CONTRACTOR OF RESPONSIBILITY FOR ENSURING THAT UNIFORM LAYER OF CORRECT THICKNESS OF MULCH OR CHIPS IS LAID DOWN AND THAT SUCH LAYER DOES NOT OBSTRUCT SURFACE DRAINAGE.



1 PERMEABLE PVMT. SECTION  
SCALE: 1"=2'



2 BIORETENTION BASIN  
NO SCALE



VICINITY MAP

NO SCALE

ABBREVIATIONS

A.B.	AGGREGATE BASE
A.C.	ASPHALTIC CONCRETE
A.P.N.	ASSESSORS PARCEL NUMBER
BC	BEGIN CURVE
BW	BACK OF WALK ELEVATION
CLR.	CLEARANCE
C/L	CENTER LINE
CONST.	CONSTRUCT
CONC.	CONCRETE
C.Y.	CUBIC YARD
D.I.	DROP INLET
DIA.	DIAMETER
DS	DOWN SPOUT
EC	END CURVE
ELEV.	ELEVATION
FF	FINISH FLOOR ELEVATION
F.H.	FIRE HYDRANT
FL	FLOWLINE
INV.	INVERT ELEVATION
L	LENGTH
L.F.	LINEAR FOOT
LT	LEFT
MAX.	MAXIMUM
MIN.	MINIMUM
O.C.	ON CENTER
P/L	PROPERTY LINE
PP	POWER POLE
PRC	POINT OF REVERSE CURVE
PVC	POLYVINYL CHLORIDE
R	RADIUS
RET.	RETURN
ROW	RIGHT OF WAY
RT.	RIGHT
SDMH	STORM DRAIN MANHOLE
SMH	SEWER MANHOLE
STA.	STATION
STD.	STANDARD
TB	THRUST BLOCK
TC	TOP OF CURB ELEVATION
TP	TOP OF PAVEMENT ELEVATION
TYP.	TYPICAL
TW	TOP OF WALL ELEVATION
UG	UNDERGROUND
W.L.	WATER LINE
W.V.	WATER VALVE

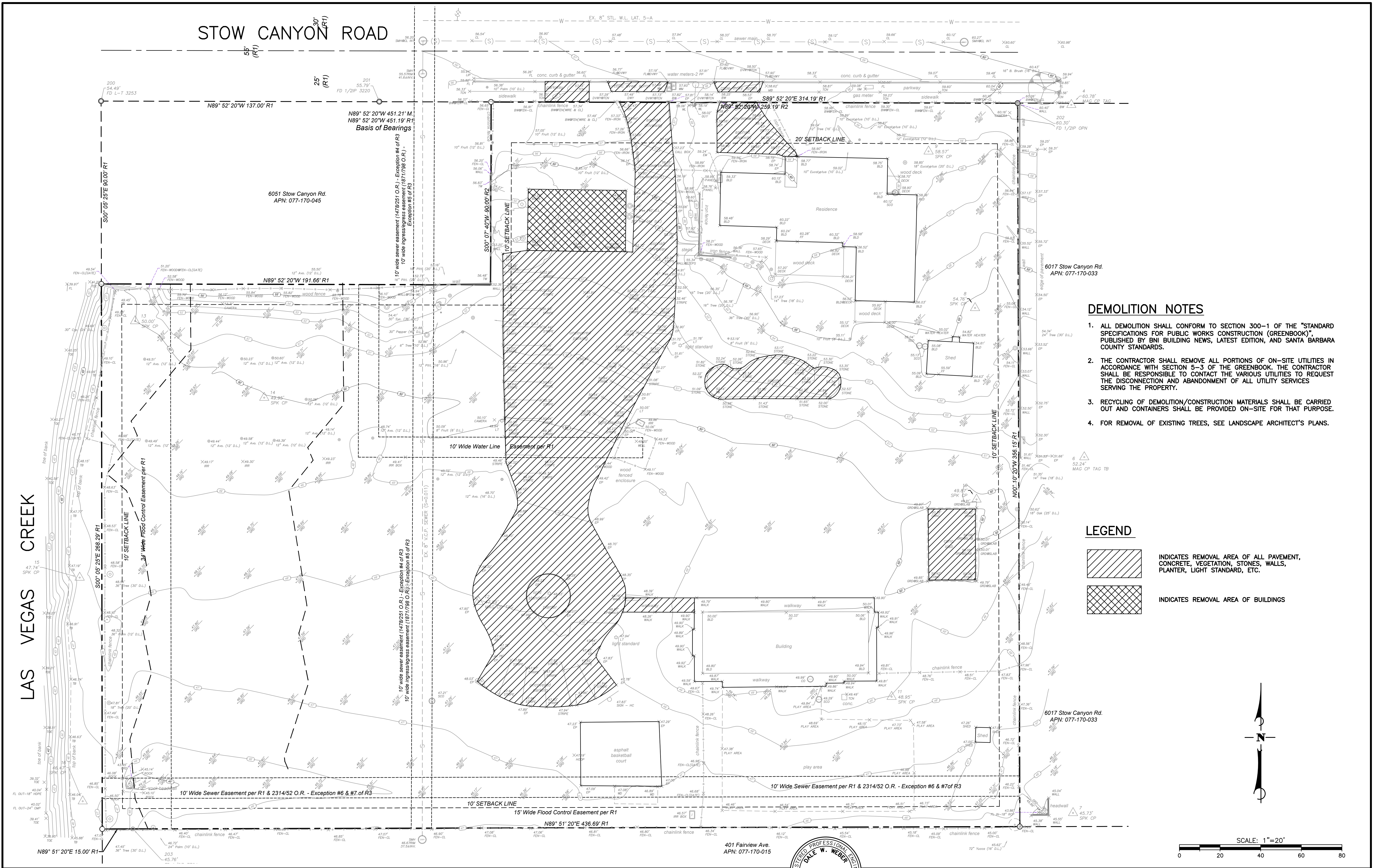
INDEX TO DRAWINGS

1. GENERAL INFORMATION
2. DEMOLITION PLAN
3. PRELIMINARY GRADING & DRAINAGE PLAN
4. PRELIMINARY UTILITY PLAN

REFERENCES: DRAWING FILE: DATA FILE: PROJECT FILE: VIEW NAME: XREF NAME(S):	NO. DATE REVISIONS APPD.	DIGIAlert DIAL TOLL FREE 1-800-227-2600 AT LEAST TWO DAYS BEFORE YOU DIG UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA	ATTENTION: ALL UNDERGROUND UTILITIES AND SUB-STRUCTURES SHOWN HEREON WERE OBTAINED FROM THE BEST AVAILABLE SOURCES AND ARE PRESENTED TO BE TO BE ACCURATE AND COMPLETE, BUT SINCE THE INFORMATION HAS BEEN OBTAINED FROM OTHERS, THE OFFICE OF MAC DESIGN ASSOCIATES CANNOT GUARANTEE THE INFORMATION IS BEING ACCURATE AND COMPLETE. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO VERIFY, LOCATE, AND PROTECT ALL UTILITIES AND SUBSTRUCTURES SHOWN ON THIS PLAN.	MAC Design Associates CIVIL ENGINEERING • LAND PLANNING • BRIDGE DESIGN 11833 CLIFF DRIVE, SUITE 16, SANTA BARBARA, CALIF. 93109 (805) 957-4746	DESIGN: DWW CHECKED: _____ DRAWN: TLA DALE W. WEBER DATE: 3-12-19 PROJECT ENGINEER R.C.E. 53753 (EXP. 6-30-21)	REGISTERED PROFESSIONAL ENGINEER DALE W. WEBER No. 53753 EXPIRES 06-30-21 CIVIL STATE OF CALIFORNIA	CITY OF GOLETA, CALIFORNIA REVIEWED BY: _____ FOR _____ DATE: _____	GENERAL INFORMATION CHABAD OF SANTA BARBARA 4065 STOW CANYON ROAD CITY OF GOLETA, CALIFORNIA	WORK ORDER W.O. 0465 SHEET 1 OF 4 FILE#
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DEMOLITION NOTES

1. ALL DEMOLITION SHALL CONFORM TO SECTION 300-1 OF THE "STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (GREENBOOK)", PUBLISHED BY BNI BUILDING NEWS, LATEST EDITION, AND SANTA BARBARA COUNTY STANDARDS.
2. THE CONTRACTOR SHALL REMOVE ALL PORTIONS OF ON-SITE UTILITIES IN ACCORDANCE WITH SECTION 5-3 OF THE GREENBOOK. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTACT THE VARIOUS UTILITIES TO REQUEST THE DISCONNECTION AND ABANDONMENT OF ALL UTILITY SERVICES SERVING THE PROPERTY.
3. RECYCLING OF DEMOLITION/CONSTRUCTION MATERIALS SHALL BE CARRIED OUT AND CONTAINERS SHALL BE PROVIDED ON-SITE FOR THAT PURPOSE.
4. FOR REMOVAL OF EXISTING TREES, SEE LANDSCAPE ARCHITECT'S PLANS.

LEGEND

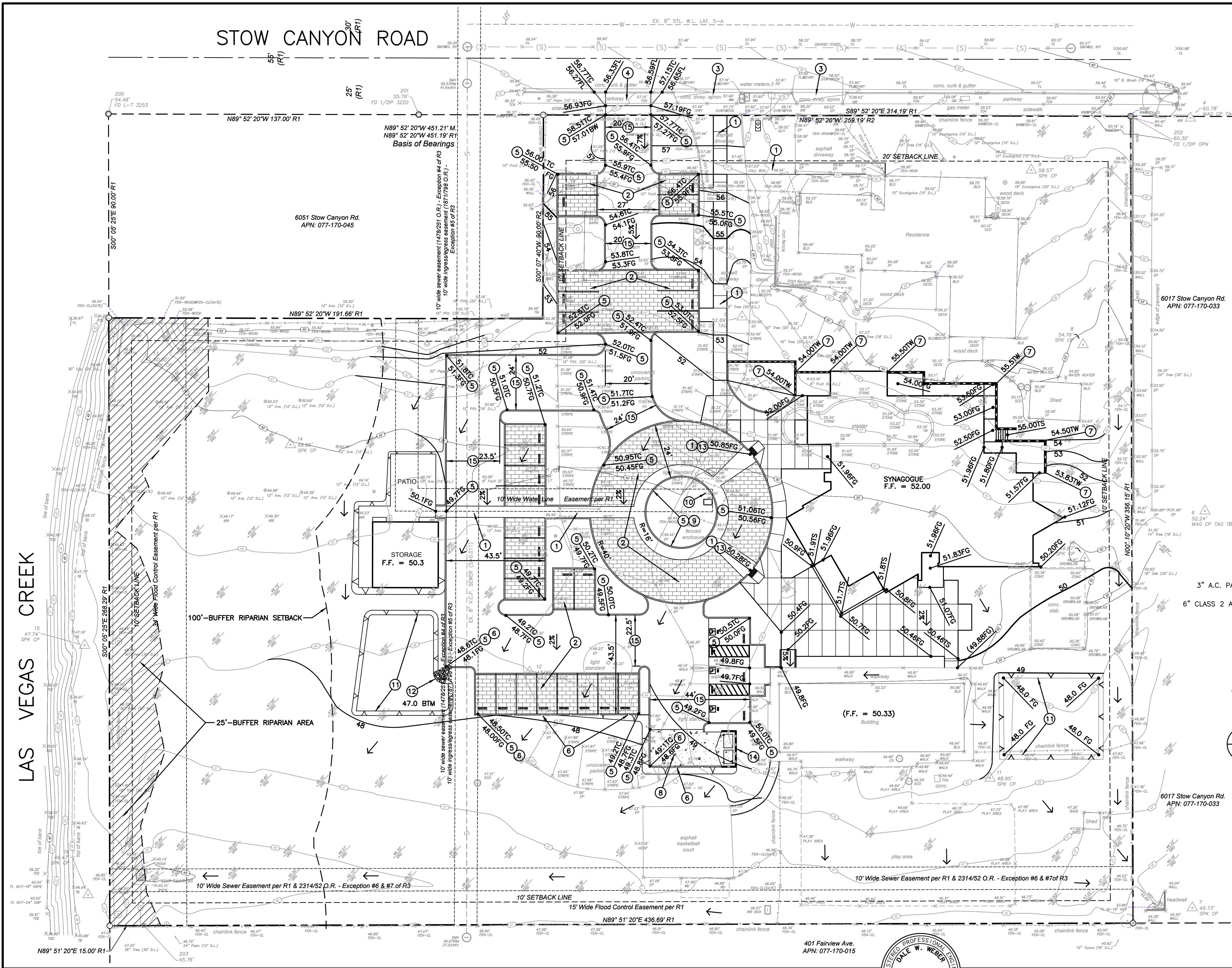
- INDICATES REMOVAL AREA OF ALL PAVEMENT, CONCRETE, VEGETATION, STONES, WALLS, PLANTER, LIGHT STANDARD, ETC.
- INDICATES REMOVAL AREA OF BUILDINGS

REFERENCES: DRAWING FILE: DATA FILE: PROJECT FILE: VIEW NAME: XREF NAME(S):  0465PGDP-DEMO.DWG 3/12/19 08:50:21 AM	NO.	DATE	REVISIONS	APPD.	 <b>DIAL TOLL FREE</b> <b>1-800-227-2600</b> <b>AT LEAST TWO DAYS</b> <b>BEFORE YOU DIG</b> <small>UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA</small>	<b>ATTENTION:</b> ALL UNDERGROUND UTILITIES AND SUB-STRUCTURES SHOWN HEREON WERE OBTAINED FROM THE BEST AVAILABLE SOURCES AND PREPARED IN ACCORDANCE WITH THE RULES AND REGULATIONS OF THE STATE OF CALIFORNIA. BUT SINCE THE INFORMATION WAS OBTAINED FROM VARIOUS SOURCES, THE MAC DESIGN ASSOCIATES CANNOT GUARANTEE THE INFORMATION IS BEING ACCURATE AND COMPLETE. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO VERIFY, LOCATE AND PROTECT ALL UTILITIES AND SUBSTRUCTURES SHOWN OR NOT SHOWN.	DESIGN: <u>DWW</u> CHECKED: _____ DRAWN: <u>TLA</u>	 DATE SIGNED _____	CITY OF GOLETA, CALIFORNIA	<b>DEMOLITION PLAN</b> <b>CHABAD OF SANTA BARBARA</b> <b>4065 STOW CANYON ROAD</b>  CITY OF GOLETA, CALIFORNIA	WORK ORDER W.O. 0465				
											SHEET <b>2</b> OF <b>4</b>				
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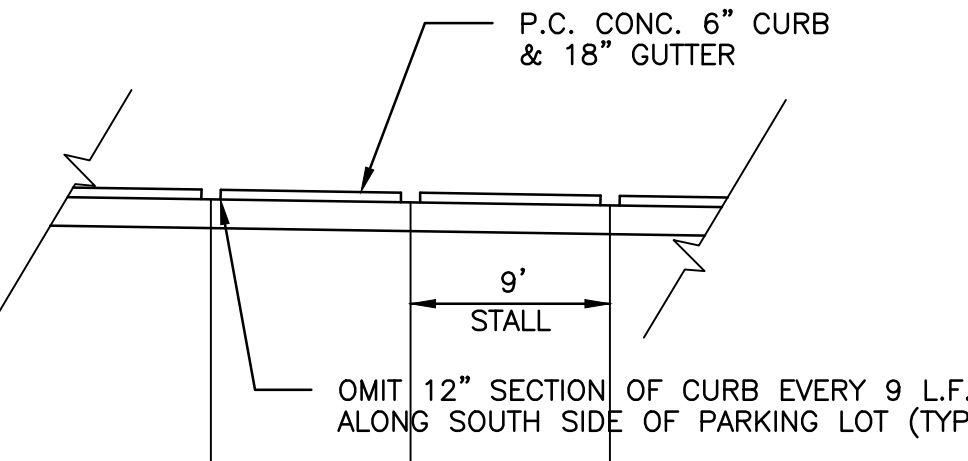
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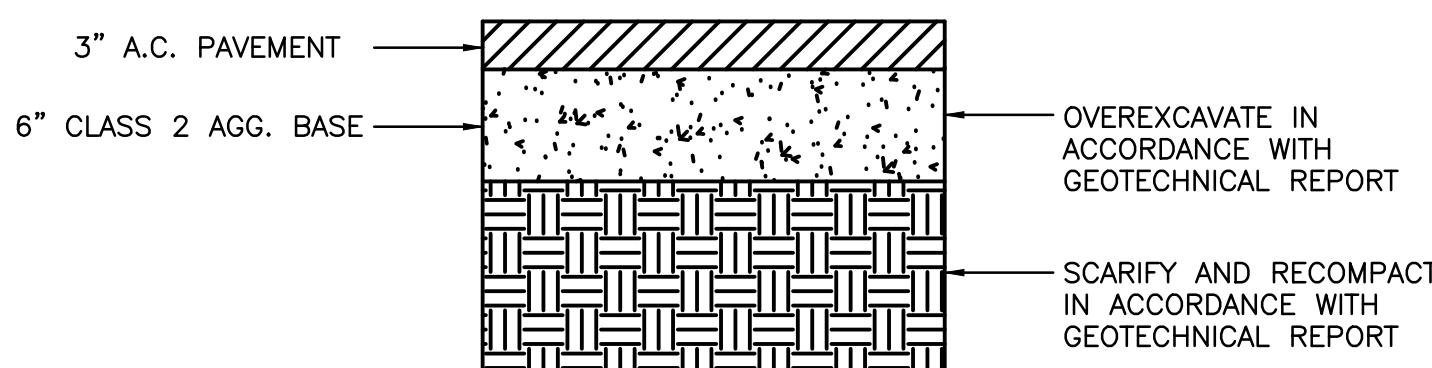


CONSTRUCTION NOTES

- 1 CONSTRUCT P.C. CONCRETE SIDEWALK.
- 2 CONSTRUCT PERMEABLE PAVEMENT PER DETAIL ON SHEET 1.
- 3 REMOVE EXISTING DRIVEWAY. SAWCUT AT GUTTER FL AND REPLACE WITH CURB, PARKWAY & SIDEWALK PER CITY OF GOLETA STD. DETAILS 3-100 & 4-040.
- 4 CONSTRUCT DROP CURB DRIVEWAY (W=20') PER CITY OF GOLETA STD. DETAILS 3-090.
- 5 CONSTRUCT 6" P.C. CONC. CURB PER CITY OF GOLETA STD. DETAIL 3-100.
- 6 CONSTRUCT 12" WIDE CURB CUT PER DETAIL 1 ON THIS SHEET.
- 7 CONSTRUCT 8" CMU RETAINING WALL.
- 8 CONSTRUCT P.C. CONCRETE PAVING.
- 9 LANDSCAPE AREA PER LANDSCAPE ARCHITECT'S PLANS.
- 10 PROTECT EXISTING WATER WELL IN PLACE.
- 11 CONSTRUCT BIORETENTION BASIN PER DETAIL 2 ON SHEET 1.
- 12 CONSTRUCT UNGROUTED ROCK RIP RAP.
- 13 CONSTRUCT PEDESTRIAN RAMP PER COUNTY OF SB STD. DETAIL 5-030.
- 14 CONSTRUCT MASONRY TRASH ENCLOSURE WITH ROOF.
- 15 CONSTRUCT ASPHALT PAVEMENT DRIVE AISLES PER DETAIL 2 ON THIS SHEET.





1 CURB CUT DETAIL  
NO SCALE NO SCALE



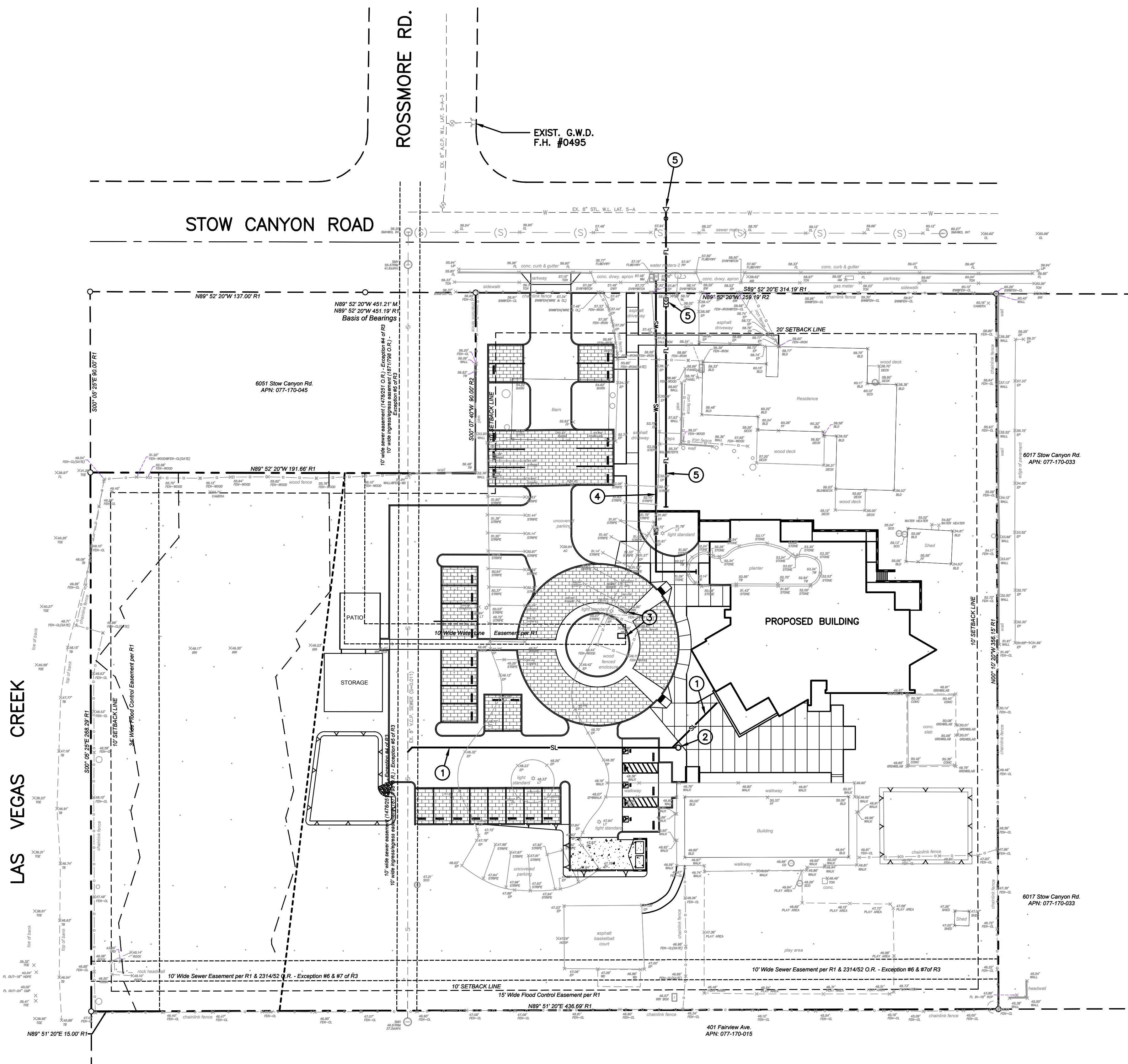
NOTES  
1. FINAL STRUCTURAL SECTION WILL BE DETERMINED BY FIELD R-VALUE TESTS CONDUCTED BY THE SOILS ENGINEER.

2 TYP. PAVEMENT SECTION  
NO SCALE

REFERENCES: DRAWING FILE: DATA FILE: PROJECT FILE: VIEW NAME: XREF NAME(S):  0465PGDP.DWG 5/23/19 08:50:21 AM PDT	NO.	DATE	REVISIONS	APPD.	 DIAL TOLL FREE 1-800-227-2600 AT LEAST TWO DAYS BEFORE THE JOB  UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA	<b>MAC Design Associates</b> CIVIL ENGINEERING • LAND PLANNING • BRIDGE DESIGN 1933 CLIFF DRIVE, SUITE 8, SANTA BARBARA, CALIF. 93101 (805) 957-4748	DESIGN: DWW	CHECKED: _____	 No. 53753 EXPIRES 06-30-21 CIVIL DATE SIGNED _____	CITY OF GOLETA, CALIFORNIA	<b>PRELIMINARY GRADING &amp; DRAINAGE PLAN</b> <b>CHABAD OF SANTA BARBARA</b> <b>4065 STOW CANYON ROAD</b> CITY OF GOLETA, CALIFORNIA	WORK ORDER W.O. 0465
							DRAWN: TLA			REVIEWED BY: _____		SHEET <b>3</b> OF <b>4</b>
							DALE W. WEBER	DATE: 5-23-19				FILE#
							PROJECT ENGINEER	(EXP. 6-30-21)				

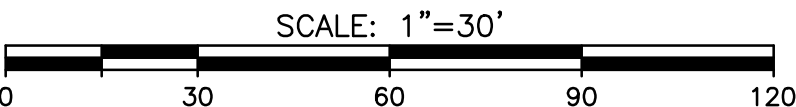
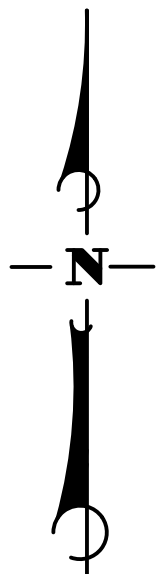


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CONSTRUCTION NOTES

- 1 CONSTRUCT 6" PVC SEWER LATERAL PER G.S.D. STANDARDS.
- 2 CONSTRUCT 6" SEWER CLEANOUT PER G.S.D. STANDARDS.
- 3 RELOCATE WELL WATER METER TO INSIDE OF CENTER ISLAND.
- 4 CONSTRUCT DOMESTIC WATER SERVICE TO NEW BUILDING. CONNECT TO EXIST. WATER METER.
- 5 CONSTRUCT 4" D.I.P. FIRE WATER LINE W/4" DOUBLE CHECK DETECTOR ASSEMBLY (WITH FIRE CONNECTION). CONNECT TO EXIST. WATER MAIN.



REFERENCES:  
DRAWING FILE:  
DATA FILE:  
PROJECT FILE:  
VIEW NAME:  
XREF NAME(S):  
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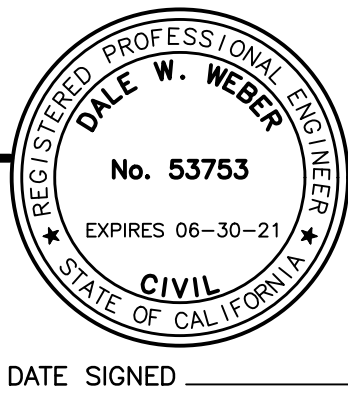
NO.	DATE	REVISIONS	APPD.

**DIGALERT**  
DIAL TOLL FREE  
1-800-227-2600  
AT LEAST TWO DAYS  
BEFORE YOU DIG  
UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA

ATTENTION:  
ALL UNDERGROUND UTILITIES AND SUB-  
STRUCTURES SHOWN HEREON WERE OBTAIN-  
ED FROM THE BEST AVAILABLE SOURCES  
AND ARE PRESENTED TO BE TO BE ACCURATE  
AND COMPLETE. BUT SINCE THE INFORMATION  
WAS OBTAINED FROM OTHERS, THE OFFICE OF  
MAC DESIGN ASSOCIATES CANNOT GUARANTEE  
SAID INFORMATION AS BEING ACCURATE AND  
COMPLETE. IT SHALL BE THE CONTRACTOR'S  
SOLE RESPONSIBILITY TO VERIFY, LOCATE AND  
PROTECT ALL UTILITIES AND SUBSTRUCTURES  
SHOWN OR NOT SHOWN.

**MAC Design Associates**  
CIVIL ENGINEERING • LAND PLANNING • BRIDGE DESIGN  
1933 CLIFF DRIVE, SUITE 6, SANTA BARBARA, CALIF. 93109 (805) 957-4748

DESIGN DWG \_\_\_\_\_ CHECKED \_\_\_\_\_  
DRAWN TLA \_\_\_\_\_  
DALE W. WEBER DATE: 3-12-19  
PROJECT ENGINEER  
R.C.E. 53753 (EXP. 6-30-21)



CITY OF GOLETA, CALIFORNIA  
REVIEWED BY: \_\_\_\_\_  
FOR \_\_\_\_\_ DATE \_\_\_\_\_

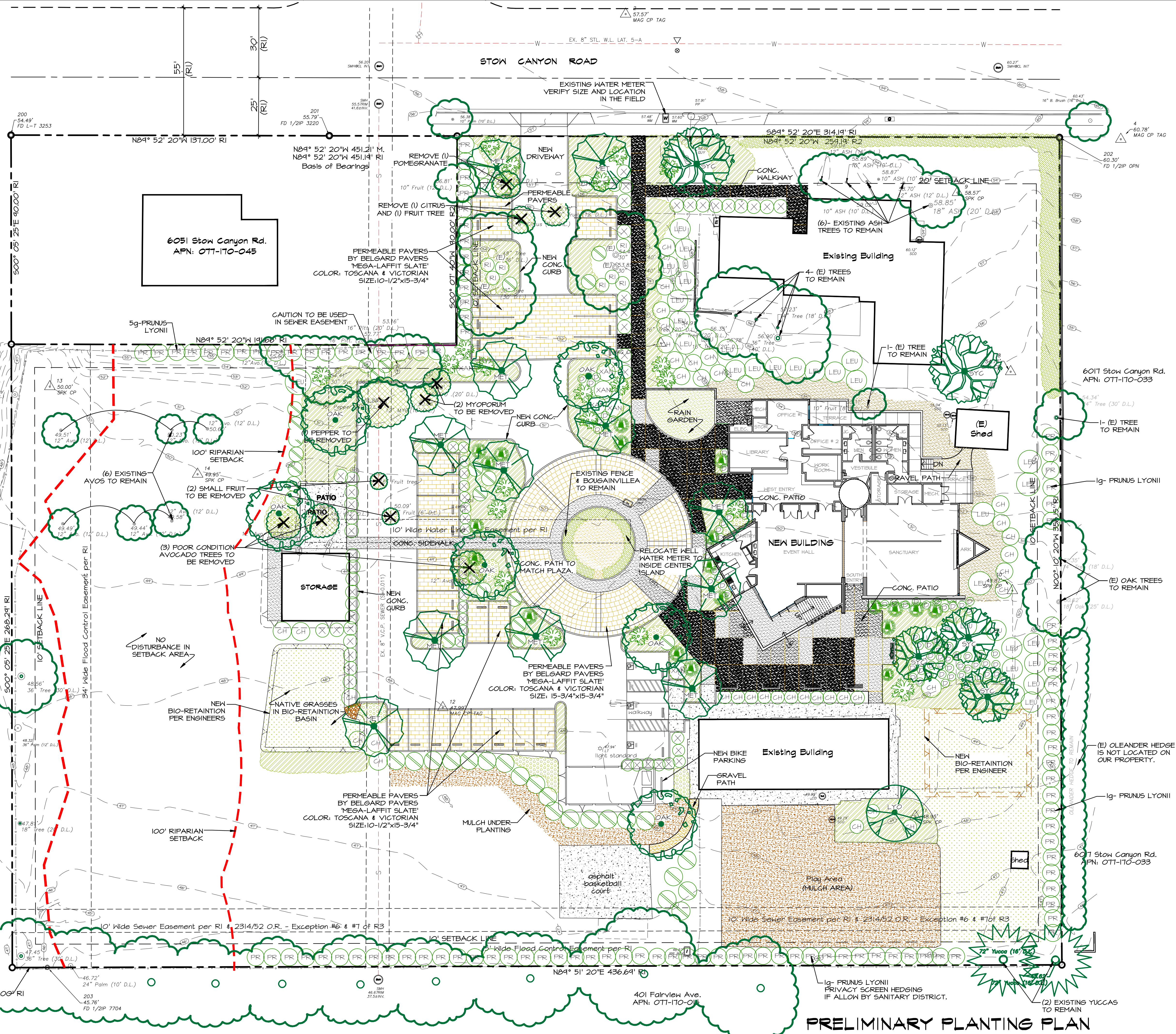
**PRELIMINARY UTILITY PLAN  
CHABAD OF SANTA BARBARA  
4065 STOW CANYON ROAD**  
CITY OF GOLETA, CALIFORNIA


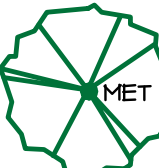












WORK ORDER  
W.O. 0465  
SHEET  
**4** OF **4**  
FILE#

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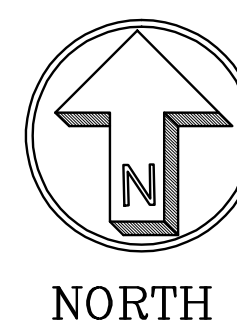


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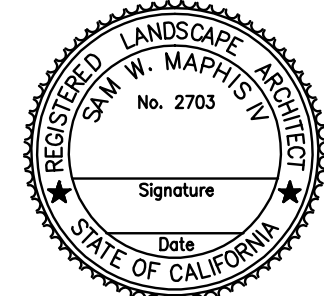
PLANT LEGEND						
SYMBOL	QTY.	SIZE	MUGOLS PLANT FACTOR	BOTANICAL NAME	COMMON NAME	
<u>TREES: choices such as:</u>						
	1	24" BOX	VL	LYONOTHAMNUS F. 'ASPENIFOLIUS'	CATALINA IRONWOOD	
	12	24" BOX	L	METROSIDEROS EXCELSUS	NEW ZEALAND CHRISTMAS TREE	
	5	24" BOX	M	PLATANUS RACEMOSA	CALIFORNIA SYCAMORE	
	6	24" BOX	L	QUERCUS AGRIFOLIA	COASTAL LIVE OAK	
<u>SHRUBS: choices such as:</u>						
	42	5g	M	CHONDROPETALUM TECTORUM	REED GRASS	
	20	5g	L	CORDYLINE A. 'TORBAY DAZZLER'		
	10	1g	L	DIANELLA C. 'CASSA BLUE'	BLUE FLAX LILY	
	31	5g	L	LEUCODENDRON 'SAFARI SUNSET'	SAFARI SUNSET	
	102	1g-5g	L	PRUNUS LYONII	CATALINA CHERRY	
	41	5g	VL	RHAMNUS 'EVE CASE'	CALIFORNIA COFFEEBERRY	
	66	5g	L	RHAPHIOLEPIS U. MINOR	DWARF YEPPON	
	7	5g	L	RIBES VIBURNIFOLIUM	EVERGREEN CURRENT	
	21	5g-15g	L	YUCCA RECURVIFOLIA	YUCCA	
	6	5g-15g	L	XANTHORRHOEA SPP.	GRASS TREE	
<u>GROUND COVERS: choices such as:</u>						
		1g	M	CAREX TUMULICOLA	FOOTHILL SEDGE	
		1g	L	CRASSULA MULTICAVA	NCN	
		1g	M	JUNCUS PATENS	CALIFORNIA GREY RUSH	
		1g	L	MYOPORUM PARVIFOLIUM 'PINK'	PINK PROSTRATE MYOPORUM	

TREE PRESERVATION LEGEND	
	=EXISTING TREE TO REMAIN
	EXISTING TREE TO BE REMOVED
1	8" APPLE TREE
1	6" APRICOT TREE
3	12" AVOCADO
2	6" CITRUS
2	6" & 8" MYOPORUM
1	36" PEPPER TREE
1	10' DIAMETER POMEGRANATE SHRUB
II	TOTAL TREES TO BE REMOVED



NORTH

SCALE: 1" = 20'-0"



REVISIONS	BY
5/1/19	AN
REV. PLANTING PLAN AND PLANT LIST PER CITY PLAN CHECKER	
5/30/19	AN
ADD TRASH ENCLOSURE & REMOVE 2 PARKING SPACES	
5/7/19	AN
CHANGE TO CAREX TUMULICOLA AND REMOVED VINES	
8/20/19	AN
UPDATE REPERIAN SETBACK LINE & STORAGE BLD.	
10/15/19	AN
ADDED BIKE PARKING & ADDITIONAL PLANTS FOR SCREENING	

EARTHFORM

DESIGN

LANDSCAPE ARCHITECTURE-LAND PLANNING-URBAN DESIGN  
1227 DE LA VINA ST. SANTA BARBARA, CALIFORNIA 93101  
TEL:(805) 963-2006 FAX:(805) 963-8335

PRELIMINARY

PLANTING PLAN

CHABAD SYNAGOGUE

6045 STOW CANYON RD.

GOLETA, CALIFORNIA

Date

3/20/19

Scale

1"=20'-0"

Drawn

A. NUNO

Job

CHABAD

Print Date

Sheet

Of

Sheets

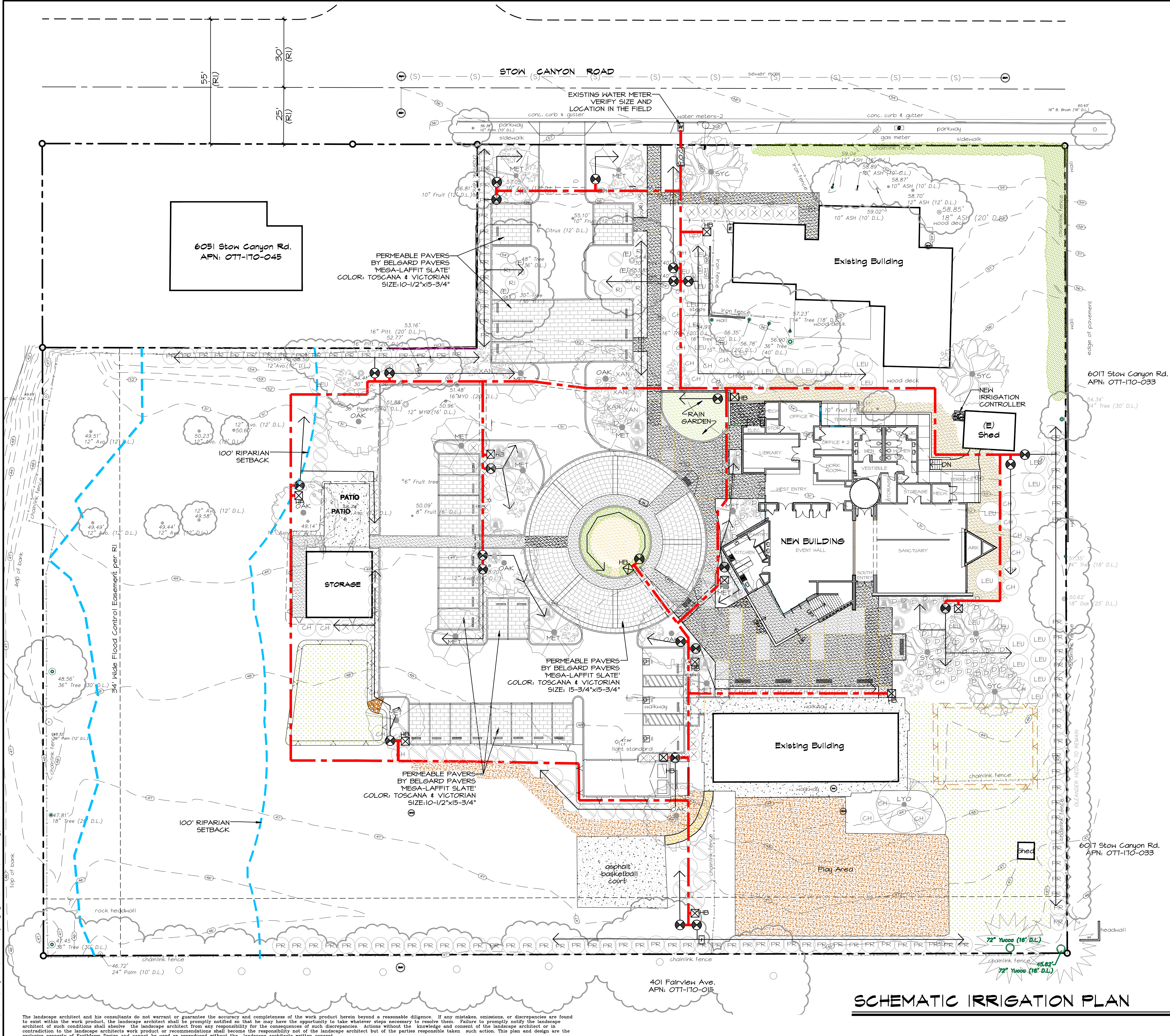
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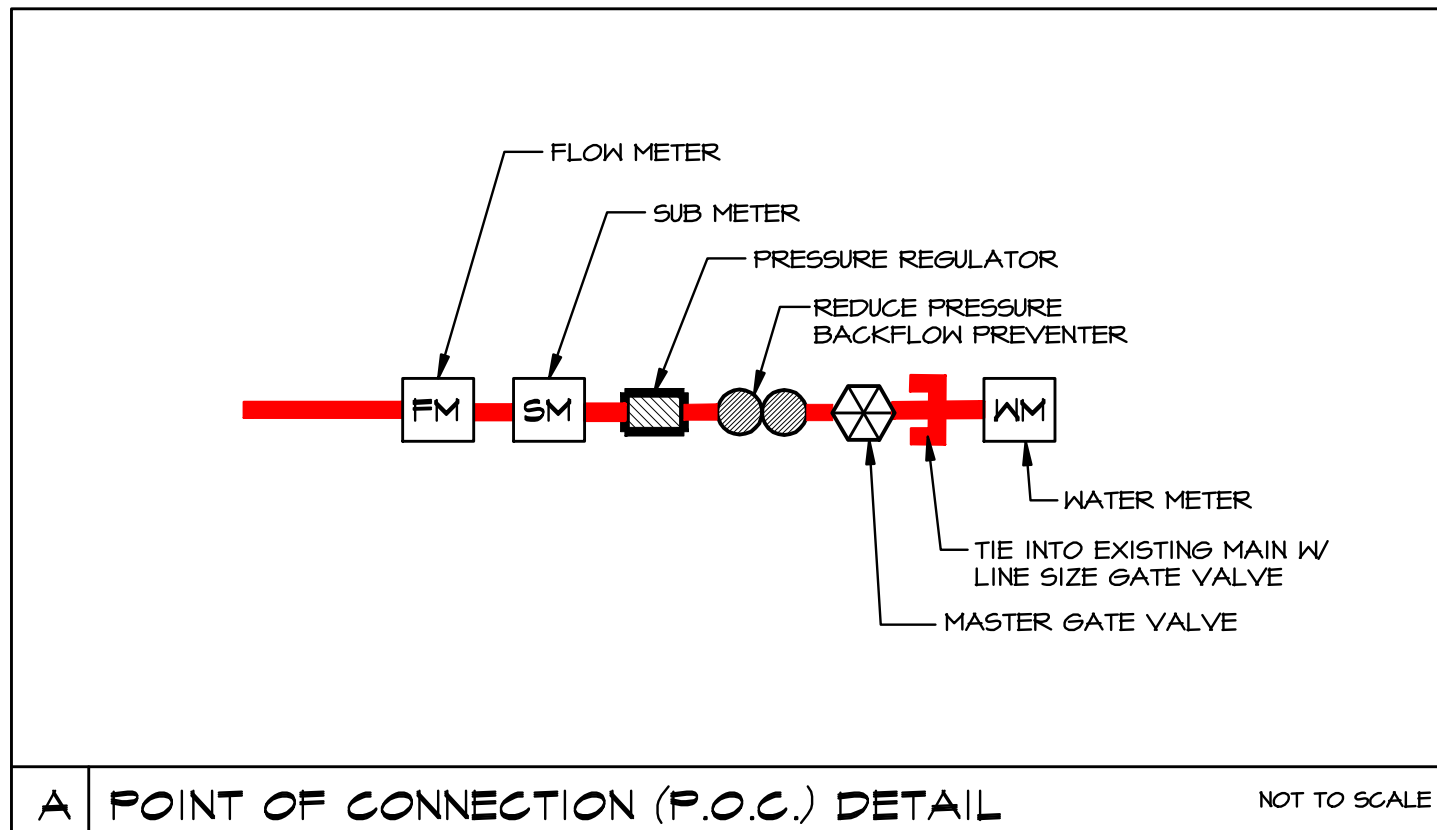
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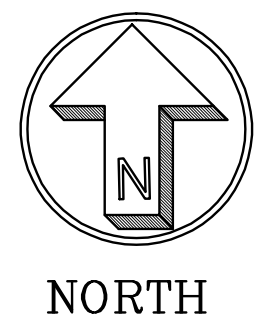
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IRRIGATION LEGEND	
SYMBOL	MANUFACTURER/ DESCRIPTION
	POINT OF CONNECTION AT PROPERTY, CONTRACTOR TO VERIFY SIZE OF METER
	LINE SIZE MANUAL GATE VALVE (NIBCO T-113)
	REDUCE PRESSURE BACKFLOW PREVENTER (FEBCO 825Y) AS NEEDED
	PRESSURE REGULATOR (WILKINS 600) SET AT OPTIMUM PRESSURE FOR HEAD PERFORMANCE AS NEEDED (85 psi) VERIFY ON SITE.
	RAINBIRD PSA SERIES, PLASTIC ELECTRIC VALVE.
	FLOW METER- (HUNTER FLOW CLICK) INSTALLED PER MANUFACTURE'S INSTRUCTIONS.
	HOSE BIB (CHAMPION B401- 3/4"x3/4" WHEEL HANDLE HOSE BIB)
	WEATHER BASE SMART IRRIGATION CONTROLLER
	IRRIGATION MAIN (SCHEDULE 40 PVC PRESSURE LINE) BURY 18" TYPICAL.
	SCHED. 40 PVC LATERAL SEE SIZING CHART.
	CLASS 200 PVC SLEEVING UNDER ALL PAVING TYP. 24" BELOW TOP OF PAVEMENT.

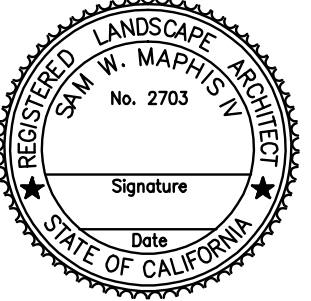


- IRRIGATION NOTES:
- CONTRACTOR TO BE RESPONSIBLE FOR COORDINATION WITH OWNER ON LOCATION OF EXISTING UNDERGROUND UTILITY AND IRRIGATION LOCATIONS.
  - CONTRACTOR TO BE RESPONSIBLE FOR FULL IRRIGATION COVERAGE OF ALL PLANTED AREAS. (LAWN, TREES, SHRUBS, AND GROUND COVER)
  - IRRIGATION PLAN TO BE COORDINATED WITH PLANTING PLAN, AND ADJUSTMENTS MADE IN THE FIELD
  - IN CASE OF DISCREPANCY, CONTACT LANDSCAPE ARCHITECT IMMEDIATELY, BEFORE PROCEEDING WITH WORK.
  - PRESSURE AT MAIN WATER SUPPLY TO BE VERIFIED BEFORE PROCEEDING. PRESSURE TEST NEW IRRIGATION MAIN FOR 24 HOURS BEFORE BACK FILL COVERING. A PRESSURE REGULATOR MAY BE NECESSARY FOR OPTIMUM SYSTEM PERFORMANCE.
  - ALL IRRIGATION LINES ARE DRAWN DIAGRAMMATICALLY AND SHOULD BE LOCATED IN COMMON TRENCHES AND PLANTING AREAS WHERE POSSIBLE.
  - SLEEVE UNDER PAVING (CLASS 200 PVC MIN.)- 24" UNDER PAVING
  - INSTALL ALL IRRIGATION EQUIP. AS PER MANUFACTURER'S INSTRUCTIONS.
  - USE GREEN OR BLACK PLASTIC (AMTEK, CARSON OR EQUAL) VALVE BOXES. ONE VALVE PER BOX.



NORTH

SCALE: 1" = 20'-0"



REVISIONS	BY
5/1/19	AN
REV. PLANTING PLAN AND PLANT LIST PER CITY PLAN CHECKER	
5/30/19	AN
ADD TRASH ENCLOSURE & REMOVE 2 PARKING SPACES	
5/7/19	AN
CHANGE TO CAREX TUMULICOLA AND REMOVE VINES	
8/20/19	AN
UPDATE REPERIAN STRACK LINE & STORAGE BLD.	
10/15/19	AN
ADDED HIKES PARKING & ADDITIONAL PLANTING FOR SCREENING	

EARTHFORM

DESIGN

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TEL:(805) 963-2006 • FAX:(805) 963-8335

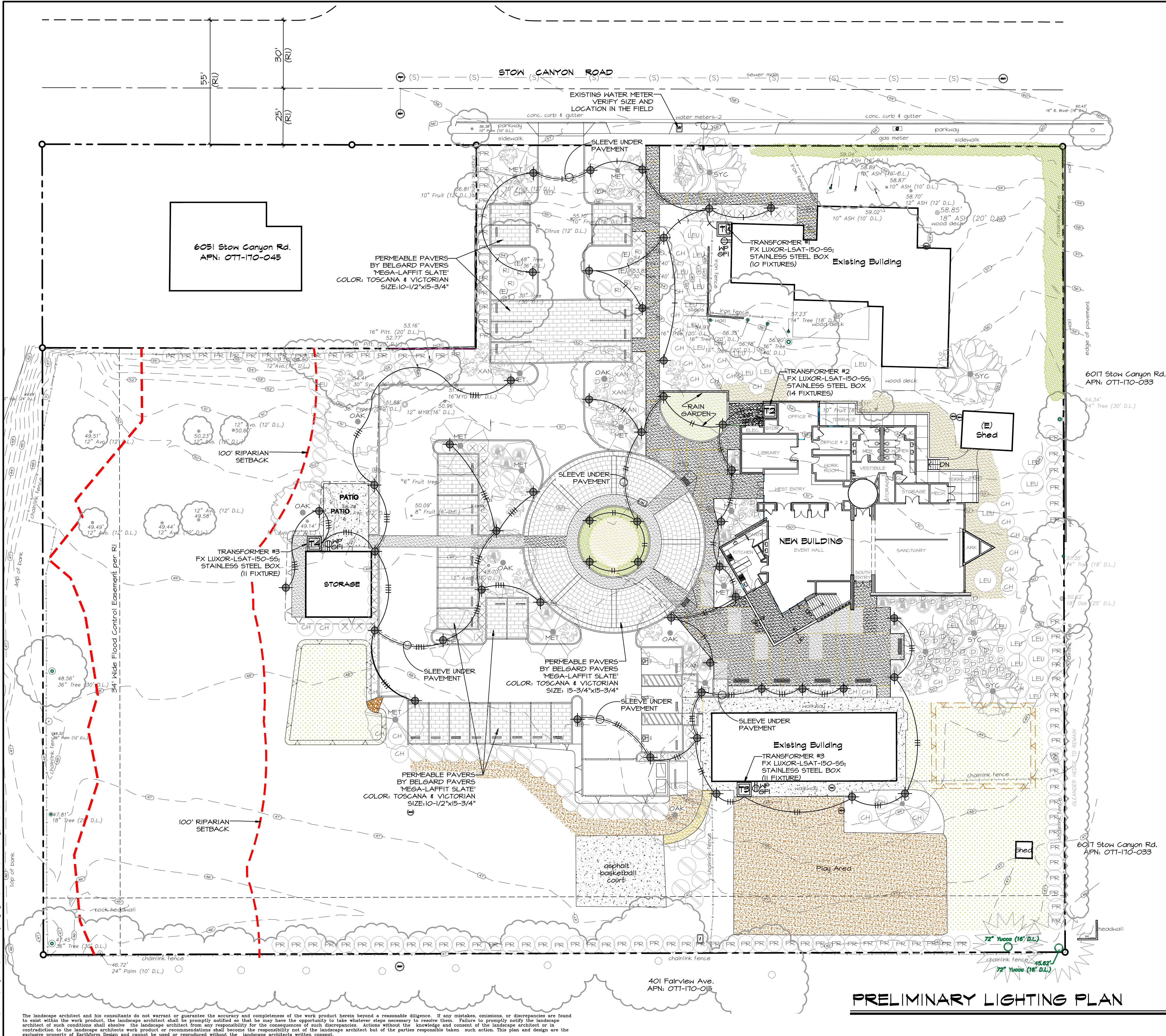
CHABAD SYNAGOGUE  
6045 STOW CANYON RD.  
GOLETA, CALIFORNIA

3/20/19  
1"=20'-0"  
A. NUNO  
CHABAD  
Print Date  
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1-2  
Of 2 Sheets



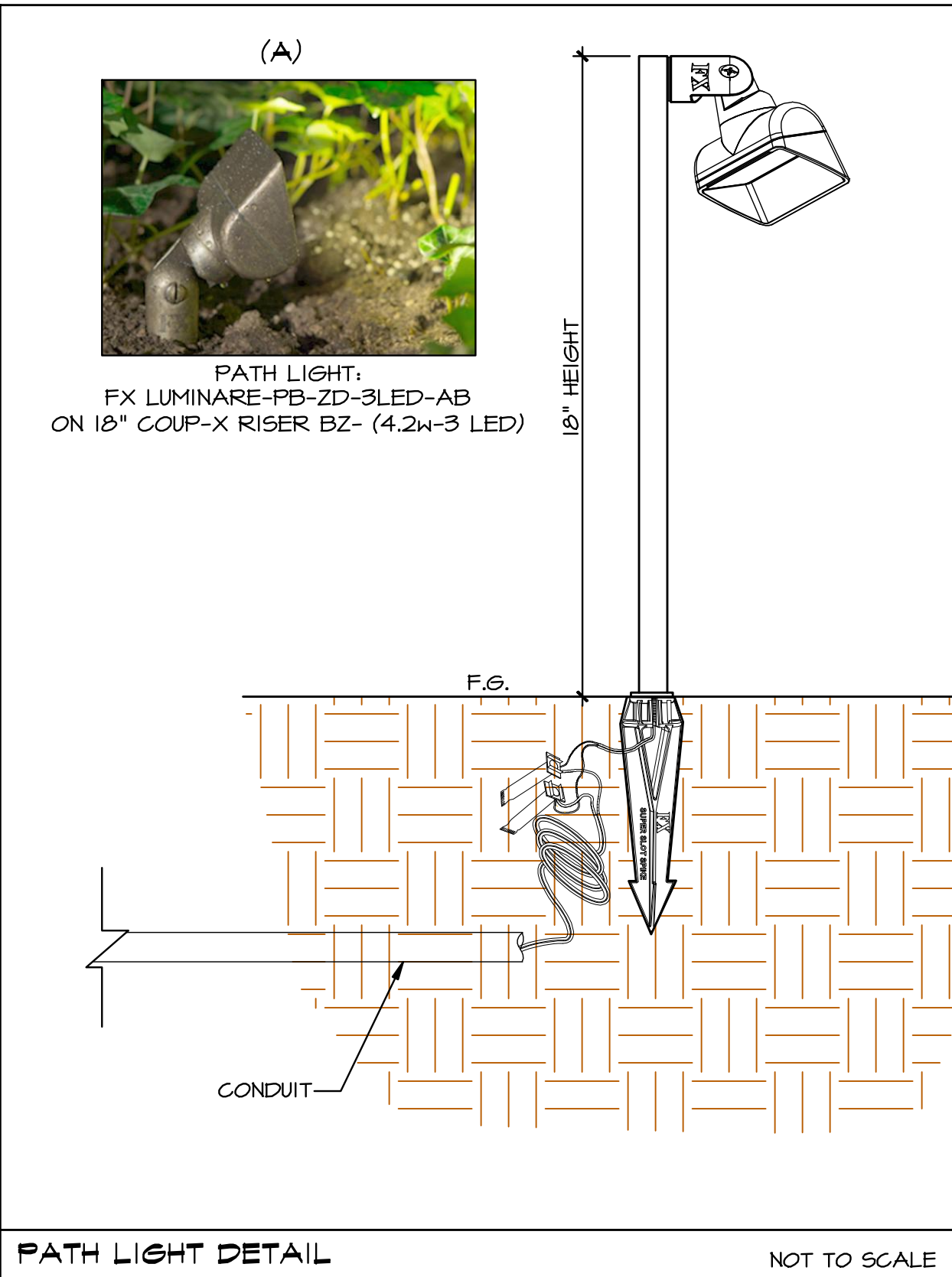
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LIGHTING LEGEND

SYMBOL	QTY.	DISCRIPTION
	4	FX LUXOR-LSAT-150W-S5; STAINLESS STEEL
	476	PATH LIGHT: FX LUMINARE -PB-ZD-3LED-AB ON 18" COUP-X RISER BZ- (4.2W-3 LED)
	4	120V 6FI, WATERPROOF OUTLET
		LANDSCAPE LIGHT SWITCHES TO BE COORDINATED WITH ELECTRICIAN AND OWNER.
		DIRECT BURIAL 10-2 LOW VOLTAGE WIRE
		CLASS 200 2"-3" SLEEVES UNDER PAVEMENT (TYP.)



REVISIONS

5/1/19	REV. PLANTING PLAN AND PLANT LIST PER CITY PLAN CHECKER	AN
5/30/19	ADD TRASH ENCLOSURE & REMOVE 2 PARKING SPACES	AN
6/7/19	CHANGE TO GARBAGE TUMULICOLA AND REMOVE VINES	AN
8/20/19	UPDATED RIPARIAN SETBACK LINE & STORAGE BLD.	AN
10/15/19	ADDED HIKES PARKING & ADDITIONAL PLANTS FOR SCREENING	AN

EARTHFORM

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PRELIMINARY LIGHTING PLAN

CHABAD SYNAGOGUE  
6045 STOW CANYON RD.  
GOLETA, CALIFORNIA

Date

3/20/19

Scale

1"=20'-0"

Drawn

A. NUNO

Job

CHABAD

Print Date

Sheet

1 of 3

NORTH

SCALE: 1" = 20'-0"

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